

NANCY McPeak

From: Eileen <Eileen@alamedamarina.net>
Sent: Thursday, May 17, 2018 10:03 AM
To: NANCY McPeak; David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; ANDREW THOMAS
Subject: Support Alameda Marina's Working Waterfront!

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Alameda Planning Board, I support Alameda Marina's plans to preserve Alameda's waterfront heritage by repairing the crumbling marina, improving maritime commercial space, providing waterlife activity, and adding housing at the site. Together, let's create a Waterfront that Works!

NANCY McPeak

From: Eileen Zedd <jus4leen@gmail.com>
Sent: Thursday, May 17, 2018 10:01 AM
To: +ateague@alamedaca.gov; +athomas@alamedaca.gov; +dburton@alamedaca.gov;
+dmitchell@alamedaca.gov; +jcavanaugh@alamedaca.gov; +rcurtis@alamedaca.gov;
+ssullivan@alamedaca.gov; NANCY McPeak
Subject: Support Alameda Marina's Working Waterfront!

Dear Alameda Planning Board, I support Alameda Marina's plans to preserve Alameda's waterfront heritage by repairing the crumbling marina, improving maritime commercial space, providing waterlife activity, and adding housing at the site. Together, let's create a Waterfront that Works!

NANCY McPeak

From: Megan Marshall <megan@sanmanproductions.com>
Sent: Thursday, May 17, 2018 10:48 AM
To: NANCY McPeak; David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; ANDREW THOMAS
Subject: Support Alameda Marina's Working Waterfront!

Dear Alameda Planning Board,

I support Alameda Marina's plans to preserve Alameda's waterfront heritage by repairing the crumbling marina, improving maritime commercial space, providing waterlife activity, and adding housing at the site. Together, let's create a Waterfront that Works!

Thank you!
Megan Marshall
Sent from my iPhone

NANCY McPeak

From: sodaguy <sodaguy@att.net>
Sent: Thursday, May 17, 2018 2:05 PM
To: NANCY McPeak; David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; ANDREW THOMAS
Subject: Support Alameda Marina's Working Waterfront!

Dear Alameda Planning Board, I support Alameda Marina's plans to preserve Alameda's waterfront heritage by repairing the crumbling marina, improving maritime commercial space, providing waterlife activity, and adding housing at the site. Together, let's create a Waterfront that Works!

P.S. The most important is as much affordable housing as possible. Alameda is a great place to grow up, and live. We should not be priced out of this awesome Island.

Sent from my T-Mobile 4G LTE Device

NANCY McPeak

From: Rosemary Reilly <reilly129@sbcglobal.net>
Sent: Friday, May 18, 2018 3:53 PM
To: NANCY McPeak; David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; ANDREW THOMAS
Subject: Support Alameda Marina's Working Waterfront!

Dear Alameda Planning Board, I support Alameda Marina's plans to preserve Alameda's waterfront heritage by repairing the crumbling marina, improving maritime commercial space, providing waterlife activity, and adding housing at the site. Together, let's create a Waterfront that Works!

I live nearby at Barnhill Marina and am excited about this project. Having lived in Alameda all my life, to see public access to the water front, improvements and quality housing looks great.

Rosemary Reilly

Sent from my iPad

NANCY McPeak

From: Becca Perata <becca@voxpathulipr.net>
Sent: Monday, May 21, 2018 9:01 AM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Support Housing at Alameda Marina!

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Alameda Planning Board,

The Master Plan for Alameda Marina helps fulfill the City's Housing Element by constructing housing at various levels of affordability along the Northern Waterfront.

Together, let's create a Waterfront that Works! I encourage your approval of the Master Plan on May 29th.

Sent from my iPhone

5/21/18

Mr. David Burton

I am counting on you to "solve" the traffic problem before you approve any new housing construction

The Master Plan for Alameda Marina may help fulfill the City's Housing Element by constructing housing at various levels of affordability along the Northern Waterfront **BUT** you need to seriously address & solve automobile traffic leaving & returning the Island before you add to the problem

Please solve this traffic problem before you approve any additional new housing construction. Remember what happened last March when the Posey tub was closed due to the sink hole

I encourage you to NOT approve the Alameda Marina Master plan on ~~May~~ May 29th

Sincerely
Daniel Ouellet
1355 Grove St
Alameda CA 94501
byteracer@gmail.com



NANCY McPeak

From: Nancy Hird <nancy.alameda1@att.net>
Sent: Monday, May 21, 2018 12:25 PM
To: NANCY McPeak
Subject: Housing at Alameda Marina!

Dear Alameda Planning Board,

The Master Plan for Alameda Marina shows Bay West intends to provide 103 affordable housing units out of a total of 760. This barely scratches the surface of the affordable housing needs while creating a traffic nightmare on a small stretch of Clement Ave. between Park and Grand Streets. This road is a Truck Route and a bike path is also being considered. Please do not add such a high number of housing units to those already approved for this street that is so close to the Park Street Bridge.

This development further erodes our already lopsided jobs housing balance. We should be using this property to expand our maritime and blue economies and adding convenient services to the north coast communities that will reduce the need for automobiles. Bay West's project does not include nearly enough parking to meet the needs of the new residents, visitors, recreational boaters who have vessels in the marina and users of the planned recreational facilities for this development.

Together, let's create a Waterfront that Works! I encourage you to think about the effects of this Master Plan and its negative effects on our community on May 29th.

Sincerely,
Nancy Hird

NANCY McPeak

From: Eric Grunseth <egrunseth@comcast.net>
Sent: Monday, May 21, 2018 4:59 PM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Not in support of housing at Alameda Marina

Dear Alameda Planning Board,

Adding an additional 700+ housing units to a crowded City is just crazy. Have you tried to get off the Island lately? Without another bridge, we are doomed to a snail's pace. What is the quality of that life?

Eric Grunseth

NANCY McPeak

From: Liz Taylor <liz@doermarine.com>
Sent: Monday, May 21, 2018 6:43 PM
To: NANCY McPeak; David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; ANDREW THOMAS
Subject: May 29th Item 7A Master Plan Alameda Marina
Attachments: Alameda Marina Eng. Rept..pdf; Weston Peer Review - AM.pdf

Regarding May 29th Item 7A: Master Plan, Density Bonus Application, and Environmental Impact Report for the Alameda Marina

Dear Alameda Planning Board,

The master plan being proposed for Alameda Marina by Bay West does little to help the affordable housing crisis with only 103 of 760 units characterized as affordable. It further erodes the on island to jobs to housing balance.

In addition, this site is not along an existing transit route. Rather, it is situated on a heavily utilized truck route that is already being impacted by new development along Buena Vista and Clement. Del Monte, which is already approved to build, will add car trips as will the Site A development as people work to find alternative routes to the tube. Alameda Marina should be a low priority for approval until after Del Monte and Site A come on line and those impacts can be addressed. The recently proposed Holiday Inn at Park and Clement is yet another factor that bears consideration.

This latest version of the master plan does retain some maritime use but it is not well thought out. There is no real investment as the proposed services are primarily barge based. These floating assets, like the floating docks, can be removed and sold at any time. Barges do not represent an enduring solution. Rather, they may create a hazard should they break away in storm condition or suffer water ingress, sinking at the dock. The lone hoist proposed is towards the west end of the complex where the most dredging is required and the parking is impractical for safe maneuvering of vehicles/trailers. It is important to remember that this developer has no maritime experience or track record, yet is asking to be entrusted with creating a "waterfront that works". Having the word "Bay" in their name is not sufficient.

On their watch, valuable skilled jobs have been lost as the tidelands have deteriorated. Please see the attached 2010 engineering report. It provided an independent evaluation of conditions and laid out a detailed plan, prioritizing repairs and a budget. The budget was recertified as correct in 2011 in a peer reviewed document (attached) The deferred maintenance over the past eight years resulted in the loss of access to the tidelands in violation of BCDC rules, damaged maritime businesses, and reduced three working boat hoists to two. That the repair costs have ballooned to \$35M is largely a result of neglect on the part of Bay West as the majority shareholder in Pacific Shops. The city and citizens of Alameda should not have to pay for it either in cash or by loss of life quality.

The master plan calls for leveling numerous unique work shop spaces while destroying the architectural integrity of the landmark "Alameda Marina" building by dividing it up into multiple floors of shops. Affordable workshop and maker spaces are even more endangered than affordable housing. Without them, innovation, research, and development is stifled. The bay area is famous for iconic brands that started in a barn, shed, garage or shop space. Let's help keep those opportunities alive by protecting these shop spaces for the next generation of thinkers, doers and blue economic development. Once buildings like these are gone, they are gone for good.

Alameda has a wealth of actual maritime experience between the boating community and maritime businesses. Together, let's create a Waterfront that Works!

I encourage you to think about the impacts of this Master Plan and the profound negative effects approving it will have on our community on May 29th.

Sincerely,
Liz Taylor



Client: Sea
Alameda Marina
1815 Governor Avenue
Alameda, CA 94601

Submitted by:
Sea Engineering, Inc.
1100 Lakeside Drive, Suite 200
San Francisco, CA 94132

December, 2010

Alameda Marina – Preliminary Engineering Report



Prepared By

Ken Israel, Senior Consultant/Project Manager, Sea Engineering, Inc.

Scott Rodensleiner, Dredging Consultant, Weston Solutions

Reviewed by: Jason Magallon, Coastal Engineer, Sea Engineering, Inc.

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Executive Summary

Alameda Marina - Preliminary Engineering Study

The Alameda Marina (Marina) is located on the north side of the City of Alameda, California adjacent to the Oakland estuary. Marina construction began in the 1960s and expansion continued into the 1970s. There have been many improvements made in the facility throughout its history. These range from shore-side repairs, electrical upgrades, pier construction, and regular maintenance in an effort to continue Alameda Marina's reputation as an efficient and attractive marina. Today it provides 464 slips and is capable of providing berthing for up to 533 vessels. The marina is arranged on ten piers known as "Piers 1 through 10" and the "East Pier." All of the berths on Pier 1 and on the east side of Pier 2 (166 total berths) are situated on a parcel owned in fee by Alameda Marina. The remaining 300 slips are within the leasehold boundaries, and the subject of this report.

The Marina has been requested to complete this Preliminary Engineering Study (Study) of the Alameda Marina facility for general operations and capital improvement guidance during the next 15 years of operation. Sea Engineering, Inc. (SEI) has completed the Study and the results of which are discussed in detail below. SEI was assisted in this study by the Alameda Marina managers and operation personnel who provided insight, suggestions and phasing recommendations. SEI also consulted with the Weston Solutions staff to draft dredge-demand recommendations, compliance, and disposal options.

This Study includes an evaluation of the physical and operational conditions of the existing marina facilities. The primary focus is upon marina basin shoaling, boating access sites, head piers and floating docks, electrical and water supply, and shoreline site conditions. Based upon the results of the evaluation, improvements were recommended by SEI and reviewed by the Marina management personnel. Marina management has prioritized the recommendations so that a phased approach can be implemented to account for funding.

1.0 Background

The Marina is surrounded by a mix of industrial and residential properties. On the West side, another marina complex exists (Grand Marina), which supports vehicle parking and light maritime operations including small ship repair. As shown in the Appendix, the southern boundary of the Marina property is Clement Avenue. Land use to the south of Clement Avenue is predominantly residential.

An infrequently dredged federal navigation channel (Brooklyn Basin) runs parallel (West to East) to the Marina shoreline and adjacent to the Marina's offshore perimeter. North of this channel is Coast Guard Island, home port to several U.S. Coast Guard vessels of varying class and size. Historical land uses of the region including the Marina have included nearby military installations involving ship repair and maintenance. Currently the Navy Reserve Center borders the Marina to the east.

The Marina owns 123,300 square feet (SF) of floating docks contained within a 16.4 acre basin and is capable of berthing 633 boats (i.e. fee simple comprises 166 slips, versus a leasehold slip count of 368) with 464 slips. For the purposes of this evaluation, the basin areas are calculated from the mean low tide shoreline to the northern limits of the marina. Since all construction more than 40 years ago, water depths have remained sufficient throughout the Marina for vessel operation. The exceptions to this are at the West end, where siltation has been triggered by Coast Guard berthing related snipping activity (e.g. tug boat sediment mobilization). Furthermore, a portion of the Lease Area A basin is unusable where old piers have been deconstructed, leaving over 1,000 deadhead wood piles cut off at mud line. These deadhead piles are exposed at low tides, but do not present a problem as the Marina is currently configured.

In contrast to average San Francisco Bay marinas, approximately half of Alameda Marina's berths are for smaller boats (e.g. less than 30'). Most marinas in San Francisco Bay have difficulty leasing berths for 30' or smaller boats. Consequently, the general trend is for marinas to replace their smaller berths with larger ones, resulting in a lower number of available berths. If Alameda Marina decides to convert smaller berths to larger berths, careful planning will be required to provide a smooth transition because of the large number of small craft that currently dock at the Marina.

The Marina faces a number of offshore challenges to rehabilitation and modernization goals. Based on the Shoreline and Marina Repair and Improvements Report, the most significant of these offshore challenges include the following:

- Dredging in the western area of the marina.
- Over 1000 deadhead piles cut off at the mud line where historic spring piers were deconstructed making a large portion of the Marina's submerged leasehold unusable.
- Approximately one half of the Marina's 317 berths are configured for smaller vessels (less than 30'). A number of the existing floating dock piers are in need of repair and/or replacement.

2. Dredging Background

The original dredging and excavation operations carried out to install the Alameda Marina docks occurred in 1967. According to available regulatory records kept by the two San Francisco Bay Area dredge permitting agencies, the U.S. Army Corps of Engineers (USACE) and San Francisco Bay Conservation and Development Commission (BCDC), dredging at the Alameda Marina has not occurred since 1967. Furthermore, the only documentation of this event is a BCDC permit (BCDC M57-1) authorizing Pacific Ship Repair to perform maintenance dredging in the Marina and subsequently dispose of dredged material at the dumping ground west of Alcatraz Island. A copy of this permit can be provided if requested.

Records of historical sediment quality testing associated with the 1967 dredging event, or any other dredge events that may have occurred since then, have not been found. However, the Clean Water Act was not passed until 1972, so it is unlikely that any such testing had been performed prior to the 1967 dredge event. The fact that no documentation of sediment testing since 1972 has occurred, also indicates that no dredging has occurred since 1967.

Typical sources of marina contaminants such as storm drains or fuel docks are not present within the Marina boundaries; however, due to the large parking area and the presence of current and historical ship repair operations, storm water runoff may be a potential historical and current contaminant source.

Of more significance, the Brooklyn Basin federal channel runs adjacent to the Marina complex. Preliminary sediment quality results performed with samples collected from this channel showed elevated concentrations of metals from various sources, butyltins, which were historically used in boat hull coating compounds; polychlorinated biphenyls (PCBs), which are electronic insulation compounds used in transformers and other electrical components, and polycyclic aromatic hydrocarbons (PAHs), the more toxic fraction of petroleum products including the compound creosote, historically used as a protective coating for pier pilings.

Current Dredging Needs

Dredging is needed to restore unencumbered maneuvering of recreational vessels throughout the western portion of the Marina. The proposed disposal site for the Marina dredged material is the SF-11 in-Bay disposal site near Alcatraz Is and Figures in Appendix A provide a vicinity map depicting the location of the Alameda Marina and the potential disposal site, SF-11. The proposed dredge depth is 12 feet below Mean Lower Low Water (-12 ft MLLW). This proposed dredge depth includes a two-foot over dredge allowance. The estimated total volume of dredged material proposed for removal from the Marina, including material accounted for by the two-foot over dredge allowance, is approximately 25,150 cubic yards (CY).

2.1 Permitting

Standard San Francisco Bay maintenance dredging permit requirements include the following:

1. Application for 10-year maintenance and navigation improvement permits with the USACE and BCDC;
2. Request Water Quality Certification from the Regional Water Quality Control Board (RWQCB);

3. Research and development of rationale and justification for appropriate authorized and required depths
4. Based on current bathymetry and justification assessment, dredge volume estimate will be confirmed
5. Description of dredge project parameters including design depth and perimeter, slope requirements, and disposal site
6. Description of all maintenance requirements including any protection/mitigation measures related to the current and future USACE dredge events. Would include dredging, pile replacement and general maintenance/repairs to marine structures, including electrical and plumbing repairs to be included in permit
7. Determination of Take (TOT) preparation per requirements of the California Department of Fish and Game to assess potential for project to adversely impact species listed by the State as endangered or threatened (i.e. longfin smelt)

Additional permitting requirements may arise depending on the recommendations of Federal and State resource agencies such as the California Department of Fish and Game (i.e. CDFG) and the National Marine Fisheries Service (NMFS). These recommendations are enforced by the USACE and/or RCDC and may include selfless surveys and consultation over impacts to essential fish habitat and/or endangered or threatened species.

According to available records from the RCDC and Alameda Marina, the only area of the Marina that has been historically permitted for dredging is the western portion. Since this is the same area currently impacted by shoaling, the process of acquiring a maintenance dredging permit for this location is likely to be a simpler process than trying to obtain a permit to dredge the entire Marina basin. In order to permit future dredging elsewhere within the Marina's basin, though, a more involved process will likely be required. If any of the proposed dredging activities are classified as "new work" the process would involve a new permit.

2.2 Sediment Quality Assessment

The most cost effective sediment disposal location for Alameda Marina dredged material is the in-Bay aquatic disposal site near Alcatraz Island (authorized as site number SF-11) in accordance with Federal and State regulations governing discharges to surface waters. Sediment samples representative of sediments proposed for dredging from the Marina will be collected and tested to determine whether this dredged material is suitable for disposal at SF-11. As described in the Sampling and Analysis Plan (SAP) approved by the Dredged Material Management Office in November 2010, Alameda Marina sediments will be tested for physical, chemical, and biological parameters in accordance with procedures recommended in Federal and regional sediment quality testing guidelines (USEPA/USACE 1998 and USACE 2001).

Results of chemical analyses performed with Marina project samples will be assessed and compared to the historical SF-11 database provided in PN 93-2 (USACE/USEPA 1993), ambient San Francisco Bay sediment concentrations (RWQCB 1996), and the San Francisco Bay Total Maximum Daily Load (TMDL) thresholds for in-Bay dredged material disposal (RWQCB 2009). Benthic toxicity results will also be compared to historical SF-11 database

values. Water column toxicity results will be evaluated against the limiting permissible concentration (LPC) as defined in the Federal guidance manual (USACE/USEPA, 1998).

If the results comparison assessment shows that the chemical contaminant levels observed with project sediments are consistent with or reduced relative to reference values, and significant biological effects are not detected, the material will be recommended as suitable for unconfined aquatic disposal (SUAD) at SF-11. In the event moderate levels of contamination are detected, confirmatory bioaccumulation potential (BP) testing may be performed to ensure undesired uptake into the tissues of organisms residing within the SF-11 disposal environment will not occur. If the project sediments cannot be classified as SUAD, additional testing, including leachate and elutriate analysis, will be performed for consideration of beneficial re-use or ocean disposal alternatives.

The Marina sampling scheme as described in the 2010 Sampling Analysis Plan includes vertical segmentation at two-foot increments of the sediment sample cores in case high-resolution analysis is necessary to delineate the extent of any potential contamination. Sampling and chemical analysis of the six-inch layer below the two-foot overdrudge allowance may also be necessary to assess the quality of the projected post-dredge surface sediments. A sufficient amount of material will be collected to allow for additional testing in case the initial physical, chemical and biological results dictate the need to consider an alternate disposal option. If BP testing, beneficial use testing, or any other analyses outside of standard requirements for SF-11 is required, a supplemental SAP will be prepared and submitted in the Dredge Materials Management Office (DMMO) for approval.

2.3 Confounding Factors

Federal Channel Dredging

The USACE and the U.S. Coast Guard are proceeding with plans to dredge the Federal navigation channel in 2011 from Coast Guard Island through Brooklyn Basin, to where the channel connects with the recently completed 40 Foot MLLW Federal Channel Deepening Project. The Federal channel limits proposed to be dredged includes (from west to east) the Grove Street Pier to Brooklyn Basin Channel, and most of the Brooklyn Basin South Channel (noted on NOAA Navigation Chart 18650).

Potential issues of concern to Alameda Marina and other marinas adjacent to the Federal channel:

The existence of contaminated sediments and the potential for them to mobilize and re-deposit into adjacent properties, including Alameda Marina.

The impact of dredging on piles that support docks and foals associated with adjacent properties, including Alameda Marina.

The impact of dredging near bulkheads where removing sediment from the toes may potentially destabilize the bulkheads, including Alameda Marina.

Measures that can be taken to address the above stated concerns can include the following:

1. Assess Current Conditions
 - a. Determine the USACE dredge schedule (currently planned for 2011).
 - b. Compare 2009 bathymetric survey to Federal channel configuration.
 - c. Using simple modeling tools, assess potential impacts of Federal channel dredging to Marina piles, side slopes, pile stability, transport of dredge material, etc.
 - d. Conduct a limited geotechnical investigation to determine soil strengths.
 - e. Conduct an assessment of the stability of existing piles and/or bulkheads near the Federal channel.
 - f. Visual inspection of the facility structures at low tide.
2. Determine the potential need to relocate susceptible piles, and design and install new pile material and sizes, pile guides, and connections to existing docks.
3. Determine the potential need to stabilize existing bulkheads.
4. Perform forensic comparison of Marina sediment quality testing results to Federal channel and U.S. Coast Guard sampling results to demonstrate potential contamination culpability.
5. Potential Protective/Mitigation Measures
 - a. Identify potential partners (e.g. other shoreline property owners and stakeholders including the City of Alameda).
 - b. Assess feasibility of replacing key piles with longer piles (deeper pile tip depths).
 - c. Engage the USACE independently or with partners regarding potential impacts to Marina. Discuss methodologies and feasibility of stabilizing or strengthening Federal channel side slopes (e.g. installation of subsurface sheet piles).
6. Monitoring During USACE Dredging
 - a. Monitor for scour and erosion of Marina sediments during dredging.
 - b. Monitor for total suspended sediments and contaminants adhered to suspended sediments.

Other Potential Sources of Contamination

Because the Alameda Marina is situated on a channel that has been historically used for several industrial and military operations, it is susceptible to contaminant sources unique to those installations. The Marina sediments are also more susceptible to higher contaminant

levels due to the period of time that has elapsed since the last dredge event. Contaminants that would likely be related to Marina operations include PAHs (possibly due to historic military land uses in the area, and from parking lot storm water run-off); heavy metals (also resulting from boat repair processes and storm water); and solvents contained in boat hull paint chips that have peeled off of boats in the Marina over the past several decades.

2.4 Future Potential Dredging Needs

Dredge Material Management Plan

A detailed dredged material management plan (DMMP) can optimize the economics and logistics associated with the Marina's maintenance and development objectives. The primary objective of a DMMP is to match a long-term dredging schedule with projected dredging needs. It also defines the processes needed to achieve the schedule, including the assessment of all potential and preferred disposal sites. A DMMP may also incorporate other items addressed in this section.

Master Sampling and Analysis Plan

A Master Sampling and Analysis Plan (MSAP) is intended to outline a long-term strategy for testing and regulatory review of sediment quality prior to each permitted dredge event. It would be applicable to all future sediment evaluations (unless significant changes are made to the regulatory guidance on which the MSAP procedures are based). A MSAP proposes sampling and analytical methods necessary for assessing sediment suitability for various disposal options available to the Alameda Marina following guidelines provided by the USACE and the US Environmental Protection Agency (USEPA). The MSAP will reduce overall dredging costs by streamlining the sampling and analysis process and eliminating the need for preparing an independent SAP for each dredge event. Once the MSAP is approved, then only a brief sediment evaluation proposal summary referencing the MSAP will be necessary for submittal to the DMMD before each future dredge event. These episodic sediment evaluation proposals will include a summary of any deviations from the MSAP and any updated data (e.g., continuity and sediment sample locations).

2.5 Dredging-Related Challenges and Opportunities

Consideration of current piling configurations will evaluate removing or replacing piles that are hazardous to vessel traffic or to expanding future Marina usage.

Graving Dock

A very small area at the entrance to the former graving dock is the only portion located within the leasehold area B in the eastern portion of the Marina. Any future plans to enhance the graving dock area will include consideration for this portion of the leasehold area.

2.6 Dredged Material Disposal/Placement Alternatives

The most cost-effective dredged material disposal option for the Alameda Marina is the SF-11 aquatic disposal site near Alcatraz Island. Since the Marina meets the small dredger classification as defined by the DMMO, they are exempted from the need to consider other disposal options unless the material proposed for dredging is contaminated. However, aquatic disposal is most sensitive to prohibition due to contaminated sediments. If the levels of contamination in the Marina exceed what is considered SUAD, then other disposal/reuse options must be considered.

Ocean Disposal

The San Francisco Deep Ocean Disposal Site (SFDODS) is a U.S. EPA authorized aquatic disposal site 60 miles west of the Golden Gate. Approval for use is less sensitive to sediment quality testing results, but significantly more expensive, and requires specialized dredge vessels (e.g. ocean-going vessels) that are, at times, less available than vessels used for conventional disposal sites.

Beneficial Use Options

There are several non-aquatic options for disposing dredged material in the Bay Area. Each one exhibits unique aspects associated with sediment quality sensitivity and logistics. These options include levee improvements in locations like the Winter Island Reclamation District, agricultural use at the Carneros River Ranch, and various habitat restoration projects. If managed properly, placement of dredged material at these sites does not significantly affect the aquatic environment and therefore the sediment quality acceptance criteria are not as restrictive as the aquatic disposal options. Due to the more complex measures necessary to transport material from the dredge scows to the beneficial use sites, though, costs can be significantly higher on a per cubic yard (CY) basis.

Montezuma Wetlands

The Montezuma Wetlands Project (MWP) is a privately owned wetlands restoration project located in Suisun Bay. This potential beneficial use option is listed separately due to its capacity to accept both clean and moderately contaminated material. The MWP is authorized to accept material more contaminated than what can be accepted at other beneficial use sites, using such material for wetland foundation construction. However, in order to accept contaminated material there must be a simultaneously available (within weeks) equal volume of clean material either from the same site or from a different dredge site.

Berth 10 and Landfill




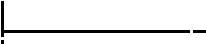



If the Alameda Marina dredged material exhibits contaminant concentrations at levels higher than acceptable at MWP or other reuse options, the only other immediately viable alternative is landfill disposal. In order for material to be taken to landfill, it must first be dried in a temporary storage site, otherwise known as a re-handling facility. The only re-handling facility available to

private dredgers in the San Francisco Bay is the permitted Berth 10 location at the Port of Oakland. The current estimate of capacity at Berth 10 for a single re-handling event is 4,000 CY. Use includes placement of dredged material within the site, space assignment (lease) of site, loading and transport to ultimate destination. Enclosed dredged material will require re-handling by exposing wet material to drying conditions (sun/dry). Re-handled material will then be stockpiled when it has reached state of approximately 80% solids. There may be a need for additional post-dredge testing and monitoring for regulatory compliance. Separate arrangements may be necessary for security of equipment remaining on site during non-working hours/days. In addition, material must be relatively free of debris. Once the dredged material has dried to 80% solids, it can be hauled to the nearest landfill. The extensive re-handling and additional landfill fees make this option significantly more expensive than all other options.

2.7 Cost Options for 2011 Dredging Plans

The overall cost of dredging the area identified in the SAP will be dependent on the level of sediment contamination, availability of an appropriate dredged material disposal or reuse site, and availability of dredge contractors possessing the equipment necessary for the disposal/placement site. If contamination is present, the SAP has been written to allow for both vertical and horizontal spatial delineation of any contaminant, to potentially minimize the volume of material that would need to be disposed of using a costlier option. The following table provides estimates and level of sediment quality restrictiveness for the disposal options described above. The costs do not include potential ancillary needs such as dredge operations team preparation, dredge and post-dredge monitoring, construction management and project mobilization and demobilization costs.

Table 1. Approximate costs per cubic yard and level of restrictiveness for each sediment disposal option.

DISPOSAL OPTION	RESTRICTIVENESS	APPROXIMATE COST/CY
In-Bay/Alameda		\$12 - \$20
Quinn		\$30 - \$45
Montezuma Sliver		\$15 - \$40
Carmichael River Ranch		\$40 - \$15
Green Island		\$40 - \$60
Montezuma Main Sliver		\$10 - \$60
Berth 10		\$70 - \$90

3.0 Alameda Marina- Inventory and Condition Survey of Existing Facilities

The following sections include an inventory summary of the existing facilities at the Alameda Marina. In addition to the results of the conditions survey, comments received from various Marina managers/employees have been included for a comprehensive assessment of the present conditions. The Alameda Marina facilities that were surveyed and the proximity of the neighboring/ adjacent properties are illustrated in Figure 1 (Existing Site Map, contained in Appendix A).

This report excludes commentary on Fee Simple property, shore-side utilities, buildings, or the boat elevator. This report also excludes commentary on paving of the shore-side, shoreline slopes and bulkheads.

3.1 Overview of Existing Facilities

Alameda Marina has received several updates to the facility since it first opened. Some of the updates have included reconstruction / repair work on selected bulk walls and docks, dredging the marina basin in 1957, replacement of deteriorated piles, and regular maintenance of docks, shore-side slope, water supply, and electrical supply.

The shoreline is split between leased property and fee simple property. Property west of Dock 2 is leased property under Lease A while the East Dock and entrance to the Graving Dock shoreline is leased under Lease B. Shoreline property between lease areas A and B and including the majority of the Graving Dock are fee simple property. There is approximately 1,355 feet of shoreline in Lease A property and 1,703 feet of shoreline in Lease B property. There is 700 feet of shoreline in fee simple property.

3.2 Shoreline Condition

The shoreline banks are stabilized by a combination of sloped embankments and bulkheads. The following photos and descriptions provide an overview of the various types of shoreline slope and bulkhead protection that exist at Alameda Marina and the general condition of each type. This report is based on a site evaluation by Sea Engineering, discussions with Alameda Marina construction staff, current and prior surveys by PLS Surveyors, review of the *Chronology of Past Improvements*, exhibit data from Clauser and Associates, and permit history. Recommendations for potential improvements over the next 15 years can be found in Section 4.

Overall Summary of Shoreline Condition Findings:

The rip-rap sloped banks are in good condition.

Bulkheads generally are in fair condition. We recommend repairs to some bulkheads, however, it will be impractical to repair all bulkheads. Some bulkheads will remain in their present-day condition as rusted and out of plumb. Furthermore, there are several inaccessible shore line bulkheads which have not been surveyed or evaluated to date (e.g., low profile bulkheads in the outer Graving Dock area).

Shoreline Type Breakdown:

Shoreline types that have been reviewed and evaluated include the following:

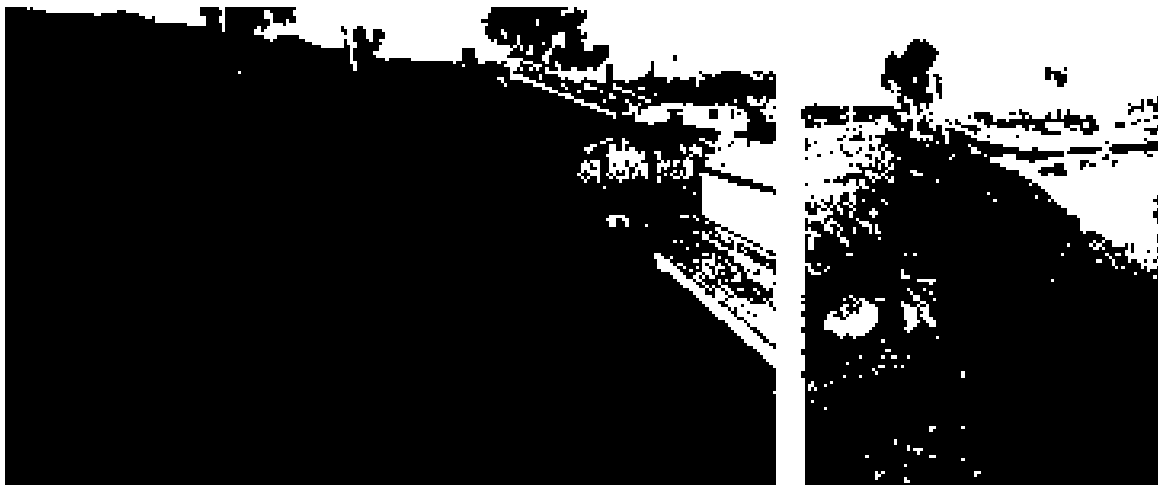
Rip-Rap embankments	265 linear feet
Steel Bulkhead	630 linear feet
Concrete bulkheads	1100 linear feet
Wood bulkheads	405 linear feet
Concrete / Wood bulkhead	250 linear feet
Steel Piers	10 linear feet
<u>Bulkhead behind piers</u>	<u>500 linear feet</u>
TOTAL BULKHEADS	2640 linear feet

A written summary of the shoreline condition of each type follows and includes a description of the condition for each section down to 0 feet MLLW, as well as representative images. Figures in Appendix A show the shoreline bulk head types and locations.

Rip-Rap Slopes:

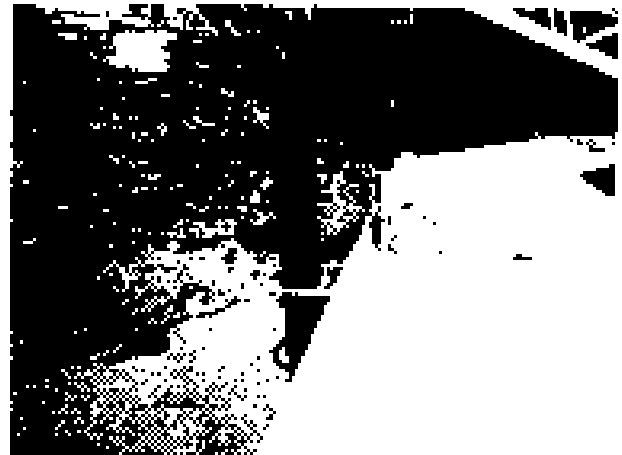
The shoreline of the Marina is protected by 850 linear feet of rip-rap stone rubble armor stone revetment wall partially covered with plant and fill material. Figures in Appendix A delineate shoreline protected with rip-rap slopes.

Section 1: Sloped shoreline from Docks 10 to 7 (340'):



The slopes along this section are composed of fill dirt with concrete and rock debris (rip-rap). The slopes are generally in good condition and are preventing the shoreline from eroding. No repair is recommended at this time.

Section 2: Sloped shoreline between docks 3 and 2 (118’):

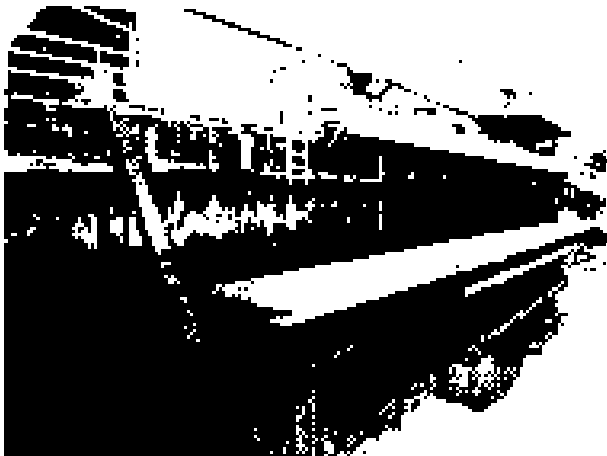


The slopes along this section consist of concrete debris visible to an elevation of 0 MLLW. Exposed fill soil is visible throughout. This section of slope is in poor condition but is not in imminent danger of failing. No additional work is recommended unless future sloughing of material occurs.

Steel Bulkheads:

The shoreline of the Marina is protected by 630 linear feet of steel sheetpile bulkhead walls that include, in most cases, two horizontal wood walers connected to tiebacks. Figures in Appendix A delineate shoreline protected with steel bulkheads.

Section 1: Bulkhead between docks 7 and 6 (100’ long):



The 13 foot high steel bulkhead (measured from existing sea level to top of bulkhead) is in fair

condition and appears to be generally plumb. The bulkhead consists of sheet pile walls with two horizontal wood walers that are likely connected to tiebacks. The sheet pile walls were originally 3/8" thick, but have deteriorated over time to approximately 3/16" - 1/4" wall thickness. The sheet piles are considered in serviceable condition, though, as there are no obvious failures or deflections.

The lower wood walers are deteriorated beyond use and are recommended for replacement. The upper wood walers are in serviceable condition and do not need to be replaced. It is assumed the seabeam tiebacks are in serviceable condition, but they are inaccessible to survey. A recommended retrofit would be to replace the lower walers and connect the replaced walers to the existing tiebacks.

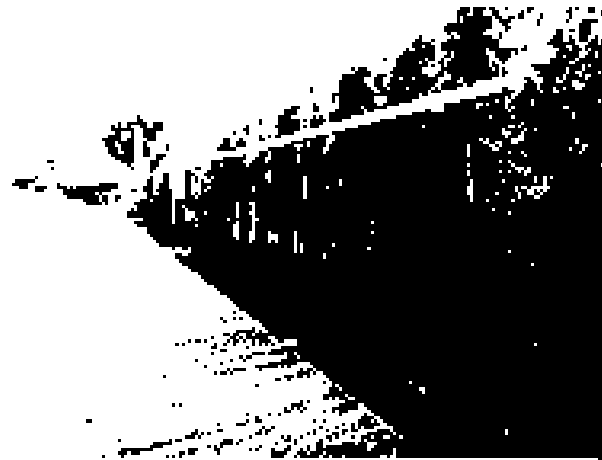
Section 2: Bulkheads between docks 6 and 5 (215')



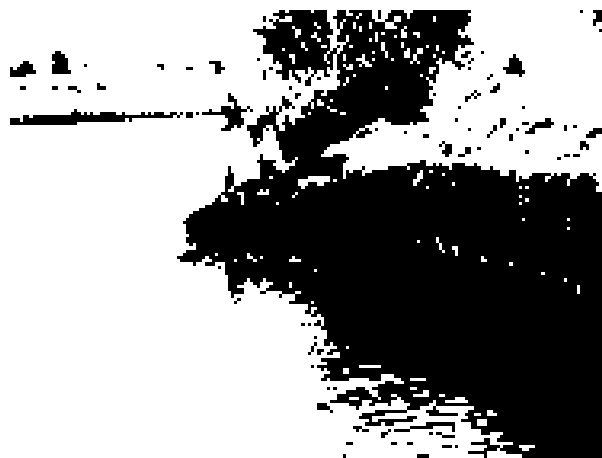
The 13.5 foot high steel/wood Bulkhead is in fair to poor condition. Presently, it averages being 0' out of plumb, and appears to be moving under the influence of active pressure (where the top of the wall is moving toward the Estuary due to soil pressure behind the wall). Though the steel appears to have rusted, there is more than 1/4" thickness of steel remaining which helps to maintain structural integrity.

There previously existed a weaker bulkhead on the estuary side of the steel bulkhead that has rotted, failed, and is resting in the tidal zone. There is a concrete shelf at -1' foot MLLW that supported the old wood bulkhead. There is a wood waler in good condition at the top of the existing bulkhead that has tiebacks at 9 foot spacing. The wood waler was connected to tiebacks via wooden piles that have since been cut off, allowing the piles to rotate and the bulkhead to deflect at the top.

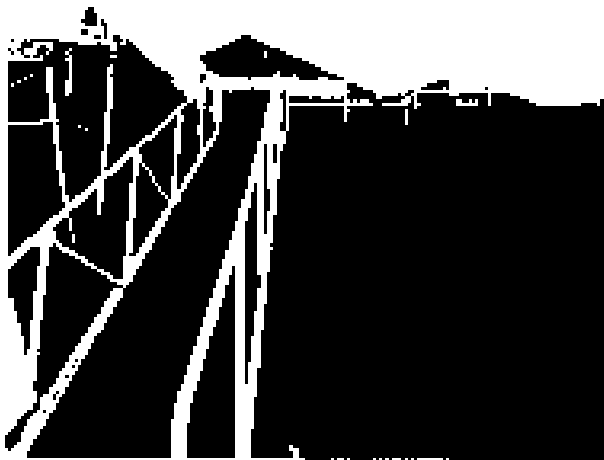
The top of the existing bulkhead is not straight, and leaning towards the estuary, indicating that it might be in a failure mode. The top hull sill has the remains of ten wood piles attached from the previous bulkhead. A recommended retrofit would be to replace the tieback connectors. The fact that the top of the wall is not straight suggests that future monitoring of this wall is recommended. There is no practical way to repair the wall without replacing it.

Section 3: Bulkhead at East docks: (165')

The 20 foot high steel bulkhead (measured from sediment elevation to +7 feet MLW) was inspected, and is in fair to good condition; however, it is out of plumb up to 3/4" per foot with the base moving toward the Estuary (active pressure). Though the steel appears to be rusting, almost all of the original 1/2" thickness remains. There appears to be closely spaced steel tiebacks connected directly to the sheet piling 5 feet from the top of wall, negating the need for walers. No repairs are recommended. In addition, no future dredging is recommended near the bulkhead as over-dredging in the past may potentially have contributed to wall movement near the base, and may continue to adversely affect the wall stability.

Section 4: Bulkhead at Graving Basin Shoreline (120')

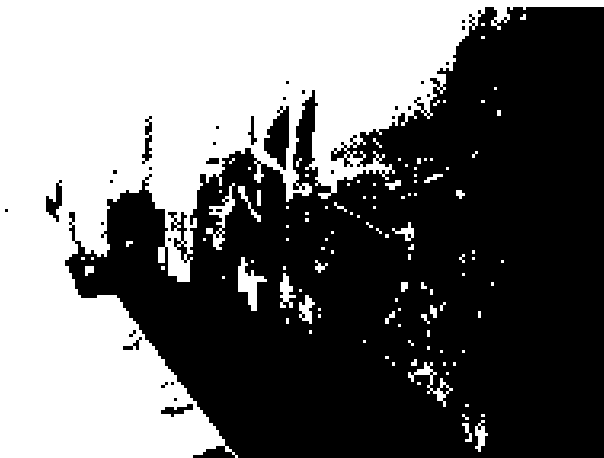
This 15 foot high steel bulkhead (measured from sediment elevation to top of wall) is in poor condition. The bulkhead has failed in one location, and no repairs are recommended as the use of the property behind the bulkhead should be cordoned off, allowing the bulkhead to continue to fail.

Section 5: Bulkhead between docks 7 and 6 (30'):

The 11 foot steel bulkhead (backs) were surveyed, and they appear to have failed several years ago, according to conversations with Alameda Marina staff. The top of the bulkhead has deflected approximately 3 feet towards the basin (active pressure) and has been mitigated by adding three steel batter piles. At the present, the wall appears stable. No additional work is recommended.

Concrete Bulkheads:

The shoreline of the Marina is protected by 1100 linear feet of concrete bulkhead walls with deep 20' X 30' cantilevered piles spaced at 6' which are in good condition. A combination of concrete or steel lagging rests between the concrete piles. Figures 10 Appendix A delineate shoreline protected with concrete bulkheads.

Section 1: Bulkhead at Graving Basin (1030'):

The 12 foot high concrete bulkhead (measured from sediment elevation to top of bulkhead) was surveyed. The bulkhead is in good condition and is plumb. One concern is that it does not extend to a low enough elevation and sediment erosion is occurring at the bulkhead toe. The lagging between concrete piles is in fair condition, but shows signs of failure. In addition, it is only 10" high and flush with the top of the piles, leaving a 2' void at the bottom of the wall. The fill materials behind the wall have sloughed downward creating a sloped surface at the toe of the wall, and subsidence is occurring at the top of the wall.

The subsided area has been cordoned off, no repairs are recommended unless the subsidence issue is addressed. The cost to resolve this situation may be prohibitive. As a result of our evaluation, we recommend the future addition of fill at the top of the wall and for the area to remain cordoned off. No further dredging is recommended near these bulkheads as it may result in further subsidence as near-bottom material is removed.

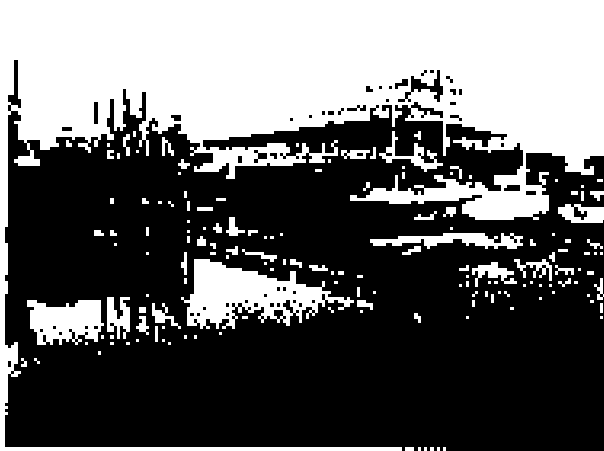
Section 2: Bulkhead at East docks (65'):



The 18 foot high concrete bulkhead (measured from sediment elevation to top of bulkhead) has failed in two locations. The bulkhead is built with 15' cantilevered octagonal piles spaced at 6'. The concrete lagging is in good condition. One recommended retrofit is to remove and replace the filling wall, which will require removal and replacement of gangway support structures as well.

Wood Bulkheads:

The shoreline of the Marina is protected by 400 linear feet of wood bulkhead walls with a walrus at the top that ties the piles together. No bulkheads exist at figures in Appendix A. A alternate shoreline protected with wooden bulkheads.

Section 1: Bulkhead between docks 4 and 3 (100' perpendicular to the shore):

The 9 – 11 foot high palisade pile wood bulkhead (measured from sediment elevation to top of bulkhead) has been reported by Alameda staff to be in poor condition. The base of this wall is along the beach, which has a 2 degree slope along the length of the wall. The bulkhead has a water at the top that ties the palisade piles together, but no tiebacks.

Heavy utilities are suspended from the bulkhead (e.g. water, electrical, fire). There are no signs of surface deterioration and no localized failures or signs that the wall is out of plumb. No rehabilitation is recommended until localized failures occur. One area of concern that should be monitored, however, is the subsidence of the ground above the wall. Future monitoring will forecast potential impending maintenance issues.

Section 2: Bulkhead between docks 4 and 3 (95' parallel to shoreline):

The 4 foot palisade pile wood bulkhead (measured from sediment elevation to top of bulkhead) is also reported to be in good condition. The base of this wall is along the beach which has a 2 degree slope along the length of the wall. The general condition description is the same as

above (Wood Bulkheads - Section 1)

Concrete/Wood Bulkheads:

The shoreline of the Marina is protected by 200 linear feet of concrete/wood bulkhead walls that are anchored with 16" square piles. The piles are spaced at 6' and have a misted water system 4' below the top of the wall. The piles are in good condition. Figures in Appendix A delineate shoreline protected with concrete/wood bulkheads.

Section 1: Bulkhead at East docks (118')

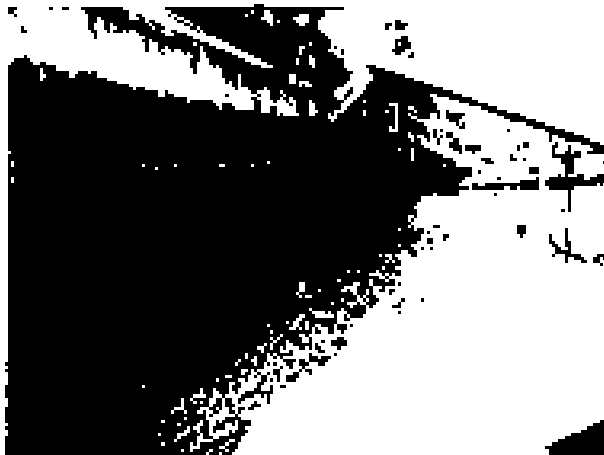


The lower elevation of the 12 -18 foot high concrete/wood bulkhead (measured from sediment elevation to top of bulkhead) is between -6' MLLW and 0' MLLW. The bulkhead was built with 16" octagonal piles that are in good condition and spaced at 6'. Shore-side of the bulkhead there exists a 4' high 1:1 granite slope that is in good condition. The top section of the bulkhead consists of heavy timber lagging that is in good condition. The bottom section of the bulkhead consists of concrete lagging that is in fair condition. The overall concrete/wood bulkhead is in good condition, but the steel water is in poor condition. The West portion of the bulkhead is up to 1/2" per foot out of plumb while the remainder of the bulkhead appears plumb. It is recommended that the steel water be replaced.

Bulkheads behind Wood Piers:

The shoreline of the Marina is protected by 500 linear feet of bulkhead walls built behind wood piers. Figures in Appendix A delineate shoreline protected with bulkheads behind wood piers.

Section 1: Bulkhead at Deck 6 (Old Pier 6) (35'):



The 25 foot wide wood pier (Old Pier 6) is in good condition with enough piles wrapped to support the deck. The steel bulkhead is in poor condition and has areas where it has failed.

Section 2: Bulkhead at Old Pier 4 (40'):



The 30 foot wide wood pier (Old Pier 4) is in poor condition. At this time an adequate number of wood piles are wrapped to support the deck. The 10' high steel bulkhead has been recently repaired, and no further repairs are needed as the pier is not used much.

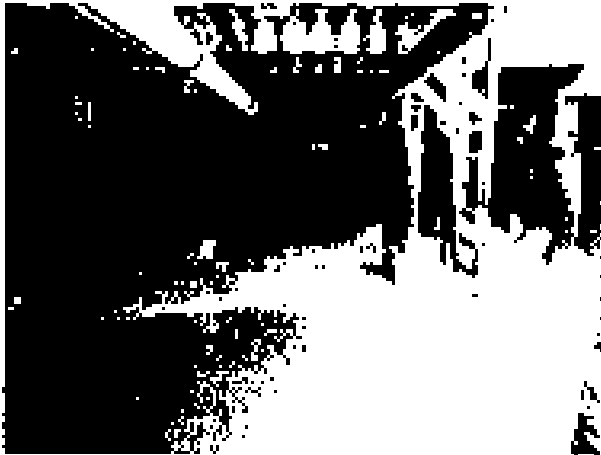
Section 3: Bulkhead between docks 4 and 3 (20'):

The 36 foot wide wood pier is in good condition. The bulkhead has not been surveyed as it is only 4' high.

Section 4: Bulkhead between docks 4 and 3 (36'):

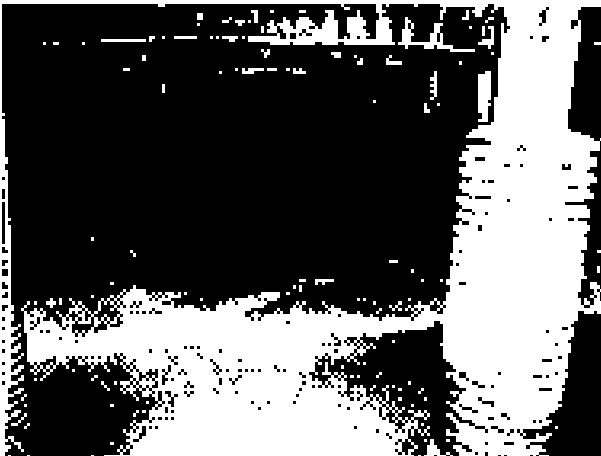
The 65 foot wide wood pier supports a two-story wood building (Building 14). No maintenance issues or significant deterioration of the pier or bulkhead has been reported to date. A detailed survey of the pier below the building and the bulkhead shore-side of the building should be conducted. Maintenance work should be considered low priority unless the proposed detailed survey identifies new concerns.

Section 5: Bulkhead between docks 1 and 2 - Old Pier 3 (44')



The 150 foot wide wood Pier (Old Pier 3) is in good condition. A 5 foot high concrete sheepie bulkhead, shore-side of the pier, appears in serviceable condition. The foundation has been repaired and reinforced. No further work is recommended.

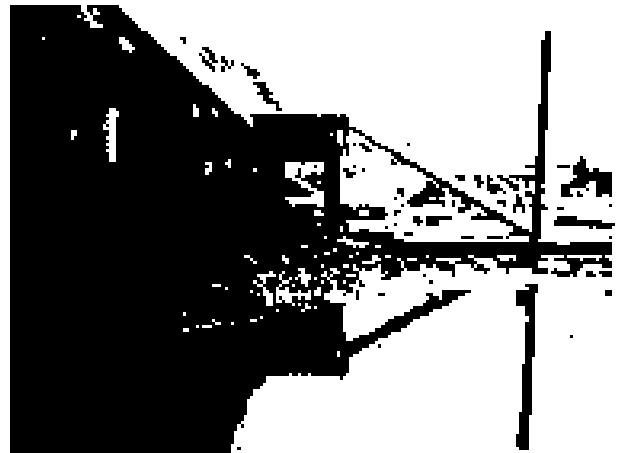
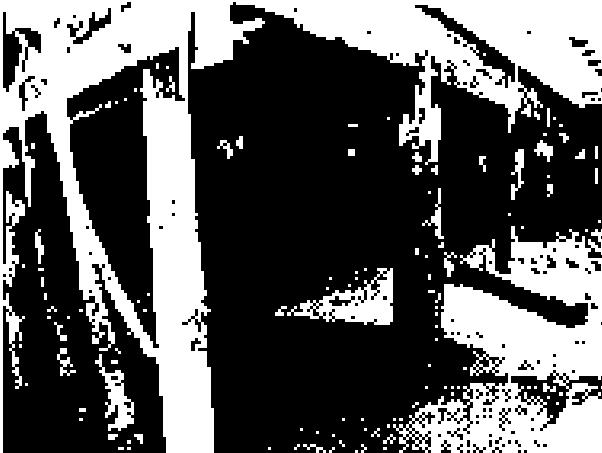
Section 6: Bulkhead adjacent to dock 3 (22')



The 45 foot wide wood pier shore-side of the travel lift is in fair to good condition. The travel lift is in good condition. No further work is recommended.

Section 7: Bulkhead shore-side of dock 2 (53'):

The 40 foot-average width wood pier supports a single story wood building (Building 13). Pier 13 was reported to be built in 1980 and has a 4 foot high concrete retaining wall on the shore side. The slope below is stabilized by a slurry seal concrete fill. No further work is recommended.

Section 8: Bulkhead shore-side of dock 2 (40'):

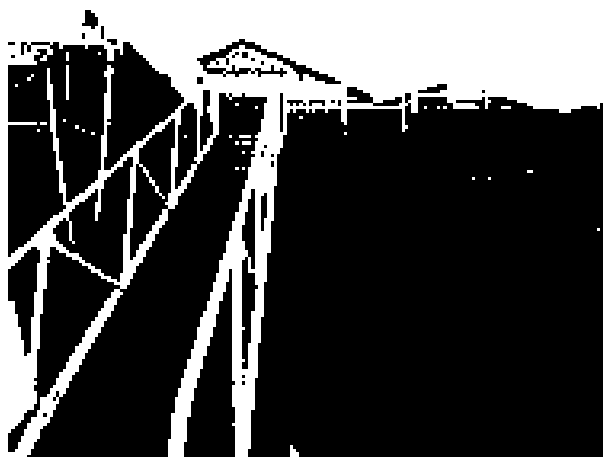
The 57 foot wide wood pier is reported to be built in 1980 and has a 4 foot high concrete retaining wall on the shore side. The slope below is stabilized by a slurry seal concrete fill. No further work is needed.

Section 9: Bulkhead adjacent to dock 1 - south (225'):

The 57 foot wide wood pier is in good condition, but the piles are generally in poor condition. Total area of the pier is 12,800 SF. The 11 foot high wood bulkhead shore side of the pier has failed in isolated locations. One recommendation would be to remove the pier and relocate it to cover a portion of the Graving Basin. Once the pier is removed, the bulkhead could be repaired or replaced. Bulkhead repair work should be considered of high priority and pier replacement should be considered a low priority. However, it will be difficult to replace the bulkhead without first removing the pier.

Steel Pier:

The shoreline of the Marina is protected by 10 linear feet of steel pile pier. The figure in Appendix A shows areas in the marina with Steel Pier.

Section 1: Bulkhead adjacent to dock 6 (15'):

Steel pier is in good condition. An unprotected sloped bank lies below the steel deck.

3.3 Floating Dock Condition

This summary of findings on the condition of the floating docks in the marina is a result of site observations, communication with Alameda Marina staff and observations reported by Clausen and Associates (2009). The floating dock condition report includes a summary of the present-day conditions of each section, as well as representative images.

In this report, a "berth" is defined as a boat docking location with a dock finger on each side. An "end tie" is defined as a boat docking location with a floating dock on one side only. When calculating marina aerial footage, the areas of berths and end ties are combined.

The basin is split between leased property and fee simple property. The westerly leased area is identified as lease area "A" while the easterly leased area is labeled as lease area "B".

Net basin areas are as follows:

Useable leased area "A"	400,000 SF	Un-useable leased area	50,000 SF
Useable leased area "B"	25,500 SF		
Useable quaying dock area	25,000 SF	Un-useable quaying dock area	5,000 SF
TOTAL USEABLE AREA	450,500 SF	TOTAL UNUSEABLE AREA	55,000 SF

Berths in each area is as follows:

Leased Area A	
30' and smaller Berths	152 Berths
32' and larger Berths	147 Berths
Leased Area B	
32' and larger Berths	18 Berths
Total Berths	317 Berths

This floating dock condition summary reflects site observations of the existing marina property mentioned below:

Structural condition of marina:

- Condition of guide piles
- Condition of water piping
- Fine nose boxes
- Condition of electrical
- Condition of other utilities
- Condition of covered docks
- Gangways and gangway support structures

Summary:

The Marina is separated into 12 main headwalk sections. Each headwalk section has been identified by its pier identity and does not include the old ship pier areas adjacent to the shoreline.

Table 2. Pier Identities, Characteristics and Evaluations.

<u>IDENTITY</u> <u>(Leased Area)</u>	<u>LENGTH</u>	<u>AREA</u>	<u>WATER</u> <u>DEPTH</u>	<u>PIER</u> <u>CONDITION</u>
Graving Dock Floats	360 LF	1,970 SF	Varies	Good
Fast Docks	750 LF	7,100 SF	Good	Good
Pier 2B	1,170 LF	9,270 SF	Acceptable	Good/Marginal
Fuel Dock	300 LF	1,800 SF	Acceptable	Acceptable
Pier 3	1,950 LF	14,810 SF	Acceptable	Marginal
Pier 4	1,950 LF	12,000 SF	Good	Marginal
Pier 5	790 LF	5,910 SF	Good	Marginal
Pier 6	1,950 LF	11,780 SF	Good	Marginal
Pier 7	1,970 LF	9,960 SF	Marginal	Acceptable
Pier 8	1,160 LF	6,750 SF	Marginal	Acceptable
Pier 9	790 LF	5,910 SF	Acceptable	Acceptable
Pier 10	470 LF	2,910 SF	Acceptable	Acceptable
TOTAL	12,000 LF	89,170 SF		

Floating Dock Structural Condition Summary For Lease Areas A & B:

Figures of the Dock configuration can be found in Appendix A.

The floating docks and piers are built with stick framed douglas fir lumber. Docks are floated on expanded polystyrene foam billets covered with vinyl which are commonly used in the marina industry. Stick frame construction is a dated method of building docks and is considered below.

the standard of industry for modern dock construction. However, stick frame dock construction is still found in many marinas.

The floating docks vary in their present-day condition, as identified in the table above. This rating is based on age of the structure and the SEI and Clausen and Assoc. (2009) condition survey. Primary problems appear when the foam vinyl covering tears, exposing the foam to moisture and marine growth. Dry rot has been caused where the dock fascia traps dirt and fresh water between the framing. Decking dry rot is common at the hose bbs as well. The docks are generally serviceable, but are in poor condition.

Piers 2 - 10



Docks are generally in serviceable, but in poor condition.

Graving Docks and East Docks



The East docks and graving docks are generally in good condition.

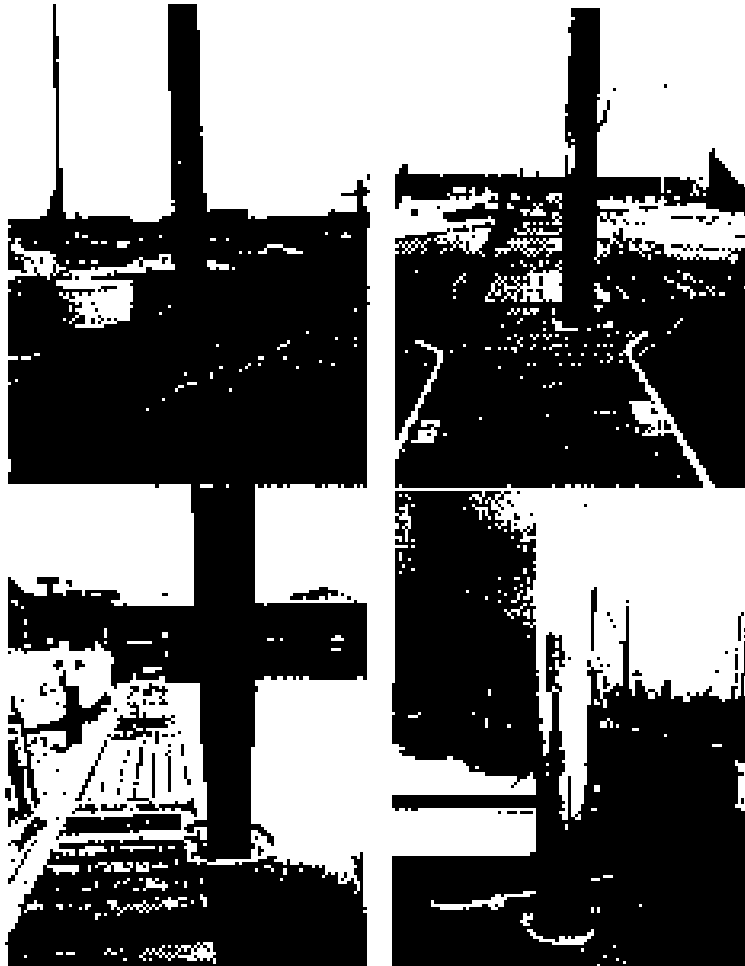
Floating Covered Docks Structures



Some berths in the Marina are "covered berths". The floating dock covers are built with both wood and aluminum. Roof structures are built with untreated stick framing and UC2A treated douglas fir lumber. Covered docks are decked primarily with untreated 2" x 12 douglas fir. The roofing of the structures is constructed of aluminum material.

The floating covered docks are in good condition overall with the exception of the aluminum roofing which has deteriorated over time due to exposure from the elements. It is recommended that the roofing be replaced in the future.

Guida Piles



In general, the marina docks were not originally designed with a sufficient number of piles when initially constructed. There are no codes for marina piling, but the piles are spaced further apart than current industry standards which may cause higher stresses on the dock elements. Prior to piles being replaced or reconfigured, an evaluation of current pile condition and placement would be conducted. This would occur during the planning/design phase and prior to permitting.

The marina docks are anchored by Class "A" 14" creosote wood piles, Class "B" 12" creosote wood piles, and 12" square concrete piles. Some wood piles have been encased with a concrete shell. Observations were made of each existing pile at low tide, with many of the wood piles displaying signs of aging. Many have lost much of their structural integrity due to marine borer damage. It is recommended that these piles be replaced or removed.

Based on our observations, and the Clouser data we recommend that an evaluation for potential replacement be made of 43 piles identified as marginal in the table below. In general,

we recommend that existing piles remain in their present locations unless marina replacement evaluation dictates change.

Existing Pile Condition Survey Summary:

East and Graving Dock

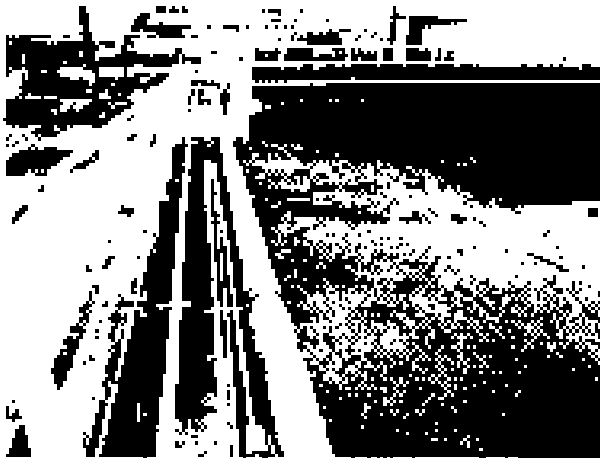
41 piles acceptable

Pier 2-10

Table 3. Floating dock anchor piles and present-day condition evaluation.

Pier	No. Piles	Marginal Piles
2	22	7
3	13	0
4	17	7
5	6	2
6	15	1
7	8	1
8	6	1
9	7	2
10	5	1

Water Piping Systems



The existing water pipelines on the docks appear to be in serviceable condition. These pipes will require continued maintenance because minor breakages are common occurrences according to Alameda Marina staff. The present system is undersized when compared to that

required a new marina. Codes published since the marina was constructed require increased water pipe sizing, 3" in this marina, for fire suppression.

Modern marinas would employ current fire department connections at the top of each gangway. Updating the Marina water system would require improvements to the shore-side water supply systems. However, Alameda Marina recently passed, without any alterations, an inspection conducted by the local fire department. Therefore, the existing suppression system is deemed sufficient for current marina use.

Fire hose boxes and fire extinguishers



All fire systems on the existing docks are wet systems that are shared with the domestic water supply. In general, existing water piping is 1 1/2" pipe, and is led with 2" pipes on the shore side. There are fire department connections on the shore-side of the fire protection systems. These systems have 1 1/2" standpipe valves on the docks spaced at regular intervals. There are no fire hoses at any of these standpipes, however, fire hoses are supplied at dry hose boxes on each dock section. We recommend that a wet box is added at each standpipe. A wet box is a cabinet that contains a fire hose that is a ready attached to the standpipe.

Electrical Systems



The electrical systems in the marina are outdated and have, in general, utilized the "serviceability" Many receptacles are straight pronged receptacles allowing users to connect ungrounded devices. Straight pronged receptacles should not be allowed on docks in any marina. The ampacity ratings are below modern industry standards. Cables that have been purchased since the marina was constructed require larger conductors and more capacity of the system. The electrical systems, at a minimum, require significant maintenance and observation to continue serviceability. The electrical systems should be replaced in the future, which will require reconstruction of the shore-side electrical systems.

Electrical General Notes Piers 2 - 10

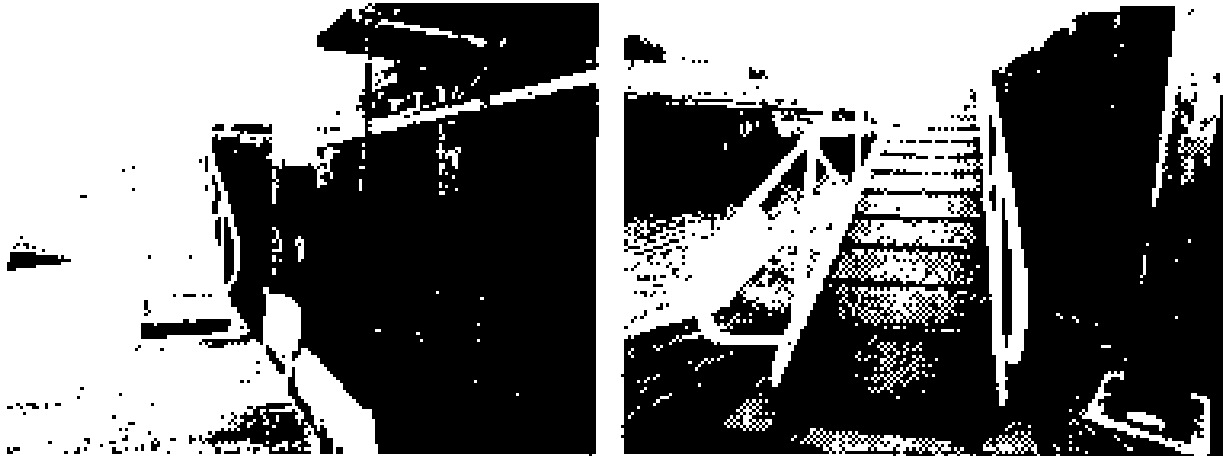
All conduit and junction pull boxes are Nema 4 schedule 40 PVC. Generally, main conductor runs are spliced in under the deck in 5' x 6' x 6" PVC Conlon JIP boxes. Most all conductors are 4-#6 copper conductors for 120 volt services (with two #10 copper conductors for lights). Grounding conductors are #4 non-insulated stranded copper conductors. Lighting controls are on photo voltaic switches at the head of each inside gangway pier.

Main conduit and junction pull boxes are undersized for retrofitting. At a minimum, all straight prong receptacles should be replaced with either 20 amp 120 volt twist lock receptacles on berths 26 and shorter and minimum 30 amp twist lock 30 amp 120 volt breaker controlled receptacles on berths 28' to 40' in length. The marina management should consider providing one 30 amp twist lock 120 volt receptacle and one 50 amp twist lock 208 volt receptacle on berths 42 and up, especially at end lies.

East Docks and Graving Docks

Electrical on those docks appear to be up to Code requirements (see Clausen Engineers, Inc. (2009)

Existing Gangways:



All the gangways in the marina are shorter than current industry standards. Modern gangways for marinas such as this one are typically at least 30 feet long (or longer) to make slopes tolerable at low tide. In a newly design marina at least one ramp would be 30' long to meet current Federal ADA and State Department of Waterways requirements; however, a very low percentage of older marinas actually comply with current ADA regulations. Adapting older marinas to current ADA regulations is cost prohibitive. This has resulted in marinas with older design standards, such as Alameda Marina being grandfathered and not required to meet current ADA regulations. Gangway design should be considered with the future maintenance work. In general, the gangway support structures are serviceable.

4.0 Alameda Marina- Reconstruction Phasing Recommendations

Phasing Recommendations

Alameda Marina reconstruction will occur in multi-year project phases over a 15 year period of time.

Six phases are recommended, and are detailed below along with the anticipated budgets of each. Sea Engineering is recommending a fifteen (15) year reconstruction phasing program for the Alameda Marina. The Marina Reconstruction Site Plan and detailed Construction Cost Estimates will need to be developed as each phase is implemented.

The budget estimates for the recommended project phases include:

- Engineering, design, bidding and construction management
- Construction contingency, and
- Added contingencies for smaller, multi-year phased projects.

The budget estimates are calculated in 2015 dollars. As various project phases are considered in future years, Sea Engineering recommends a re-examination of the budget estimates as each new project phase is considered.

Project Reconstruction Phasing Priorities and Costs

Phase 1 – Dredging and Pile Repairs and Upgrades

Significant portions of the submerged leasehold at the west end are unusable due to shoaling and deadhead piles. The Marina has not been dredged (knowingly) since 1967 and was constructed around the deadhead piles. It is recommended that the western area of the Marina be dredged in the immediate future to alleviate vessel navigation concerns. In addition, the deadhead pile area may, if deemed necessary, be evaluated at some point in the future. Currently this area does not pose a hazard to either navigation or marina operations as currently configured.

Dredging and pile driving will require permitting from 11 different Federal and State agencies with an approximate timeline of up to 1 year for permit processing. Upon completion of the maintenance dredging a new baseline depth for the entire marina will be conclusively established using approved surveying techniques. **The total budget for phase 1 maintenance dredging is estimated to be \$395,000.**

Phase 2 – Pile Evaluation and Replacement

The number of floating dock piles anchoring the docks is marginal and is below industry standards. In addition, several of the existing piles are in a deteriorated state. It is recommended that an evaluation be conducted to determine which existing piles should be

replaced or repaired. Second, it is recommended that an evaluation be conducted to determine whether additional piles be installed to assist with dock anchorage, bringing the Marina more in line with industry standards. **The total budget for phase 2 is estimated to be \$100,000.**

Phase 3 – Fire / Water Supply Systems Upgrades

The utilities running 50 feet from and parallel to the shoreline in the central area of the property found most to be in poor condition. These 40-year old shore-side utilities include electrical systems, water and fire water systems, telephone lines, fire alarm systems, storm sewers, and sanitary sewers.

Localized repairs have been made to these utilities on an as needed basis, but most of this infrastructure has served its lifetime, and is in need of replacement and modernization. **The total budget for phase 3 is estimated to be \$255,000.**

Phase 4 – Dock and Facility Electrical Systems Upgrades

The recommended repairs in the West end utilities are all serviced from Building 22. The electrical system is in need of replacement and its budget to be \$75,000. The items which need to be addressed are as follows: panels and transformers in Building 22, wrong in size conduit junction box at Pier 10 and upgrading/replacement of ampacity of shoreside electrical service serving Pier 5.

The recommended repairs in the Central end utilities are all serviced from Building 12. The electrical system is in need of replacement with a budget of \$145,000. The items which need to be addressed are as follows: replacement of undersized wires, undersized conduits and outdated pull boxes. Upgrading and/or replacement of ampacity of shoreside electrical service serving Pier 4 and 5 are recommended.

The total budget for phase 4 is estimated to be \$220,000.

Phase 5 – Dock Electrical System Maintenance

The current electrical system in place for the docks is significantly outdated and beyond its useful lifespan. Many of the receptacles are ungrounded straight pronged plugs which are no longer allowed by code. The ampereage ratios are well below the industry standards for newer marinas. The electrical systems should be replaced in the future, which will also require reconstruction of the shore-side electrical systems previously described. **The total budget for phase 5 is estimated to be \$115,000.**

Phase 6 – Shoreline Repairs to Leasehold Areas

The Marina shoreline is stabilized by a combination of sloped embankments and bulkheads comprised of steel, concrete and/or timber. In general, the sloped embankments are in good condition and the bulkheads are in fair condition. Certain bulkhead locations have failed or are in a state of failure. It is recommended that the most imminent failures be rehabilitated, as it is impractical to replace all failing bulkheads. Bulkheads that are in state of failure but have not lost structural integrity need no replacement.

Bulkhead repairs are recommended in Leasehold "A" where steel bulkheads are stable, but the supporting walers have the potential for failure. Specifically, it is recommended that 310' of waler replacement be implemented. In addition, a 25' section of the old Pier 6 bulkhead should be repaired. **The total budget for phase 6 is estimated to be \$380,000.**

Table 4. Project Phase Summary.

Phase	Project	Estimated Budget
Phase 1	Dredging Maintenance	\$395,000
Phase 2	Pile Evaluation and Replacement	\$100,000
Phase 3	Fire/Water Supply Systems Upgrades	\$256,000
Phase 4	Dock and Facility Electrical Systems Upgrades	\$220,000
Phase 5	Dock Electrical System Maintenance	\$115,000
Phase 6	Shoreline Repairs to Leasehold Areas	\$380,000
Total Estimated Reconstruction Budget		\$1,465,000

Final Design Requirements

When the marina proceeds with the final design of each reconstruction phase this will include detailed surveying and/or inspections of existing features, completing final design engineering, and preparing plans and specifications for the bidding process. The preliminary design intentions included within this Study, were based on available information and conditions at the time of the Study. Existing conditions may change and/or worsen before each project phase is implemented. Therefore, it will be required to reevaluate conditions prior to final design of each reconstruction phase.

Permit Requirements

The completion of the improvements proposed for the Alameda Marina must be constructed in accordance with all applicable permits (i.e. USACE Permit, BCDC, City of Alameda, etc.).

Depending upon the scope of individual reconstruction phases, the following is a list of potential permits which may be required:

- An Alameda County Soil Erosion and Sedimentation Control Permit when improvements involving ground disturbance are processed.
- A Water System Construction Permit may be required for water system distribution improvements.
- An Electrical System Construction Permit may be required for electrical system distribution improvements.
- USACE, EPA, RWQCB, and BCDC permits may be required for dredging, pile replacement, or shoreline improvement projects.

Conclusion

It is anticipated that fire shuttling, shoreline, floating docks and utility projects identified above will be completed within the first 15 years of the new lease term. The implementation of marina improvement projects is based upon available cash flow from operations of the marina and the economic feasibility of each construction/repair and improvement task. None of the identified projects are "revenue producing" in that they will not increase revenues but rather preserve the existing marina for continued use. The total budget for these identified projects is \$1,465,000.

5.0 References

1. *Shore Protection Manual* 1984 U.S. Army Waterways Experiment Station, Coastal Engineering Research Center
2. California Code of Regulations Title 24 Part 9 California Fire Code 2010
3. NFPA 303, Fire Protection Standard for Marinas and Boatyards, 2011 Edition
4. NFPA 70 National Electrical Code (NEC), 2011 Edition
5. ALTA Survey by PLS Surveyors
6. Chronology of Past Improvements
7. *Classen and Assoc*
8. Interviews with Auneda Marina staff and engineers

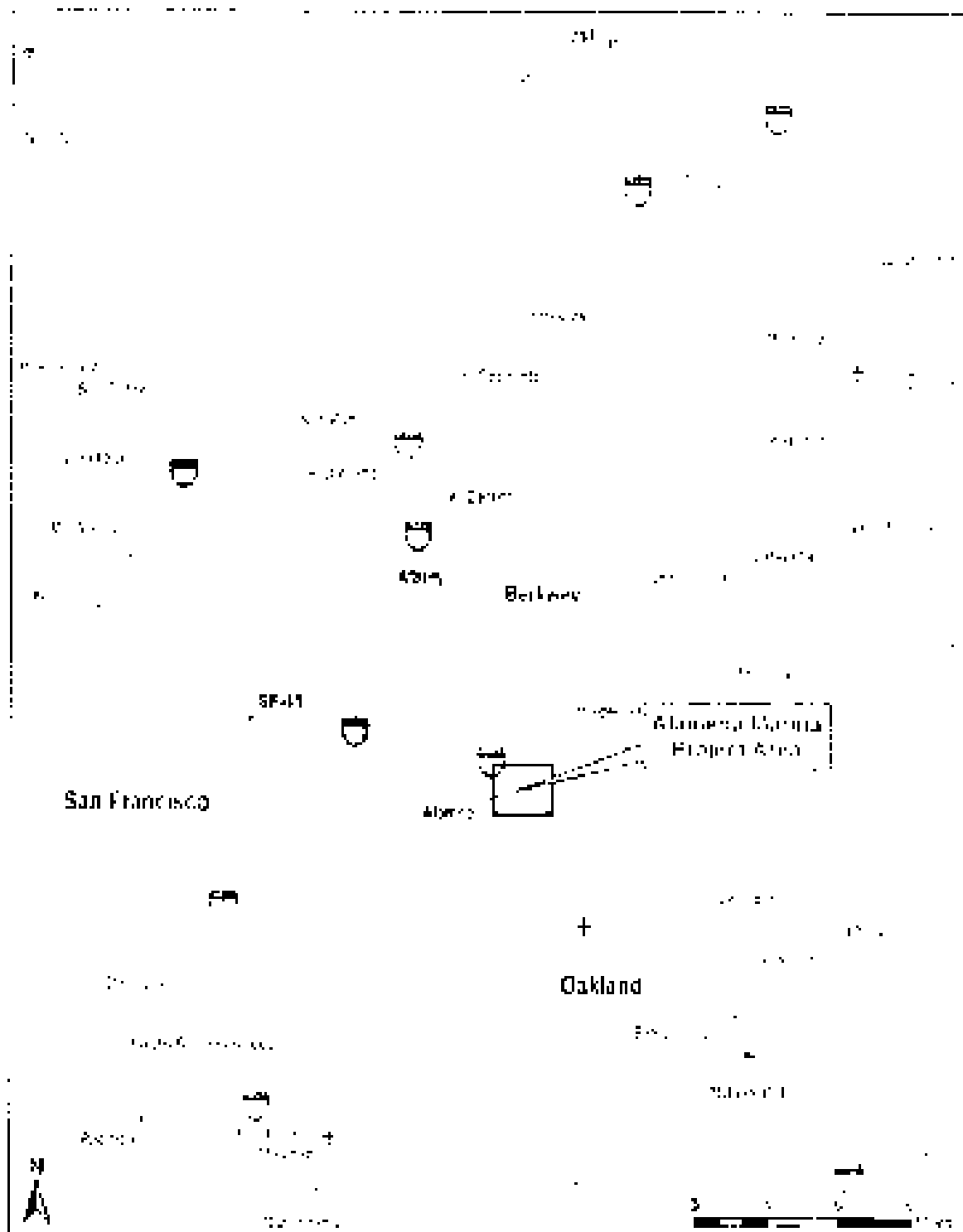


Figure 1. Vicinity Map – Alameda Marina and SF-11 (figure from Alameda Marina SAP, 2010).



Figure 2. Project Location - Alameda Marina (figure from Alameda Marina SAP, 2010)

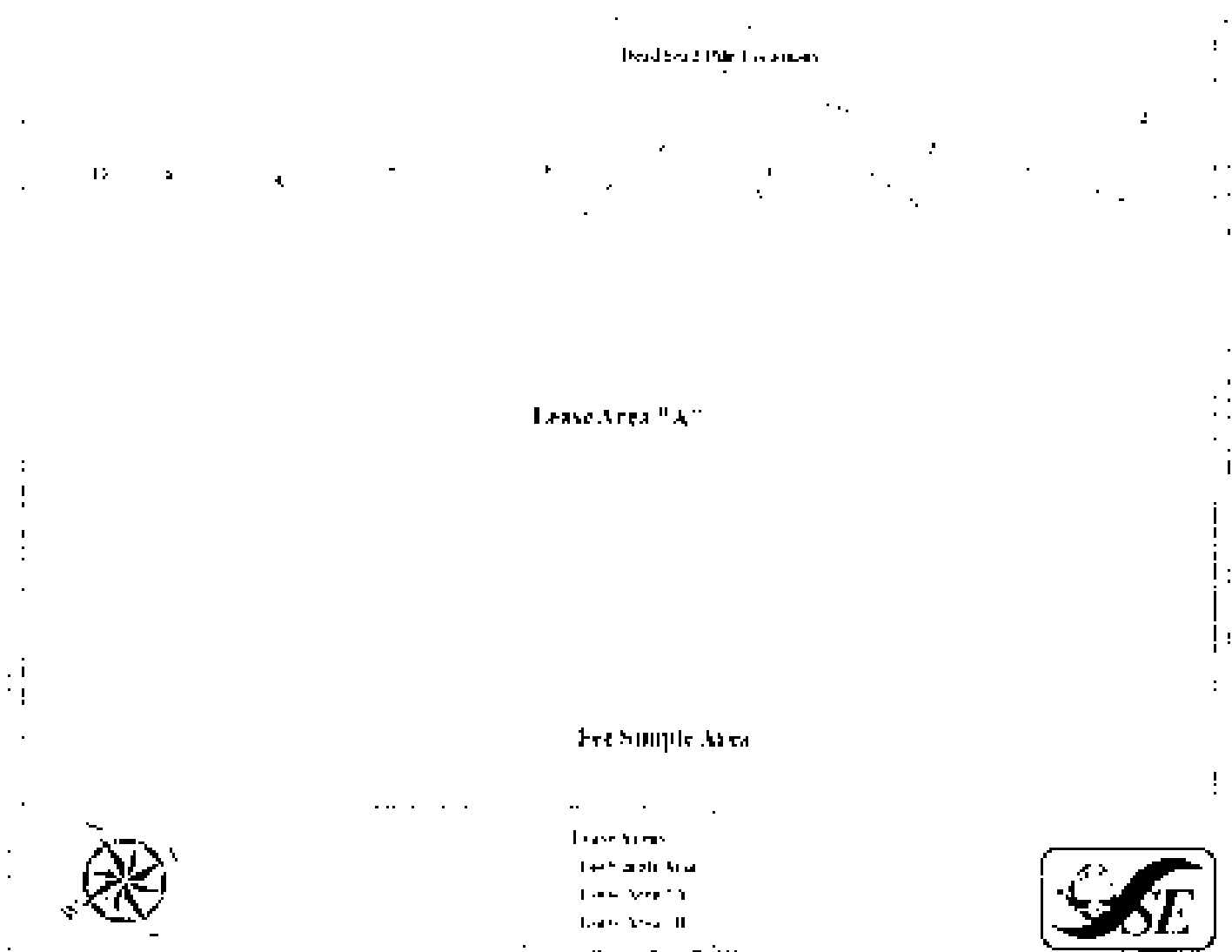


Figure 4. Delineation of Lease Area A, dock identification, and location of deadhead piles.

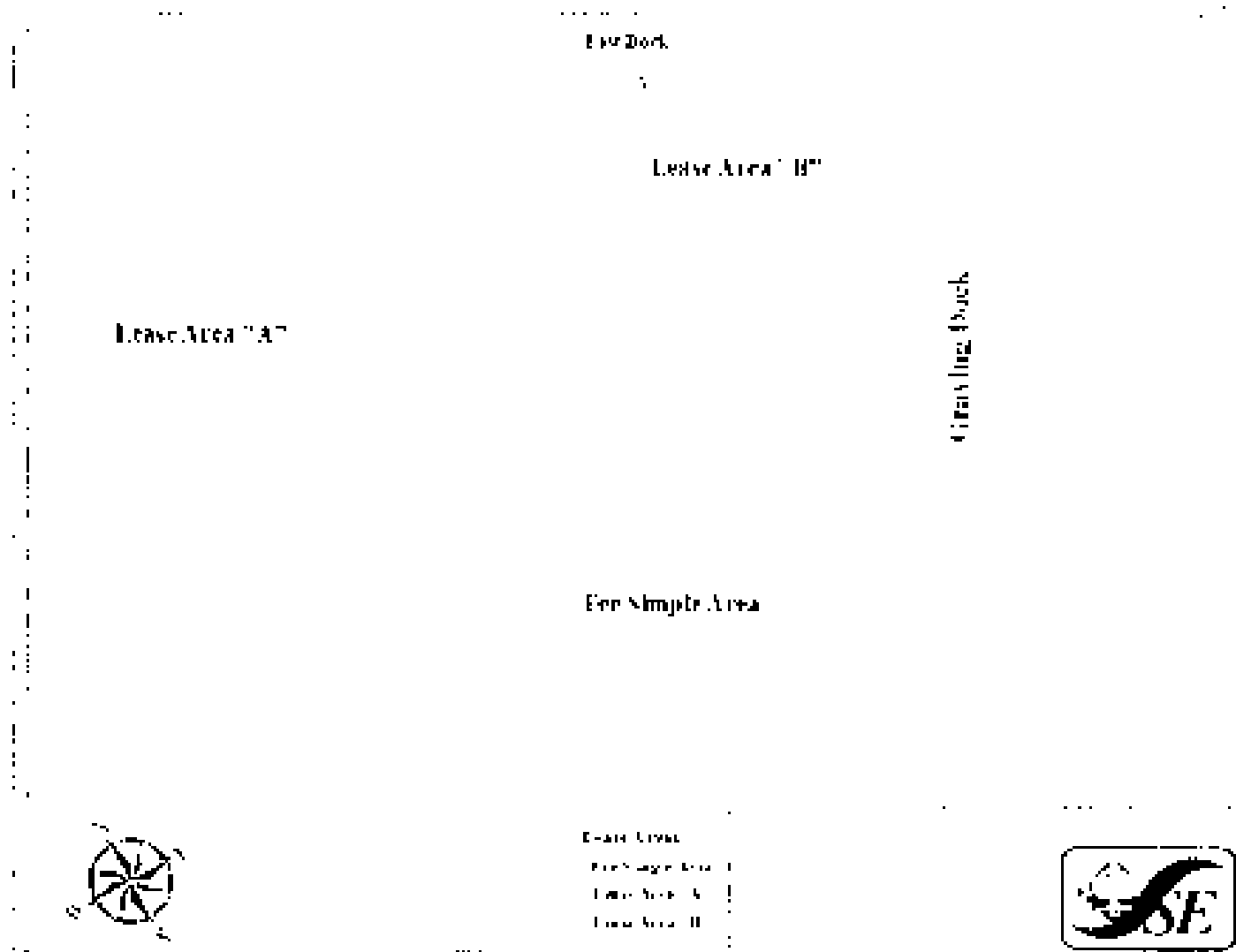


Figure 5. Delineation of Lease Area B, dock identification, and location of Graving dock area.

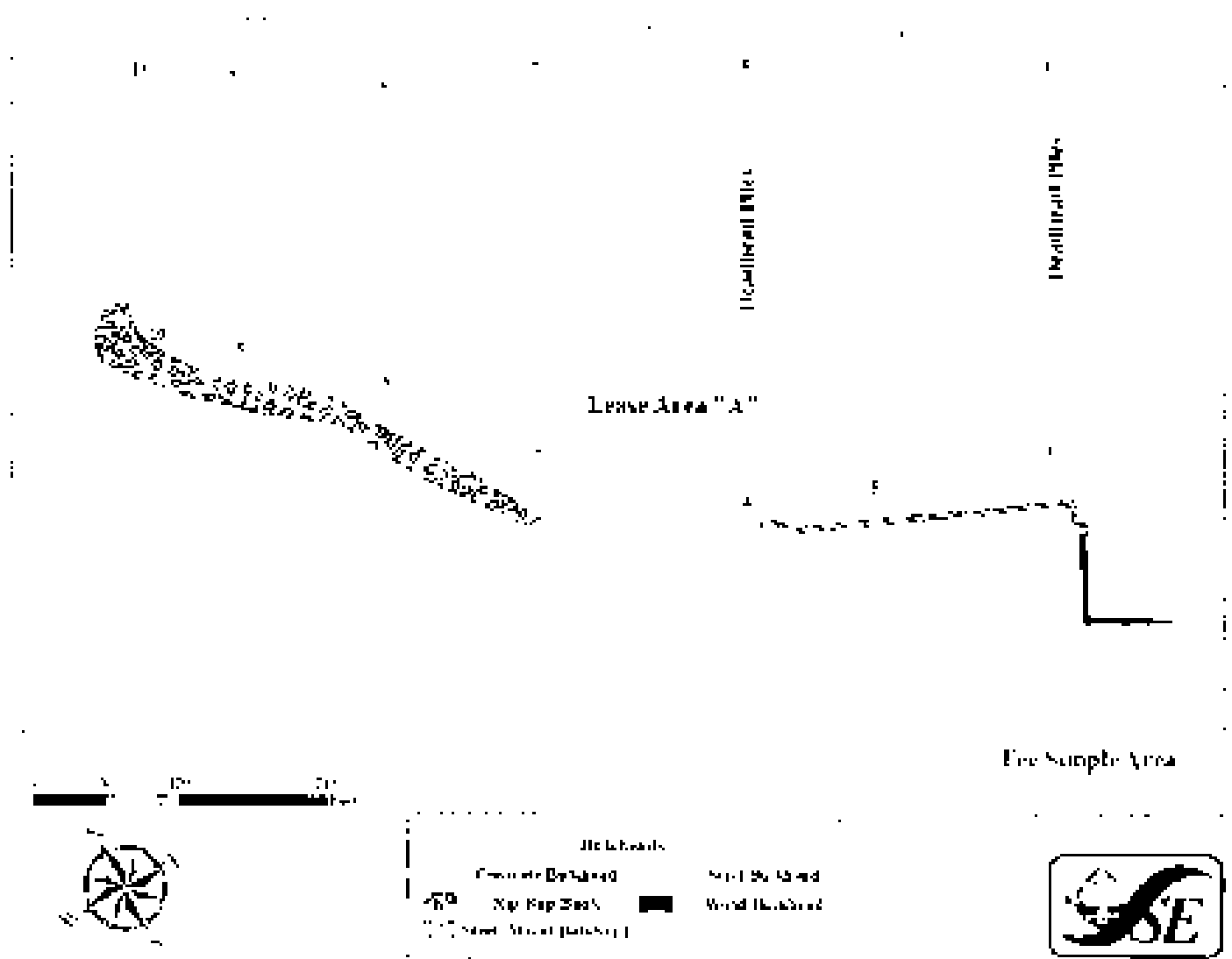


Figure 6. Western section of Alameda marina showing types of shoreline bulkheads identified.

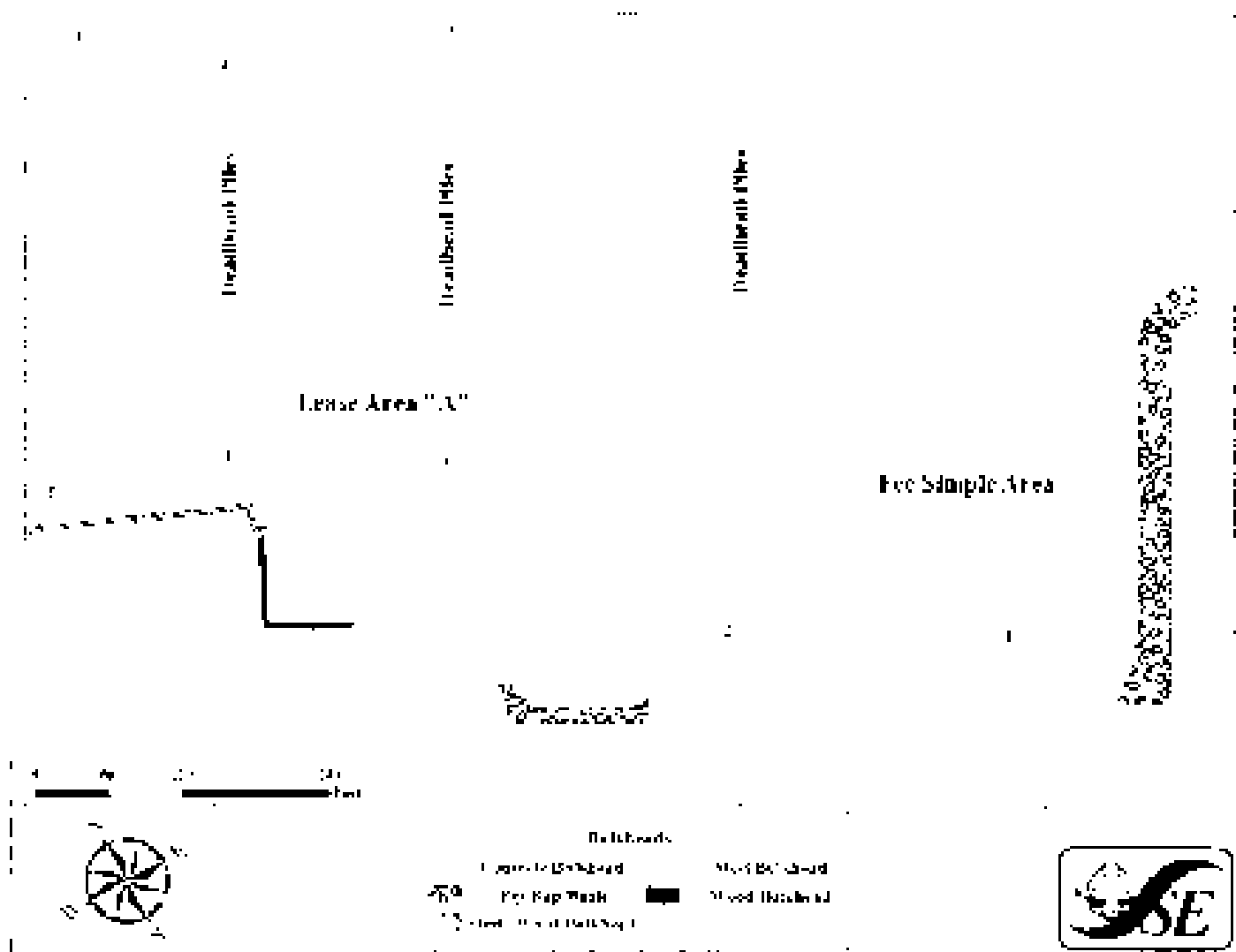


Figure 7. Central section of Alameda marina showing types of shoreline bulkheads identified.

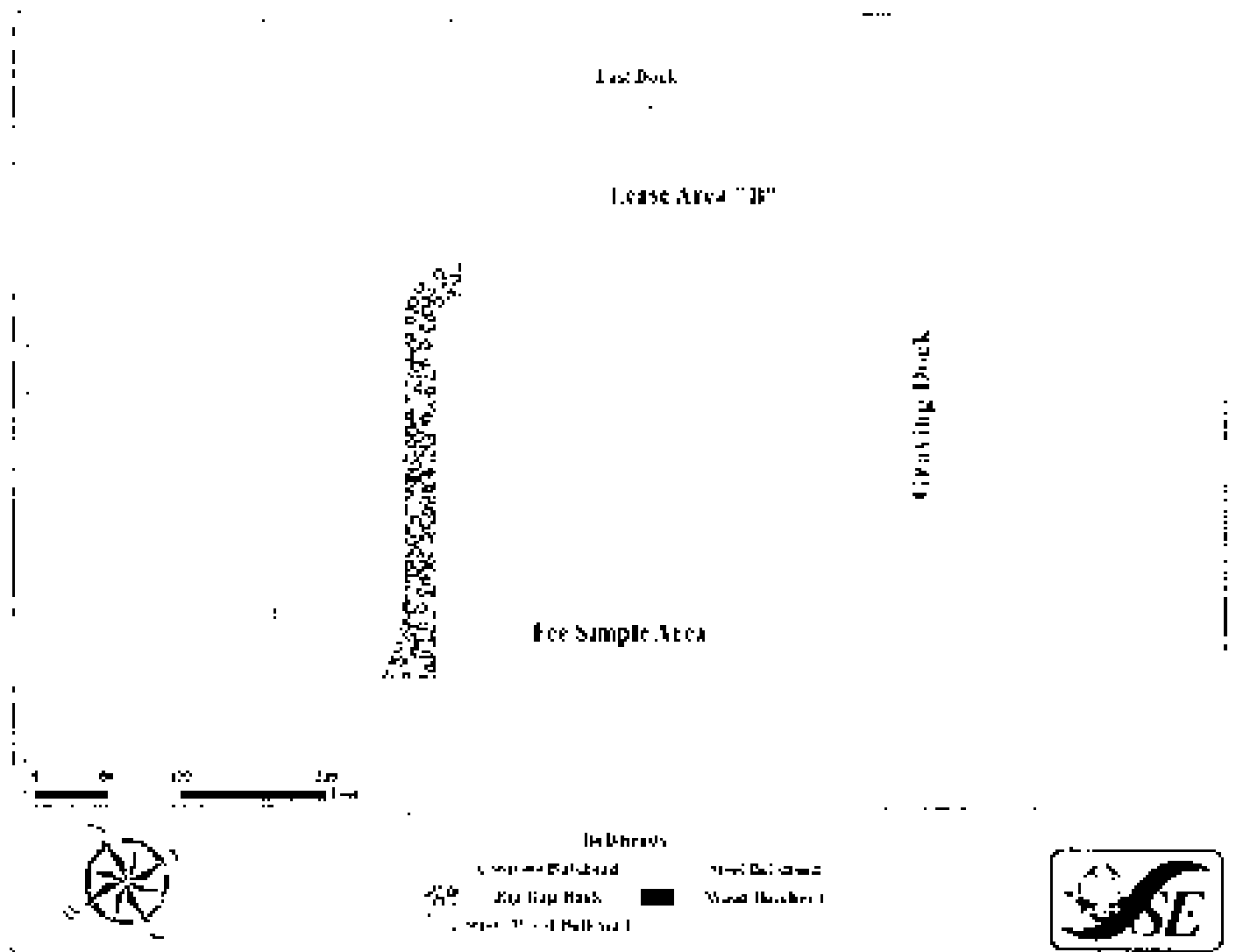


Figure 8. Eastern section of Alameda marina showing types of shoreline bulkheads identified.



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March 15, 2011

Dorine E. Soto
Economic Development
Manager, Business Development Division
City of Alameda
2263 Santa Clara Avenue, Room 120
Alameda, California 94501-7552

Subject: Peer Review: Alameda Marina - Preliminary Engineering Report

Dear Ms. Soto:

WESTON is pleased to present this peer review of the Alameda Marina - Preliminary Engineering Report, dated December 2010 (Report) to the City of Alameda, as Task 1 of the Consultant Agreement with the City, dated February 23, 2011. I organized into the following sections, consistent with the Report:

- Executive Summary
- Background
- Dredging Background
- Alameda Marina - Inventory and Condition Survey of Existing Conditions
- Alameda Marina - Reconstruction Phasing Recommendations
- Conclusion
- References

In general, the Report is well written, accurate, concise, and comprehensive. I suspect that the preponderance of my comments included in the peer review were outside the original scope of work for the Report and are included as supplemental information provided to Alameda Marina (Marina) and City of Alameda on the current condition and potential of the Marina. I added *emphasis* to existing Report language as appropriate. I also inserted recommendations as and where appropriate.

The Report appears to be limited to the lease-hold portion of the Marina, with the exception of the discussion on berths. Recommend that the scope of work for the Report be expanded to include both the lease-hold and fee simple areas, together with upland areas directly related to the operation of the harbor area. Marinas are most efficiently and effectively managed, operated and maintained as an integrated system.



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Executive Summary

The City of Alameda (City) is negotiating with Alameda Marina (Marina) for a long term lease relating to property that the Marina currently operates as a recreational marina, together with ancillary activities. The City is particularly interested in the current condition of the Marina; required and discretionary repairs; safety; potential reconfiguration; and upgrades. The aforementioned features, once constructed and maintained, will result in an environmentally and economically sustainable enterprise providing valuable revenue and services to the Marina owner operators, City, and associated tenants.

The Report accurately described the Marina's location on the north side of the City of Alameda, California. The Report accurately describes the history and construction of the Marina, including improvements.

The Report accurately described anticipated dredging requirements, permitting, sampling and analysis. I included minor recommendations and revisions to include the need for recent surveys and importance of verifying required depths. I also included an estimated duration of three months (90 days) for the sampling and analysis process (SAP-SAR-DMMO approval). I expanded the discussion of concerns related to maintenance dredging of the adjacent Federal channel and United States Coast Guard (USCG) berths, and evolving issues related to resource and regulatory agency concerns, including essential fish habitat. These resource and regulatory agency concerns have resulted in additional expense for maintenance dredging throughout the San Francisco Bay Area.

I added a short discussion including challenges (problems) and opportunities related to the former graving dock.

Finally, I added **resource indicators for reconfiguration based on recent experience at Oakland Yacht Club.**

The Report generally provided an accurate overview of existing facilities, including construction history, upgrades, repairs, and maintenance. The report also described the difference and delineation between the leased property and fee simple property. I added recommendations related to more detailed surveys and structural analysis in certain areas. This information is important to accurately describe needed repairs, establish priorities, detailed scopes of work, develop cost estimates, and reduce / control change orders by providing detailed information to contractor(s).

The Report generally provided an accurate overview of existing conditions of docks, ramps, guide piles, and infrastructure. I added additional detail with respect to the latest technologies related to stick frame construction; and included additional indicators that may limit adequate servicing to berth tenants. I also added recommendations for infrastructure repairs / upgrades and harbor reconfiguration based on recent experience at Oakland Yacht Club.



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The number and size of berths is an important indicator of the Marina's ability to provide service to recreational boaters and potential for revenue. Available area is another indicator of the Marina's ability to generate revenue by taking advantage of lease-hold and fee simple property resources. The executive summary should state that there is a significant potential for increased berthing as well as the potential to decrease vacancy (increase occupancy) by constructing additional berths and widening furrows within the Marina now limited by submerged piling. Another potential mechanism to decrease vacancy (increase occupancy and resulting revenues) is to better service the existing existing recreational boating demographics through harbor reconfiguration. Reconfiguration by converting smaller berths into larger berths would increase occupancy revenues, as demonstrated by Oakland Yacht Club's recent experience.

The report comprehensively described a 15 year plan incorporating six multi-year project phases, including cost estimates. I recommended combining certain phases to achieve economies of scale and provided revised cost estimates generally based on recent experience with dredging projects within the San Francisco Bay Area and Oakland Yacht Club's harbor reconfiguration.

Background

Alameda Marina previously arranged for professional services resulting in the document entitled "Alameda Marina – Preliminary Engineering Report" dated December 2010. WESTON previously contributed to the dredging portion of the report focusing on sediment characterization. I did not have an opportunity to review the report before receiving it as an e-mail attachment on February 14, 2011.

The Report included a brief history of the Marina, dredging background and anticipated needs, permitting, sediment quality assessment, associated confounding factors, challenges and opportunities, dredged material disposal/re-use alternatives, estimated costs, inventory and condition of existing facilities, shoreline structures, floating docks, infrastructure, construction phasing, and other recommendations.

I conducted a peer review of the document entitled "Alameda Marina – Preliminary Engineering Report" dated December 2010 (Report). The peer review consisted of a thorough review of the Report, and verification of factual data therein. The peer review also included an evaluation for technical merit and completeness.

As requested, the peer review extended beyond the scope of the document to include issues such as navigational and tenant safety, climate change (sea level rise), operations, and maintenance. All of this was oriented towards a complete assessment of the Marina in terms of value (current and future). Value is further defined as opportunities for revenue, servicing of the boating public and related demographics, cost of construction, operation, maintenance, value added, and return on investment. In addition to a comprehensive review of the document and appendices, the peer review included an office visit to Alameda Marina to examine project documents and a site visit. The latter involved a minimal effort since I am already familiar with the Marina due to previous visits.



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The Report accurately described the location, environs, history and construction of the Marina, including improvements. The Report accurately described the Marina's location on the north side of the City of Alameda, California. It can be further described as located on the south side of the Brooklyn Basin South Channel, south of Coast Guard Island, as depicted on National Oceanic and Atmospheric Administration (NOAA) Navigation Chart No. 18650, entitled San Francisco Bay, Candlestick Point to Angel Island.

Sediment stabilization and scouring at the west end of the Marina may have likely resulted from the berthing of a larger class of Coast Guard vessel and attendant vessel assist vessels.

It is important to note the area of the harbor that is currently unusable is due to over 1000 submerged piles (not deadhead piles). Deadheads are generally defined (USCG usage; The Hull Truth Boating Forum) as floating piles that represent a hazard to navigation. The unusable area of the harbor due to fixed submerged piles is approximately 54,110 square feet (sf). Recommend that the aforementioned submerged pilings be removed, rather than broken off at the mud line, as is often the practice, to provide more room to construct berths. An order-of-magnitude cost estimate for the removal of the pilings together with subsequent disposal is \$500,000.

The Report accurately described current recreational boating demographics. These indicate that there is decreasing demand for vessel berths 30 ft. or less. The current demand, reflected in boat sales and marina waiting lists, is for boats in the 30-40 foot range.

The Report accurately described the need for careful planning for reconfiguration. This can be accomplished through harbor reconfiguration phasing and sequencing, as was recently done at Oakland Yacht Club.

Oakland Yacht Club recently reconfigured almost 50 % its harbor to decrease the number of smaller (vacant) berths into a fewer number of larger (occupied) berths. Although this resulted in a fewer number of berths within OYC's harbor, revenue was significantly increased since the vacancy rate went from 20% to 2% within a few days (by accommodating boats from OYC's waiting list). Moreover, revenue increased since tenants who own larger boats pay a higher rate than smaller boats. The estimated proforma cost of reconfiguring 25 28' berths to 13 40' berths (including the movement of docks to increase fairway widths) and installation of additional piles (due to increased loads) is \$100,000.

Another benefit of reconfiguration, reflecting Oakland Yacht Club's recent experience is to reduce the number of abandoned vessels occupying smaller berths. These vessels represent a liability to any marina for several reasons:

- Loss of revenue
- Discouraging new tenants (due to unsightly appearance of abandoned vessels)
- Potential for environmental contamination
- Expense of removal



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The Report accurately described the offshore (harbor) challenges to rehabilitation and modernization (repairs – reconfiguration) goals. Presumably, the realization of these goals will lead to increased service to the boating community and resulting revenues to the owner/operator of the Marina and the City of Alameda. These challenges include (*emphasis added to Report language*):

- *dredging in the western area of the Marina*
- *Over 1000 submerged, not "ideal head" piles cut off at the mud line where historic shipping piers were deconstructed making a large portion (54,110 sf) of the Marina's submerged leasehold unusable.*
- *Approximately 20% of the Marina's 515 berths are configured for smaller vessels (less than 30'). A number (approximately 22) of the existing floating dock piles (guide piles) are in need of repair and/or replacement.*
- *Pile repairs and harbor reconfiguration should take climate change and sea level rise, especially in number, height, and placement of piles.*

The number and size of berths is an important indicator of the Marina's current ability to provide service to recreational boaters and produce revenue. The number of vacant berths, by size is an indicator of the boating market. A number of berths are unusable, either due to ongoing maintenance or shoaling. Available area is another indicator of the Marina's ability to generate revenue.

The unusable area associated with the Marina (generally due to submerged piles) is approximately 54,110 sf, or 7.4 % of the leasehold.

Table 1 lists berths by category (length). The table also lists current vacancies.

The current inventory of berths and vacancies is an indicator that occupancy and revenues can be increased by accomplishing required dredging, as indicated by the Report, and converting smaller sized berths (24-32 feet) into larger sized berths (36-45 feet). It is important to note that fairways must also be widened to accommodate the larger boats. This can be accomplished by the removal of submerged piles and resulting reduction of unusable area.



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Table 1. Berth Size and Vacancy

Berth size	Total Number	Vacant
22	22	2
24	111	18
28	101	36
32	120	33
36	80	1
40	31	0
44	10	0
45	5	1
50	8	0
60	8	0
70	1	0
Total	515	93

Dredging Background

The Report accurately described the history of dredging in the Marina. It is important to emphasize the importance of establishing (and verifying) authorized permitted depths. The Report also accurately describes the importance of adjacent and inland activities with respect to the potential of introducing contaminants to the harbor area. Extremely important is the potential relationship, impact, and effect of the Brooklyn Basin South Channel reach of the federal channel and the United States Coast Guard (USCG) berth dredging on the proposed Marina maintenance dredging. Brooklyn Basin is the general term used by the U.S. Army Corps of Engineers to describe the federally authorized channel complex that runs from the eastern perimeter of the Oakland Inner Harbor Channel to the Park Street Bridge, including the South Channel.

The Report described current dredging needs in general terms. *It is important to verify these needs (condition survey), especially within the basins that support boat lifts/cranes.* It is also important to establish (and verify) dredged depths required to accommodate future needs (larger sailboats with deeper drafts) including visiting tall ships and potential America's Cup challengers, if these are consistent with the Marina's business plan. The proposed disposal site, SF-11, near Alcatraz Island, is only appropriate if the Dredged Material Management Office approves the testing results as Suitable for Unconfined Aquatic Disposal (SUAD). The Report accurately describes the process of developing (and implementing) a Sampling and Analysis Plan (SAP). The SAP, in turn leads to a Sampling and Analysis Report (SAR). It is important to note that this process (SAP-SAR-DMMO approval) will likely require duration of three months (90 days).



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It is important to determine required depths and current conditions adjacent to the travel lift area where boats are lifted out of the water for maintenance and relocation. It is also important to determine required depths and current conditions adjacent to the cranes where dry-stored boats are placed into the water for use. Currently, shoaling within and adjacent to these areas preclude lifting/launching of deep draft boats at low tides.

Recommendation: Conduct sampling and analysis within the area adjacent to the federal channel to determine base line conditions.

Permitting: It is important to note that the ten year permits with the United States Army Corps of Engineers (USACE) and Bay Conservation and Development Commission (BCDC) should run concurrently. The aforementioned permits should include provisions for advance maintenance, knockdowns, and grading to increase intervals between dredging episodes; although this may not be an issue for Alameda Marina. Water Quality Certification is a direct function (resulting from) sediment characterization (sampling and analysis)

Essential Fish Habitat is an evolving concern of the National Marine Fisheries Service (NMFS) and Environmental Protection Agency (EPA), as well as State/Regional agencies (California Department of Fish and Game (DFG), BCDC, and RWQCB). Specific issues related to the Estuary include, but may not be limited to, Pacific eelgrass (*Zostera marina*) and native Olympia oysters (*Ostrea conchaphila*). Surveys may be necessary to establish the presence of these species. If present, it is likely that the resource/regulatory agencies will require a mitigation plan.

The Report accurately described the importance of demonstrating that historical dredging has taken place through the entire Marina, *not just the western portion*. Otherwise, more extensive National Environmental Policy Act (NEPA) analysis may be required to provide environmental documentation in support of new work dredging.

The Report accurately described the potential relationship, impact, and effect of the Brooklyn Basin South Channel, *not Brooklyn Basin* federal channel and the United States Coast Guard berth dredging on the Marina dredging. Potential issues include the *documented* existence of contaminated sediments and potential for contaminant mobilization. Dredging within the federal channel would also have a potential impact on piles and other structures within the Marina, including bulkheads.

The Report accurately described potential measures to address the aforementioned issues and concerns, including assessment of current conditions, relocation needs, and financial comparison. The Report also addressed protective methodology, including partnerships with the City of Alameda/ neighboring property owners and stakeholders; pier replacement; and meeting(s) with the US Army Corps of Engineers. Finally, the Report appropriately recommended monitoring during federal channel dredging. Monitoring should also take place during USCG *berth* maintenance dredging.



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The U. S. Army Corps of Engineers recently conducted bathymetric surveys and sediment characterization (sampling and analysis) of the Brooklyn Basin – South Channel reach of the Federal channel. Dredging, however, is currently not funded for 2011 or 2012.

The Report accurately included a discussion on other potential sources of contamination. This is an issue of intense interest by the Dredged Material Management Office (DMMO) agencies during the process of developing and approving a Sampling and Analysis Plan (SAP). Other *contaminants of concern* may also include Polychlorinated biphenyls (PCBs). PCB's were widely used in transformers and may have entered the water column and become entrained in sediments as a result of transformer leaks.

The Report accurately described the benefits of a dredge material management plan and master sampling and analysis plan. These two planning tools can be done in collaboration with other marina's and property owners in the area, *achieving economies of scale* in controlling dredging costs.

The Report introduced the concept of dredging related challenges and opportunities. Guide piles, together with the *patent and latent presence of submerged pile, remnants, sunken vessels, debris, and rock* will certainly complicate dredging and lead to expensive change orders and claims if not adequately described to dredging contractors.

The former graving dock (basin) (combination leasehold – fee ownership) presents both a *potential problem area and an opportunity* to both the Marina and City of Alameda. Further deterioration of the structure can lead to loss of land and the introduction of contaminants into the harbor area. *The graving dock can also be used as a repository for contaminants, sediments if approved and permitted, providing more land area for parking and other revenue producing features.*

The Report mentioned the potential need for dredged material re-use – disposal alternative analysis. This will be required if Alameda Marina is not approved for inclusion within the alternative analysis exemption listing as a small dredger. *Although the Marina appears to qualify for the small dredged exemption in terms of dredge depths and cubic yardage of dredged material, a request to be listed as so may be initiated.*

The report accurately described the only ocean disposal alternative available to San Francisco Bay Area dredging projects consisting of the San Francisco Deep Ocean Disposal Site (SFDODS). The specialized dredge vessels mentioned in the Report generally consist of *larger (3,000-5,000 cubic yard capacity) dumpsons that would not be able to enter and be maneuvered even with an extra tug within the Marina harbor area.*

The Report accurately described how upland beneficial re-use, although desirable and encouraged by the DMMO and others, is likely to be impracticable due to cost. Although the Report accurately lists the Montezuma Wetlands Project as a potential upland re-use site, this



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may be impractical due to cost and distance (over 50 nautical miles) from the Marina. In addition, it is likely that 2-3 times the volume of clean material will be needed to cover contaminated material, rather than the equal volume stated in the Report. This option can become more viable depending on the volume of material determined to be contaminated and the potential for coordinating with other concurrently planned dredge projects within the vicinity that may also be challenged with contaminated sediments.

The Report accurately described the costly rehandling process required to support beneficial reuse of contaminated sediment as daily fill and cover at a local landfill. However, the Berth 10 Rehandling Site, located at the Port of Oakland, has only accepted material from public agencies to date.

The Report accurately described the range of costs for 2011 dredging plan. The level of restrictiveness may be confusing due to a lack of footnotes. The level of restrictiveness is based on a variety of factors including levels of contaminants, with on-bay disposal being the most restrictive and Berth 10 (ultimate placement as daily fill and cover at landfills) generally being the least restrictive.

Alameda Marina – Inventory and Condition Survey of Existing Conditions

The Report generally provided an accurate overview of existing facilities, including construction history, upgrades, repairs, and maintenance. The report also described the difference and delineation between the leased property and fee simple property.

The Report accurately described the existing shoreline condition including the sloped shoreline between docks 3 and 2 (118'). The lower slopes of the existing nrap should be examined at extreme low tide, during the day, to determine its condition. Failure along the lower slopes of the nrap shore protection could be problematic with respect to dredging. Moreover, there appears to be extensive placement of concrete as well as the accumulation of other debris including pipe, wire and cable, along the shoreline. This accumulated debris should be removed prior to dredging.

The Report accurately described the existing steel bulkhead system providing shoreline protection between docks 7 and 6 (190' long). In addition to the replacement of the lower water (also water) some forensic analysis should be conducted to determine if a corresponding water was constructed on the inside of the bulkhead, as per industry practice. If the inside water is present, determine its condition.

The Report accurately described the existing bulkhead system providing shoreline protection between docks 6 and 5 (215' long). I am particularly concerned about the poor condition as described in the Report. I suspect that there is a significant potential of catastrophic failure due to the continuation of live and dead loads imposed on the structure. The aforementioned loads could result from heavy boat transport, and the use of the crane to place boats in the water.



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land or retrieve them). Concur with recommended repairs - replacement, including the tieback connections.

The Report accurately described the existing steel bulkhead system providing shoreline protection at the East Docks (165'). Concur with the Report's recommendations establishing a "no-dredge" area outward of the bulkhead to guard against additional damage to the structure.

The Report accurately described the existing bulkhead system at the Graving Basin shoreline.

Recommendation: Develop a plan for future use of the Graving Basin (dock) as described above (Dredging Background)

The Report accurately described the existing steel bulkhead system providing shoreline protection between Docks 7 & 6 (30').

Recommendation: Conduct additional structural analysis to confirm potential for catastrophic failure and develop recommendations for repairs - replacement as required.

The Report accurately described the existing concrete bulkhead (1100 linear feet) system providing shoreline protection for the Marina. I am particularly concerned about the reported erosion along the bulkhead toe. *This could lead to the formation of large voids behind (shoreward) of the bulkhead system. The voids in turn could result in sinkholes where the flexible pavement fails directly above the void. This represents a significant safety hazard.*

Recommendation: Fill and armor the eroded area at the area along the bulkhead toe

The Report accurately described the existing bulkhead system providing shoreline protection at the east Docks (65'). Concur with recommended repairs; retrofit, including replacement of gangway (ramps) support structures.

The Report accurately described the existing wood bulkhead system (400 linear feet) providing shoreline protection for the Marina. The water located at the top should have a corresponding inside water as per industry practice

The Report accurately described the existing wood bulkhead system (400 linear feet) providing shoreline protection between docks 4 and 3.

Recommendation: Conduct additional structural analysis, including inspection of suspended utilities (water, electrical, fire) to confirm potential for failure and develop recommendations for repairs - replacement as required. Electrical utility failure could be a significant safety hazard.

The Report accurately described the existing wood bulkhead system (95 linear feet parallel to the shoreline) providing shoreline protection between docks 4 and 3.



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The Report accurately described the existing concrete / wood bulkhead (200 linear feet) system providing shoreline protection for the Marina. It is not clear if the "triple-tube water system" is for potable water or fire suppression. Nor is it clear with respect to serviceability. Concur with the Report's replacement recommendations with respect to the steel water. Recommend that an inner water be constructed as per industry standard.

The Report accurately described the existing bulkhead system behind the wooden piers.

Recommendation: Develop a plan for the use / disposition of Old Pier 4, consistent with the Marina's business plan.

The Report accurately described the existing bulkhead system between docks 4 and 3 (20'). Concur with the Report's recommendation for a detailed structural survey of the pier below the building (Building 14).

The Report accurately described the existing bulkhead system between docks 3 and 2 (Old Pier 3) (44').

Recommendation: **Conduct a detailed structural analysis, including inspection of the infrastructure and pilings supporting the travel lift, to confirm its serviceability.**

The Report accurately described the existing bulkhead system shore side of dock 2 (53'). The photographs included in the report appear to include **unprotected, handwired utilities and/or steel cables.** Recommend removal of same. The photographs also depict wooden piles wrapped with reinforcement to extend their design life. The piles themselves should be inspected to determine if further deterioration has occurred. Recommend a structural survey of the pier.

The Report accurately described the existing bulkhead system shore side of dock 2 (43'). Recommend a structural survey of the pier.

The Report accurately described the existing bulkhead system adjacent to dock 1 - south (225'). Concur with the Report's recommendations with respect to pile replacement. However, it may be more cost effective to demolish the pier structure and rebuild rather than relocate.

The Report accurately described the steel pier providing shoreline protection to the Marina.

Recommendation: Consider a complete and detailed structural survey of the entire bulkhead system(s) providing shoreline protection for the Marina. This would involve the condition of the bulkhead itself, toe of same, walers, tieback systems, etc. The order-of-magnitude cost estimate of a detailed structural survey, as described above, is \$55,000.

The Report accurately described the condition of the floating dock system within the Marina.



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The Report accurately described stick frame dock construction. It is important to note that this basic technology has been significantly improved since the Marina's construction by including tension bars within fingers, lumber treated to extend its life, stainless steel fasteners, and floats manufactured with modern, sustainable materials, including expanded polystyrene (EPS) foam filled pycnoons (floats). Stick frame technology construction can be further improved and stabilized by the installation of guide roller frames, as was done at Oakland Yacht Club.

The design life of modern, stick frame dock construction, as described above, is 20 years. It is important to note that this is an extremely conservative estimate, based on construction operation in an aggressive environment (heavy loadings, extreme tides, ice, etc.). Oakland Yacht Club is planning on its reconfigured berths lasting a minimum of forty years.

The Report accurately described the existing condition of the floating docks. Additional problems, besides those noted in the Report include (but are not limited to) varying elevations due to deteriorating floats; twisting fingers due to floatation and frame issues; missing and/or rusting fasteners; missing/loose/broken mooring cleats, undersized and broken roller guides; pile heaps; and missing/loose/deteriorated deck edging (cushioning). These issues are common to aging marinas and are normally properly classified as maintenance. At some point in time, it becomes practical and cost effective to replace the entire structure rather than continue with repairs.

The Report accurately described the existing condition of the floating covered berths. Concur with the Report's recommendations for aluminum roofing replacement. *Further recommend consideration of a sustainable roofing system, such as photo-voltaic, to provide electrical energy to the Marina.*

The Report accurately described the inadequate number and spacing of guide piles within the harbor. In addition to dead (dock structures) and live (boats) loads, pile supported floating dock systems must be designed and constructed to account for environmental factors. In spite of the Marina's generally protected location within the Oakland Estuary, winter storm events and boat wakes can and do cause damage to docking systems. Inexperienced boaters also will damage dock structures when departing or entering berths. Concur with the Report's recommendation for a design evaluation of the pile supported dock structure prior to extensive modifications and/or reconfiguration. Also concur with the report's recommendations for immediate replacement of "marginal" piles.

The Report accurately described the condition of the piping/delivery systems for potable water. Recommend additional survey to determine if manne-grade back flow preventers (valves) have been installed and are serviceable.

The Report accurately described the condition of the existing fire suppression system. Concur with the Report's recommendations for increased sizing of pipes to meet current codes for



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combined portable fire suppression systems; and the installation of “wet boxes” (cabinets that contain a fire hose attached to the standpipe).

The Report accurately described the condition of the existing electrical systems in the harbor. Concur with the report’s recommendations for replacing receptacles with current marine grade receptacles, circuitry, conduit, wiring, and junction pull boxes. Further recommend a survey be taken of berth tenants to determine electrical demand. Combined power/water pedestals (incorporating marine grade 30 amp 120 volt twist lock (meeting the majority of boat needs) receptacles, meters, safety lighting and hose bibs) are generally available. Concur with the report’s recommendations to evaluate the demand for 30 amp 208 volt twist lock receptacles for larger berths and end ties.

The Report accurately described the condition of the existing gangways (ramps) within the Marina. Concur with the Report’s recommendations for replacement with longer ramps and compliance with ADA regulations. Lightweight aluminum ramps are generally available. Further recommend non-skid surfaces and side bars as a safety feature for children and equipment.

Recommendation: Alameda Marina should consider the utilization of safety ladders placed at appropriate locations within the harbor to provide a method of egress for staff, boaters and guests who may fall into the water.

Alameda Marina – Reconstruction Phasing Recommendations

The report comprehensively described a 15 year plan incorporating six multi-year project phases. Generally concur with the Marina’s phasing plan as described in the document. Concur that a Marina Reconstruction Site Plan will need to be developed. Further concur that detailed construction cost estimates be developed for each phase of the plan. *Further recommend that a permitting plan be developed, together with a plan for relocating boats between phases.*

Generally concur with the Report’s recommendations for Phase 1 – Dredging, Pile Repairs, and Upgrades. *Further recommend that the Marina conduct a risk/hazard analysis assessment to identify water issues and place them at priority for repairs/replacement as appropriate.*

Dredging and immediate repairs, as required to make berths serviceable may result in increased occupancy and revenues. It is not clear if the \$395,000 estimated cost for maintenance dredging is meant to include permitting, testing, and analysis. If so, I suspect that the estimate is low. Order-of-magnitude estimate for maintenance dredging, including permitting, testing, and analysis should be \$425,000.

Recommend that Phase 2, Pile evaluation and replacement, be done concurrently with Phase 1 Dredging may result in further damage to deteriorated pilings and dock system. Docks may need to be temporarily relocated to accommodate dredging equipment. Significant economies



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of scale will likely be realized if the same contractor accomplishes pile replacement and dredging. Assuming that 22 marginal piles will need to be replaced, per Table 3 of the Engineering Report, and that the aforementioned economies of scale are realized, the \$100,000 cost estimate is appropriate.

Generally concur with the Report's recommendations for Phase 3, with the provision that safety/emergency measures take place within Phase 1. Also concur with the estimated cost of \$255,000, assuming that repairs/replacement are confined to the combined potable/fire suppression system; and do not include the deteriorated electrical, sanitary/storm water and other utilities mentioned in the paragraph following Phase 3.

The cost estimate for Phase 4, Dock and Facility Electrical Systems Upgrade is low, based on recent experience bringing electrical systems up to code at three docks at Oakland Yacht Club. Although Phase 4 is entitled "Dock and Facility Electrical Systems Upgrade", the accompanying scope of work only includes electrical repairs and upgrades. Recommend that this phase include replacement of pumps as recommended in the Report's Inventory and Condition Survey. Also recommend inclusion of budget for Dock system component repairs and replacement, as it will become evident, during preconstruction surveys, that this will be required (based on experience with local marinas with analogous construction). Revised order-of-magnitude cost estimate for Phase 4 - \$300,000.

The cost estimate for Phase 5, Dock Electrical System Maintenance is low (also based on recent experience bringing electrical systems up to code at three docks at Oakland Yacht Club). Recommend that Phase 5 be undertaken concurrently with Phase 4. Economies of scale will be realized by combining permitting, code compliance issues, design, purchase of components/materials, construction, construction management, and contract administration. Assuming realization of aforementioned economies of scale, the revised order-of-magnitude cost estimate for Phase 5 should be \$200,000.

Concur with the scope of work and phasing of Phase 6, Shoreline repairs to Leasehold Areas. The cost estimate may be low, depending on the extent of repairs. Recommend revised order-of-magnitude cost estimate for Phase 6 - \$425,000.

Recommend a revised total estimated reconstruction budget of \$1,705,000.

Concur with the Report's recommendations for Final Design Requirements and Permit Requirements.

Further recommendations: Recommend that a Marina Reconstruction Site Plan, mentioned above be developed to include harbor reconfiguration. Some elements to the harbor reconfiguration plan may include, but not be limited to the following:

- Removal of submerged pilings and relocation of docks to establish adequate fairway widths to accommodate larger berths/boats.
- Reconfiguration of Marina to convert smaller berths into larger berths.



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Table 2. Phasing Comparison / Summary

Phase	Description	Engineering Report Budget	Peer Review Recommendations Budget	Estimated Duration
1	Dredging Maintenance	\$395,000	\$425,000	1 year
2	Pile Evaluation & Replacement	\$100,000	\$100,000	3 months
3	Fire Water Supply Systems Upgrades	\$255,000	\$255,000	4 months
4	Dock Facility Electrical Systems Upgrades	\$210,000	\$300,000	6 months
5	Dock Electrical System Maintenance	\$115,000	\$200,000	6 months
6	Shoreline Repairs to Leasehold Areas	\$380,000	\$425,000	9 months
Total		\$1,465,000	\$1,705,000	2.5 years*

* Some tasks/phases can be done on a concurrent basis

Conclusion.

Generally concur with the Conclusions presented in the Report. Although I concur with the Report's language that none of the identified projects are "revenue producing"; it is important to note that as minimum, dredging and required repairs, as addressed in the Report will reduce the Marina's ability to realize revenue from existing facilities (generally berths). Recommend that the total budget for identified projects be revised to \$1,705,000.

Recent experience at Oakland Yacht Club (OYC) clearly demonstrated that electrical upgrades, pump replacement, conversion of smaller berths into larger berths, and reconfiguration of docks to provide appropriate fairway widths and take advantage of OYC's property provided positive results in terms of increased revenue. Specifically, OYC's vacancy rate decreased from 20% to 2%. It may be interesting to note that the current vacancy rate at OYC results from berths not meeting current boaters' needs, with respect to widths.

References

Cardoza, L.E., "Environmentally Sustainable Design, Construction, Maintenance, and Operation Feasible Applications for Ports and Harbors of All Sizes". Paper and Presentation to Western Dredging Association, November 6, 2009.



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Guidelines for Marina Berthing Facilities. California Department of Boating and Waterways, 2005

The Hull Truth Boating Forum. Posting 05/25/09 www.thehulltruth.com/berthing-forum.

Farrall, Tracey-Kirsten. Yacht Broker. Pers. Com.

Wong, Craig. Oakland Yacht Club General Manager

I am pleased and available to discuss the aforementioned peer review with you at your convenience. I can be reached at 510-333-0757 or at Len.Cardozo@westonsolutions.com

Sincerely,

*Len Cardozo, CEP
Senior Technical Advisor
Weston Solutions, Inc.
428 13th Street, 6th Floor, Unit B
Oakland, CA 94612
(510)808-8102 Direct / Fax
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NANCY McPeak

From: Esther Burlingame <esther.burlingame@gmail.com>
Sent: Monday, May 21, 2018 9:40 PM
To: NANCY McPeak
Cc: Robert Burlingame; Esther Burlingame
Subject: Regarding May 29th Item 7A: Master Plan, Density Bonus Application, and Environmental Impact Report for the Alameda Marina

Dear Alameda Planning Board,

We will be out town when you have the public hearing on May 29th to consider a recommendation to approve the master plan, density bonus application, and environmental impact report for the Alameda Marina Property Located at 1815 Clement Avenue (APN 071-0288-003 and 071-0257-004) in the Northern Waterfront Priority Development Area. We are home owners and residents of the West End of Alameda for the past 35 years and rent space at the Alameda Marina and would like you to take our concerns into consideration.

Over the past 6 years we have watched the City approve additional housing units to be in compliance with State housing law following the City's actions to accommodate Alameda's share of its Regional Housing Needs Allocation. The increased housing has led to incredible traffic challenges getting on and off the island. A simple 3 mile commute home from downtown Oakland now takes 40-50 minutes between 4:30 and 5:30 p.m. When leaving the island in the morning if the Posey tube is backed up, traffic is already grid locked on Clement, Buena Vista, Pacific and Lincoln in attempts to get to the Park Street bridge.

The thought of adding 760 additional housing units on the Alameda Marina Property will just further intensify the traffic nightmare on our beloved island. I encourage you to carefully consider the negative impact of the Master Plan on the community. This area should be used to create and expand our maritime and blue economies and to restore a balance to jobs and housing.

Sincerely,

Esther and Robert Burlingame

NANCY McPeak

From: gdhsmove@aol.com
Sent: Thursday, May 17, 2018 10:29 AM
To: NANCY McPeak; David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; ANDREW THOMAS
Subject: Support Alameda Marina's Working Waterfront!

Dear Alameda Planning Board, I **DON'T** support Alameda Marina's plans to preserve Alameda's waterfront heritage by repairing the crumbling marina, improving maritime commercial space, providing waterlife activity, and adding housing at the site. Together, let's create a Waterfront that Works!

Sent from [Mail](#) for Windows 10

NANCY McPeak

From: Roberta Bobba <rebobba@att.net>
Sent: Thursday, May 17, 2018 12:59 PM
To: NANCY McPeak; David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; ANDREW THOMAS
Subject: Support Alameda Marina's Working Waterfront!

Dear Alameda Planning Board, I support Alameda Marina's plans to preserve Alameda's waterfront heritage by repairing the crumbling marina, improving maritime commercial space, providing waterlife activity, and adding housing at the site. Together, let's create a Waterfront that Works!

NANCY McPeak

From: Eileen Morrison <vtscrub@aol.com>
Sent: Thursday, May 17, 2018 4:39 PM
To: NANCY McPeak; David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; ANDREW THOMAS
Subject: Support Alameda Marina's Working Waterfront!

Dear Alameda Planning Board, I would like to know what exactly the planning board is planning on doing about egress from the island. I work in an operating room in Oakland and have to take call. I must be at the hospital in 30 minutes when I am called in for emergencies. There are weekend days that I barely make it and the Sunday of Alameda's Antiques Market I have to go to Oakland and just stay there all day because I could not get off the island in time. I love on the tunnel end of the island. The city wants to develop every square inch of this island but I never hear of any plans for more means of getting OFF the island. What about disasters, plane crashes, fires and all the other horrible scenarios that are real possibilities when we would need to get to safety. Those are my true concerns that I never hear ANYTHING ABOUT.

Eileen Morrison

Sent from my iPad



ENCINAL YACHT CLUB

May 17, 2018

Andrew Thomas
Assistant Community Development Director
City of Alameda
Alameda, CA 94501

RE: Alameda Marina Project

Dear Mr. Thomas,

The Encinal Yacht Club was founded in the City of Alameda in 1890. The Club has continuously promoted recreational yachting on the bay since its conception. Our Club is an active part of our community and has contributed extensively to promotion of junior sailing, adult sailing, and personal watercraft use.

The City of Alameda has a rich maritime history dating back to the Gold Rush with substantial contributions during World War II. Since that time, legacy infrastructure has been transformed to a working waterfront with significant recreational marina development. The City and the surrounding community are home to approximately 3000 recreational vessels which make up 1/6th of the entire fleet on San Francisco Bay.

The City needs to maintain its working waterfront which includes a full-service boat yard for recreational vessels. All in-water vessels should be hauled, and bottom painted every 3 years on average. The Svendsens Boatworks and the Boatyard at Grand Marina were our only options for maintaining our vessels. The Svendsens and Grand Marina boatyards could accept 37 and 18 boats respectively – which could barely keep up with demand.

The proposed development for the Alameda Marina Project will significantly change the existing uses of the Alameda-Oakland Estuary. The project will invest \$75 Million in infrastructure improvements including the replacement of the seawall and provisions for sea level rise. The Board of Directors of the Encinal Yacht Club is supportive of the project, but requests that the developer include the infrastructure necessary to accommodate a full-service boatyard, including bottom painting, for the same number of vessels as the previous boatyard.

Please do not hesitate to call at 510-522-3272 or email me at bnorkusa@gmail.com if you would like to discuss the matter further.

Sincerely,

Bill Nork
President EYC

OAKLAND



ALAMEDA

PORTOBELLO MARINA
TOTAL SLIPS: 67
OCCUPIED SLIPS: 40

N. EMBARCADERO COVE
TOTAL SLIPS: 113
OCCUPIED SLIPS: 100

UNION POINT BASIN
TOTAL SLIPS: 93
OCCUPIED SLIPS: 58

JACK LONDON SQ. MARINA
TOTAL SLIPS: 137
OCCUPIED SLIPS: 92

5TH AVENUE MARINA
TOTAL SLIPS: 94
OCCUPIED SLIPS: 89

CENTRAL BASIN
TOTAL SLIPS: 281
OCCUPIED SLIPS: 233

OUTBOARD MOTOR SHOP MARINA
TOTAL SLIPS: 16
OCCUPIED SLIPS: 10

BARN HILL MARINA
HOUSE BOATS: 50
SAIL BOATS: 23

MARINER SQ. MARINA
TOTAL SLIPS: 50
OCCUPIED SLIPS: 46

PACIFIC MARINA
TOTAL SLIPS: 225
OCCUPIED SLIPS: 183

GRAND MARINA
TOTAL SLIPS: 317
OCCUPIED SLIPS: 262

NOB HILL MARINA
TOTAL SLIPS: ~10
OCCUPIED SLIPS: 9

MARINA VILLAGE
TOTAL SLIPS: 735
OCCUPIED SLIPS: 660

FORTMAN MARINA
TOTAL SLIPS: 497
OCCUPIED SLIPS: 410

ALAMEDA MARINA
TOTAL SLIPS: 466
OCCUPIED SLIPS: 327

PARK ST. MARINA
TOTAL SLIPS: 23
OCCUPIED SLIPS: 19

BALLENA MARINA
TOTAL SLIPS: 523
OCCUPIED SLIPS: 254

EAST END RESIDENCES
TOTAL SLIPS: ~100
OCCUPIED SLIPS: ~45

AEOLIAN YACHT CLUB
TOTAL SLIPS: 96
OCCUPIED SLIPS: 67

ALAMEDA-OAKLAND
MARINA COUNT
EXHIBIT

OAKLAND



GRAND MARINA BOAT YARD
TOTAL SPACES: 18
OCCUPIED SPACES: 17



SVEDSEN'S BOAT WORKS
TOTAL SPACES: 37
OCCUPIED SPACES: 36

ALAMEDA

Alameda

ALAMEDA-OAKLAND
BOAT YARD
EXHIBIT

NANCY McPeak

From: Amy Rose <amyjrose9@gmail.com>
Sent: Tuesday, May 22, 2018 4:21 PM
To: NANCY McPeak
Subject: May 29 Item 7A

Dear Alameda Planning Board,

I agree strongly with these comments by Save Alameda's Working Waterfront:

The Master Plan for Alameda Marina shows Bay West intends to provide 103 affordable housing units out of a total of 760. This barely scratches the surface of the affordable housing needs while creating a traffic nightmare on a small stretch of Clement Ave. between Park and Grand Streets.

This road is a Truck Route and a bike path is also being considered. Please do not add such a high number of housing units to those already approved for this street that is so close to the Park Street Bridge.

This development further erodes our already lopsided jobs/housing balance. We should be using this property to expand our maritime and blue economies and adding convenient services to the north coast communities that will reduce the need for automobiles. Bay West's project does not include nearly enough parking to meet the needs of the new residents, visitors, recreational boaters who have vessels in the marina and users of the planned recreational facilities for this development.

Together, let's create a Waterfront that Works! I encourage you to think about the effects of this Master Plan and its negative effects on our community [on May 29th](#).

Sincerely,
Mrs. Amelia Rose
1917 Chestnut St
Alameda
Sailboat owner

NANCY McPeak

From: Brian Schumacher <bdschumacher@gmail.com>
Sent: Wednesday, May 23, 2018 8:38 AM
To: NANCY McPeak
Cc: Kathleen Schumacher
Subject: Regarding May 29th Item 7A: Master Plan, Density Bonus Application, and Environmental Impact Report for the Alameda Marina

Dear Members of the Alameda Planning Board:

Seven hundred sixty new housing units near Clement Ave and Park St. will worsen the already bad morning and evening traffic on and off the Island. New housing is needed but a better place to put it is on the acres of open space at Alameda Point, among others.

Instead of so much housing at Alameda Marina, please use the waterfront there to support more jobs on the Island, and reserve that space mostly for commercial/light industrial uses.

A few dozen units at each end of the site, as initially proposed a few years ago, or at most, about half of the proposed 760 units, would still provide the developer the funding needed to rehab the seawall.

Sincerely,
Brian and Kathy Schumacher

NANCY McPeak

From: Cari Trease <caritrease@gmail.com>
Sent: Wednesday, May 23, 2018 10:03 AM
To: NANCY McPeak; Becca Perata
Subject: Support Maritime Commercial at Alameda Marina!

Dear Alameda Planning Board,

Redevelopment of Alameda Marina will both retain the site's maritime commercial core and also foster an environment to attract a new generation of business to the island— in up to 250,000 square-feet of commercial space. The plan will help retain some well-paying maritime jobs and bring approximately 300 sustainable career opportunities with future businesses.

This is only made possible by reinvesting in the aging Marina. Pacific Shops Inc. has committed to \$57 million in infrastructure. Alameda gets a new sea wall, utilities along Clement Avenue, Bay Trail extension and bike-pedestrian path, aquatic park, open space, and preservation and restoration of 11 historic buildings for future commercial use.

Together, let's create a Waterfront that Works! I encourage your approval of the Master Plan on May 29th.

Sincerely,

Cari Trease, local resident (Buena Vista and Elm)

NANCY McPeak

From: Claudia Viera, Esq. <claudiaviera@earthlink.net>
Sent: Wednesday, May 23, 2018 10:34 AM
To: NANCY McPeak
Subject: Support for Development at Alameda Marina!

Dear Planning Board,

I support the development of the aging Alameda Marina (and of the old Del Monte warehouse area), mostly because it will open up a publicly under-utilized area for pedestrians/cyclists, families and neighbors. As a local neighbor, I believe this may be the only way to create more access to the estuary shoreline which is hidden behind an unattractive set of buildings/fences on Clement Ave. I also hope that some smaller offices and conference room spaces may become available for small business owners, like myself. Bringing in cafes, smaller grocery stores (think Berkeley Bowl), restaurants and housing while also retaining some maritime access makes a lot of sense. But please keep it human-size in scale (3-4 stories); the one thing I do NOT support is 14+ story buildings.

Redevelopment of Alameda Marina will both retain the site's maritime commercial core and also foster an environment to attract a new generation of business to the island— in up to 250,000 square-feet of commercial space. The plan will help retain some well-paying maritime jobs and bring approximately 300 sustainable career opportunities with future businesses.

This is only made possible by reinvesting in the aging Marina. Pacific Shops Inc. has committed to \$57 million in infrastructure. Alameda gets a new sea wall, utilities along Clement Avenue, Bay Trail extension and bike-pedestrian path, aquatic park, open space, and preservation and restoration of 11 historic buildings for future commercial use.

Together, let's create a Waterfront that Works! I encourage your approval of the Master Plan on May 29th.

Claudia Viera
Saint Charles St.

NANCY McPeak

From: Aaron <lancetonearms@gmail.com>
Sent: Wednesday, May 23, 2018 12:37 PM
To: NANCY McPeak; becca@voxpathulpr.net
Subject: Support Maritime Commercial at Alameda Marina!

Dear Alameda Planning Board,

Redevelopment of Alameda Marina will both retain the site's maritime commercial core and also foster an environment to attract new business to the island— in up to 250,000 square-feet of commercial space.

This is only made possible by reinvesting in the aging Marina. Pacific Shops Inc. has committed to \$57 million in infrastructure. Alameda gets a new sea wall, utilities along Clement Avenue, Bay Trail extension and bike-pedestrian path, aquatic park, open space, and preservation and restoration of 11 historic buildings for future commercial use.

As a nearby resident, I am excited by the possibility of being able to enjoy more of the waterfront with my family, and walking to the shopping and amenities that will be created.

I encourage your approval of the Master Plan on May 29th.

Thanks,

Aaron Clements
1538 Clement Ave
Alameda

NANCY McPeak

From: Eileen <Eileen@alamedamarina.net>
Sent: Wednesday, May 23, 2018 10:05 AM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Support Maritime Commercial at Alameda Marina!

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Alameda Planning Board,

Redevelopment of Alameda Marina will both retain the site's maritime commercial core and also foster an environment to attract a new generation of business to the island— in up to 250,000 square-feet of commercial space. The plan will help retain some well-paying maritime jobs and bring approximately 300 sustainable career opportunities with future businesses.

This is only made possible by reinvesting in the aging Marina. Pacific Shops Inc. has committed to \$57 million in infrastructure. Alameda gets a new sea wall, utilities along Clement Avenue, Bay Trail extension and bike-pedestrian path, aquatic park, open space, and preservation and restoration of 11 historic buildings for future commercial use.

Together, let's create a Waterfront that Works! I encourage your approval of the Master Plan on May 29th.

NANCY McPeak

From: Cameron Imani <cameron_imani@me.com>
Sent: Wednesday, May 23, 2018 4:21 PM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Maintain basic boat maintenance at Alameda Marina!

Dear Alameda Planning Board,

As a boat owner with children who sail and represent Alameda in high school sailing, I'm concerned about the lack of boat maintenance facilities in Alameda. You can't have boats without painting bottoms. It's like wanting automobiles, but revoking permits for tire shops.

The anchor on the Alameda city flag is not just for decoration.

I encourage your amend the master plan to require facilities for basic boat maintenance (no major overhauls or refits - just basic maintance). This should be a topic of discussion on May 29th.

Thanks You, Cameron Imani

Sent from my Phone

NANCY McPeak

From: Eileen Zedd <jus4leen@gmail.com>
Sent: Wednesday, May 23, 2018 10:52 AM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Support Maritime Commercial at Alameda Marina!

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Alameda Planning Board,

Redevelopment of Alameda Marina will both retain the site's maritime commercial core and also foster an environment to attract a new generation of business to the island— in up to 250,000 square-feet of commercial space. The plan will help retain some well-paying maritime jobs and bring approximately 300 sustainable career opportunities with future businesses.

This is only made possible by reinvesting in the aging Marina. Pacific Shops Inc. has committed to \$57 million in infrastructure. Alameda gets a new sea wall, utilities along Clement Avenue, Bay Trail extension and bike-pedestrian path, aquatic park, open space, and preservation and restoration of 11 historic buildings for future commercial use.

Together, let's create a Waterfront that Works! I encourage your approval of the Master Plan on May 29th.

NANCY McPeak

From: John Platt <johntplatt@gmail.com>
Sent: Thursday, May 24, 2018 9:47 AM
To: NANCY McPeak
Cc: ANDREW THOMAS
Subject: Alameda Marina

Follow Up Flag: Follow up
Flag Status: Flagged

Good morning Ms. McPeak,

I am not sure that I will be able to attend next Tuesday's meeting so I thought I had better send my thoughts on the Alameda Marina Plan.

Considering the traffic impact of this project I am surprised (Did I miss it?) that there is not a dedicated space for water taxis and small ferries (20-40 passengers) to a land and unload at the Alameda Marina. Considering our limited bridges and tube it seems essential that we develop the water highway in conjunction with waterfront development. The only thing you need to build is docks , no roads, no bridges etc.

FYI I have worked in the marine industry for over 40 years.

I have seen the working barges be successful in Richmond and they do present an interesting cost efficient way to service boats without going through the cost of a haul out.

The plan talks of room for 30 boats to be worked on the land. I question this number or must ask what size boats? The plan states that sanding and painting will not be allowed, which are the core of what a boatyard does. I am not sure what work this on land yard would do. Straightening a shaft, installing a thru-hull are a few of the jobs I can think of that might fit this requirement. However usually when you go to the expense of hauling a boat you will want to refresh the bottom paint when it comes out.

I am happy to see that the boating community has been taken into consideration in this plan. I do believe however that we will see a negative impact on our boating community with the loss of another fill service boatyard.

Below is the passage in the Plan that got my attention.

Thank you all for the time and dedication you take for serving our community.

All the best,
John Platt

Maritime Commercial Area Improvements: The Master Plan provides for up to 180,972 square feet of maritime commercial floor area and yard space and all new infrastructure to support maritime commercial uses. The Master Plan includes a new boatyard and dockyard facilities and spaces on both land and water for recreational boat service and repair. In water, "Service Ships" will provide facilities for boat maintenance and repair with direct access to the boats in the water and enable traditionally underserved services like engine installation and service, marine electronics, canvas, fiberglass, rigging, and mechanical services to occur at Alameda Marina. The facilities will also provide for "incubator" opportunities, co-op arrangements, apprenticeship programs, and synergy between the various providers.

The Master Plan also includes space for the servicing of rigging and boats on land that cannot occur conveniently on the water utilizing the existing deep water and existing travel lift rails. The space is adequate to service up to 30 boats (up to 60 feet in length) on land and in the adjacent maritime buildings. For comparison purposes, the facilities will be more than twice the size of the nearby Grand Marina boatyard, and roughly equivalent in size and capacity to the Berkeley Marine Center, the San



Service Development
1600 Franklin Street, Oakland CA 94612

May 23, 2018

Alameda Planning Board
2263 Santa Clara Avenue
Alameda, CA 94501

Re: May 29, 2018 Agenda, Item 7A—Alameda Marina Master Plan

Dear President Mitchell and Planning Board Members:

AC Transit supports the approval of Item 7A—the Master Plan, Density Bonus, and Environmental Impact Report (EIR) for the Alameda Marina property. The Plan would allow up to 760 housing units, as well as commercial and public space development. The Plan includes a suite of Transportation Demand Measures (TDM), including provision of AC Transit passes to all residents and employees.

AC Transit appreciates the City of Alameda's continued coordination with our agency on projects and initiatives. We look forward to enhancing our partnership with the City, and to continuing to help mitigate traffic impacts resulting from new projects, such as this proposed residential development under the Alameda Marina Master Plan.

AC Transit is working with the City on an ongoing basis to build a robust transit network. This network allows residents and employees to travel to multiple key destinations—especially in Alameda, Oakland and San Francisco—via public transit. This network reduces the need for private automobile use and reduces overall congestion, particularly through the City's tubes and bridges. We are currently exploring how to optimize Transbay service to maximize ridership on our existing resources. This includes high-capacity transit vehicles to meet growing demand on the island. Should voters approve Regional Measure 3 in the June 5 primary, AC Transit will be in a position to add service on existing Alameda Transbay routes.

The addition of multi-unit projects, such as development under the Alameda Marina Master Plan, will help AC Transit continue to improve the transit network in the Northern Waterfront of Alameda. A year ago, we reinstated the Line 19 on Buena Vista Avenue. Line 19 runs one block from this site, and provides direct service to two BART stations in Oakland. This line currently serves existing neighborhoods, as well as residents in the new communities at Marina Shores and Mulberry, and will serve both the Del Monte Warehouse property and Encinal Terminals, as well as Alameda Marina. We therefore anticipate increased ridership on AC Transit buses on the Line 19 and throughout the Northern Waterfront.

AC Transit will continue to work with the City of Alameda's recently established Alameda Transportation Management Association (Alameda TMA), whose members will include all future Northern Waterfront projects. We are especially excited to add another project to the EasyPass program, which provides each household with one AC Transit EasyPass (a transit pass for unlimited rides on local and Transbay service) with their annual TMA assessment.

AC Transit looks forward to working with the City to further develop and implement these proposals to provide a high-level of quality and sustainable transit service.

Sincerely,



Robert del Rosario

Director of Service Development and Planning

CC: Andrew Thomas, Assistant Community Development Director
Jennifer Ott, Director of Base Reuse and Transportation
Michael Hursh, AC Transit General Manager

Francisco Boatworks at China Basin, KKMI in Sausalito, the Vallejo Boatworks, and the San Rafael Yacht Harbor.

Given the proximity of the proposed residential uses in the Master Plan and current permitting requirements for boatyards, it is unlikely that landside facilities will be suitable for boat painting or sanding. Boats that require bottom work can be serviced at Grand Marina, the Berkeley Marine Center, and a variety of other facilities around the Bay. Houseboats may be serviced at Bay Ship and Yacht. "

NANCY McPeak

From: Eileen Zedd <jus4leen@gmail.com>
Sent: Friday, May 25, 2018 10:05 AM
To: NANCY McPeak; Becca Perata
Subject: Plans for Alameda Marina Include a New Dockyard

Dear Alameda Planning Board,

Alameda Marina's latest Master Plan advocates for a modern, clean and efficient way to service boats which maintains a fully-functioning working waterfront while also opening up the property for all Alamedans. This merge of the working waterfront with new housing features affordable units, public open space, trails, and a new aquatic park where people can access the estuary like never before.

After more than two years of public review and changes, I recommend you move this plan forward so Alameda has a waterfront that works!

NANCY McPeak

From: Eileen <Eileen@alamedamarina.net>
Sent: Friday, May 25, 2018 10:13 AM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Plans for Alameda Marina Include a New Dockyard

Dear Alameda Planning Board,

Alameda Marina's latest Master Plan advocates for a modern, clean and efficient way to service boats which maintains a fully-functioning working waterfront while also opening up the property for all Alamedans. This merge of the working waterfront with new housing features affordable units, public open space, trails, and a new aquatic park where people can access the estuary like never before.

After more than two years of public review and changes, I recommend you move this plan forward so Alameda has a waterfront that works!

NANCY McPeak

From: sodaguy <sodaguy@att.net>
Sent: Friday, May 25, 2018 10:29 AM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Plans for Alameda Marina Include a New Dockyard

@Low Rent spots need to be opened to be available to Alameda County Employees as well. @

Dear Alameda Planning Board,

Alameda Marina's latest Master Plan advocates for a modern, clean and efficient way to service boats which maintains a fully-functioning working waterfront while also opening up the property for all Alamedans. This merge of the working waterfront with new housing features affordable units, public open space, trails, and a new aquatic park where people can access the estuary like never before.

After more than two years of public review and changes, I recommend you move this plan forward so Alameda has a waterfront that works!

Sent from my T-Mobile 4G LTE Device

NANCY McPeak

From: cespmanager@eastshorepark.org
Sent: Friday, May 25, 2018 10:06 AM
To: NANCY McPeak
Cc: Shirley Dean
Subject: Citizens for East Shore Parks Letter and Maps for Planning Board Meeting, May 29, 2018, regarding Alameda Marina
Attachments: CESP Letter Alameda Marina 05.25.18.pdf; CESP Maps Attachment to Alameda Marina Letter 05.25.18.pdf

Dear Ms. McPeak,

Thank you so much for agreeing to distribute the attached letter and maps to each Planning Board Member and attach to their electronic agenda.

There are two attachments:

- CESP's letter for the May 29, 2018, Planning Board Meeting regarding the Alameda Marina Property at 1815 Clement Avenue.
- Comparative maps of Sea-Level Rise based on Our Coast, Our Future mapping tool.

Please send email confirmation that you have received these items to CESP email: cespmanager@eastshorepark.org

Again, thank you.



Shirley Dean,
Board President

Citizens for East Shore Parks
P.O. Box 6087
Albany, CA 94706
(510) 524 - 5000 (office)
(510) 526 - 3672 (fax)
www.eastshorepark.org
Like us on [Facebook](#)
Follow us on [Twitter](#)

[Opt-in to receive shoreline updates and announcements from CESP.](#)



Citizens for East Shore Parks

Mail: PO Box 6087, Albany, CA 94706 Office: 1604 Solano Avenue, Albany, CA 94707
Ph: (510) 524-5000 | Fax: (510) 526-3672 cespmanager@eastshorepark.org
www.eastshorepark.org

May 24, 2018

President David Mitchell and Planning Board Members Burton, Cavanaugh, Curtis, Köster,
Sullivan and Teague
Andrew Thomas, Assistant Community Development Director

Subject: The May 29, 2018 Planning Board Meeting to Consider the Recommendation to Approve the Master Plan, Density Bonus Application, and Environmental Impact Review for the Alameda Marina Property Located at 1815 Clement Avenue

Dear President Mitchell, Planning Board Members Burton, Cavanaugh, Curtis, Köster, Sullivan and Teague, and Mr. Thomas:

Citizens for East Shore Parks (CESP) welcomes the opportunity to comment on the important 44 acre Alameda Marina Project. While there are positive aspects to this proposed development, there are also issues that greatly concern us.

CESP is a 30-year plus, non-profit organization dedicated to the preservation and protection of parks and open space along San Francisco Bay's eastern shoreline stretching from the Carquinez Bridge to San Jose. In responding to establishing a resilient shoreline to meet the challenge of inevitable future sea-level rise, we strongly advocate protecting the shoreline through natural resources such as horizontal levees and marshes, rather than the construction of horizontal seawalls. However, since this project abuts Alameda Harbor, an existing active shipping lane, we understand the need to replace the 70-year old seawall, embankments and bulkheads that have become a public safety hazard with new construction. In this instance, our concerns center not on the replacement of the old seawall with a new one, but on the reliance of planning on a document that is five years old. Chapter 4.7, page 4.7-6 of the EIR states:

"The State of California has provided planning guidance for assessing and adapting to the impacts of sea level rise. The State's current guidance (Ocean Protection Council, 2013) incorporates the most recent scientific findings from the National Academy of Science National Research Council (NRC, 2012).

As you are aware, the State of California Sea-Level Guidance Document first released in 2010, was updated in 2013. On March 14, 2018, the California Ocean Protection Council adopted the second update of that document to reflect recent advances in ice loss and projections of sea-level rise. The 2018 action was based on a 2017 peer-reviewed document, *Rising Seas in California* which lists seven key findings as follows:

1. Scientific understanding of sea-level rise is advancing at a rapid pace. *Projections of future sea-level rise have increased substantially over the last few years, primarily due to*

new and improved understanding of mass loss from continental ice sheets. These sea-level rise projections will continue to change...

2. The direction of sea level change is clear. *Coastal California is already experiencing the early impacts of a rise sea level, including more extensive coastal flooding during storms, periodical tidal flooding and increased coastal erosion.*
3. The rate of ice loss from the Greenland and Antarctic Ice Sheets is increasing. *These ice sheets will soon become the primary contributor to global sea-level rise.*
4. New scientific evidence has highlighted the potential for extreme sea-level rise. *If green house gas emission continue unabated, key glaciological processes could cross thresholds that lead to rapidly accelerating and effectively irreversible ice loss.*
5. Probabilities of specific sea-level increases can inform decisions.
6. Current policy decisions are shaping our coastal future. *Before 2050, differences in sea-level rise projections under different emissions scenarios are minor, but they diverge significantly past mid-century....*
7. Waiting for scientific certainty is neither a safe nor prudent option. *High confidence in projections of sea-level rise over the next three decades can inform preparedness efforts, adaptation actions and hazard mitigation undertaken today, and prevent much greater losses than will occur if action is not taken. Consideration of high and even extreme sea levels in decisions with implications past 2050 is needed to safeguard the people and resources of coastal California.*

The report concludes:

In summary, the current pace of global sea-level rise (1.2 inches per decade) is already impacting California's coastline. New ice-sheet projections suggest the rate of rise could accelerate sharply later in this century, with the potential for two meters (6.6 feet) or more of total sea-level rise by 2100. While the uncertainty in these projections remains high, the risk is not negligible given the stakes to future society, development, and infrastructure.

The EIR for the project makes several statements regarding how sea-level rise is to be addressed within the project such as

Proposed elevations of the public access areas and proposed building foundations will be established to provide built-in protection against a minimum of 36 inches of sea level rise. Shoreline design will also accommodate future adaptive measures for potential future sea level rise in excess of 36 inches. Adequate land and right of way shall be preserved along the shoreline to accommodate further elevated shorelines and/or floodwalls in the future should it be necessary to further manage and adapt to sea level rise.¹

The Master Plan for the proposed project is based on the scenario of 3 ft of sea-level rise. However, the likely scenario is a broad range of 3 ft to 6.5 ft, with a potential up to 10 ft by the year 2100, numbers which are dependent upon the rate of green house gas emissions and subsequent Arctic and Antarctic ice loss. CESP questions planning which is based on the lowest outcome. For the sake of all concerned and the significant investment which will be required of existing and new businesses, new residents, the developer, and the City, the Master Plan should evaluate the full range of the likely scenario as is recommended by the California Department of Natural Resources.

¹ EIR, Chapter 5, Infrastructure Plan, 5.2 Flood and Sea Level Rise Protection

While Alameda's Mixed Use Zoning District requires that the Master Plan include at least three uses, one of which must be public open space, it is clear that the "adaptive measures for potential future sea level rise" will include at least part of the 3.59 acres of public open space, including the expansion of the Bay Trail. Both the Master Plan and the Environmental Impact Report ignore or dismiss what the impact of the loss of public open space will be, in such an eventuality. In the EIR discussion of Impact HYD-7 regarding exposure of people or structures to the impacts of sea level rise, it is stated:

The project would also have 50-to 100-foot-wide corridors along the shorelines reserved for the Bay Trail and which can be used for protection measures and adaptive measures to address climate change. All residential structures within the project site would be located at or above the 100-year tidal elevation plus 36 inches for sea level rise considerations. Shoreline design would also accommodate future adaptive measures for potential future sea level rise in excess of 35 inches. Future adaptive measures would involve expanding the coastal region of the site with raised levees or floodwalls.²

Eight important questions need to be addressed in the documents before the Planning Board at this time. These include:

1. While the developer and City seem to agree that the proposed residential units are necessary to pay for the initial replacement of the seawall, embankments and bulkheads, who pays for the on-going maintenance of these items? Businesses and residents within the project? Or the city of Alameda through fees/bonds/taxes?
2. Who pays for the adaptive measures, should the initial projection of 3 ft prove incorrect? Businesses and residents within the project? Or the city of Alameda?
3. How will the City replace any loss of public open space that might occur should the current Master Plan prove incorrect, including expansion of the Bay Trail and the loss of some part of the \$7.5 million in anticipated revenue to the Alameda Recreation and Parks Department.
4. If "expanding the coastal region of the site with raised levees or floodwalls" is an adaptive measure, will that mean a loss of existing maritime businesses, new businesses and new residential units, and what will that mean to the density bonus application?
5. What is the anticipated effect of a sea-level rise in excess of 3 ft and a 100-yr flood tide on the 180,972 sq ft for Maritime Commercial Area Improvements, and the revenue and reinvestment into infrastructure to support those improvements?
6. The Master Plan contemplates a phased development, presumably to accommodate the developer's financing structure. Why shouldn't the city of Alameda require that the developer construct and fully complete the replacement of the seawall, embankments and bulkheads for the entire project, giving priority to the Public Trust Lands and those areas that pose the highest public safety problems before proceeding with other aspects of the proposed plan? CESP believes this is an essential requirement for this and other such projects that involve the construction of seawalls.
7. Since the seawall and its component parts line the Alameda Harbor and since that shipping lane lies between two active earthquake faults, the San Andreas and Hayward Fault, there should be a requirement within the Master Plan that before any agreement

² Chapter 4, Environmental Setting, Impacts, and Mitigation Measures, Chapter 4.7 Hydrology and Water Quality, Impact HYD-7: pg 4.7-26

between the City and the developer, there will be an independent review of construction standards. The independent review would be managed by the city of Alameda, with the consultant selected by and reporting to the City.

8. The Master Plan should require that the developer include in all documents for residential property sales and leases of dwelling units information regarding possibilities, costs, and implementation of any adaptive measures required by sea-level rise over 3 ft. Such notification is similar to State requirements when individuals purchase property within the Alquist-Priolo Earthquake Zone.

In the light of these questions and others, CESP requests that the Planning Board take no action on the approvals before you. You need to take the time to find answers.

The city of Alameda, founded on an island in San Francisco Bay, faces a unique challenge in responding to climate change. CESP believes that the city of Alameda is committed to environmentally sound planning, and the process to approve plans and fashion development agreements for projects like the Alameda Marina proposal is an opportunity to get it right. CESP trusts that you will do just that.

Thank you in advance for your consideration of these comments,



Shirley Dean,
Board President



Robert Cheasty,
Executive Director

Attachments for Comparison (in separate file):

Map 1: Our Coast, Our Future, Sea-Level Rise, 3 ft, 100-year flood

Map 2: Our Coast, Our Future, Sea-Level Rise, 6.5 ft, 100-year flood

Information
Map

3 Choose a topic

Roofing
Roofing

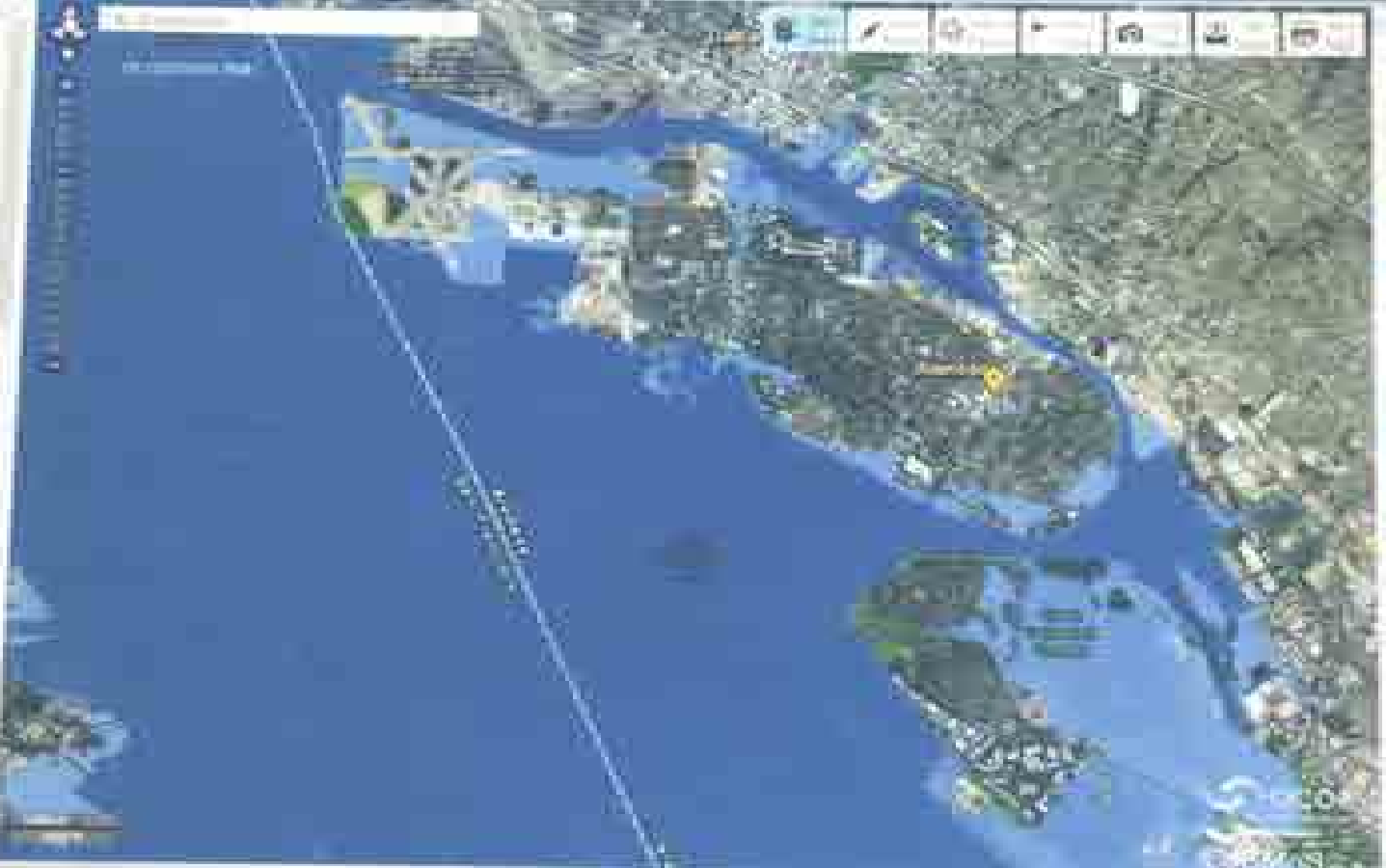
3 Choose an amount of sea level rise

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3 Choose an event

3 Choose an event



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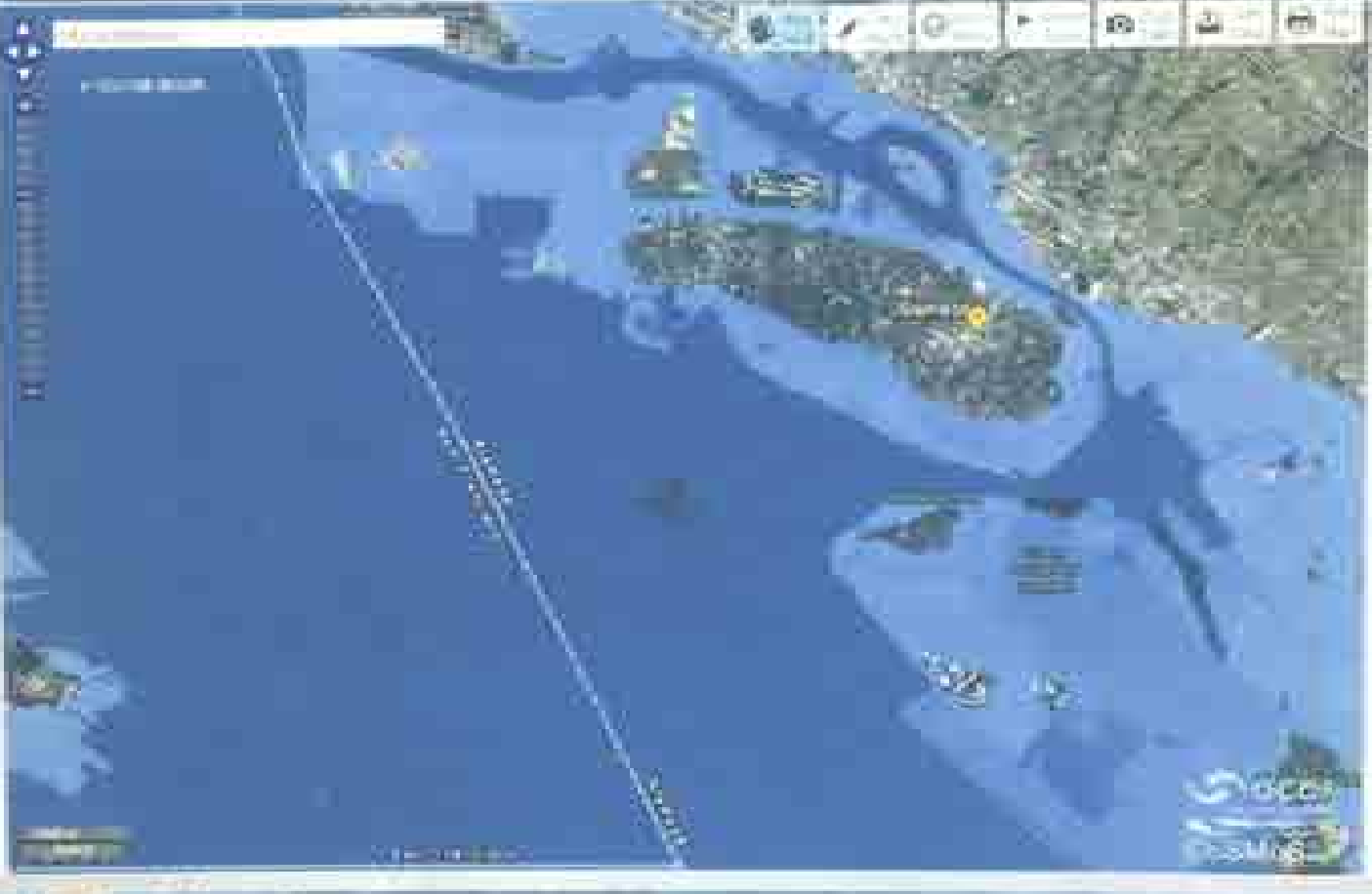
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Map Attribution

NANCY McPeak

From: Patsy Baer <2baers@att.net>
Sent: Friday, May 25, 2018 1:27 PM
To: NANCY McPeak
Subject: Alameda Marina

Dear Alameda Planning Board,

The percentage of affordable housing for this project is not high enough -103 affordable housing units out of a total of 760. It needs to be 25% of the total to serve the community.

We should be using this property to expand our maritime and blue economies and adding convenient services to the north coast communities that will reduce the need for automobiles.

Bay West's project does not include enough parking to meet the needs of the 760 new units, visitors, recreational boaters who have vessels in the marina, and users of the planned recreational facilities for this development.

If this property is to be developed, its needs should be more balanced.
Please consider these needs for the general community in the Master Plan on May 29th.

Sincerely,
Patsy Baer

NANCY McPeak

From: Allen Michaan <amichaan@michaans.com>
Sent: Friday, May 25, 2018 1:36 PM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Plans for Alameda Marina Include a New Dockyard

Dear Alameda Planning Board,

Alameda Marina's latest Master Plan advocates for a modern, clean and efficient way to service boats which maintains a fully-functioning working waterfront while also opening up the property for all Alamedans. This merge of the working waterfront with new housing features affordable units, public open space, trails, and a new aquatic park where people can access the estuary like never before.

After more than two years of public review and changes, I recommend you move this plan forward so Alameda has a waterfront that works!

Thank you,
Allen Michaan

NANCY McPeak

From: jimsweeney2@comcast.net
Sent: Sunday, May 27, 2018 4:39 PM
To: Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; NANCY McPeak
Subject: Regarding May 29th Item 7A: Master Plan, Density Bonus Application, and Environmental Impact Report for the Alameda Marina.

Dear Alameda Planning Board,

I think it critically important that we preserve this shoreline and retain this location as a full service boatyard which has helped maintain the history of the marina industry in Alameda.

As such it will not only help retain and foster historic maritime business and a variety of jobs, but will help traffic because of less housing units. We have a serious and rapidly increasing traffic problem in Alameda. The Marina Master Plan provision for 760 units of housing, of which only 103 are affordable, is excessive. The 103 affordable units amount to tokenism to appease the public to accept housing that will crowd out a full service boatyard.

Further, This location is a bad location to add substantial housing, given its' historic character and compatibility with the other businesses along Clement Avenue and nearby residential neighbors. Construction of 800 units of housing and supporting businesses has begun at Alameda Point, a vastly more preferable location for housing. The City Council declined to approve the 589 units of housing proposed for Encinal Terminals despite its' location being far more compatible with surrounding businesses, residential neighborhoods, and traffic flow, than that of the Marina Master Plan.

I urge your careful consideration of all the factors involved with view towards providing for the best interests of the City in sustaining our maritime uses and rich historical function.

Sincerely,
Jim Sweeney



(510) 595-4690
P.O. Box 2732
Alameda, CA 94501

Dear Planning Board Members:

Board of Directors
Brian McGuire
President

Thank you for the opportunity to comment on the Master Plan proposal for Alameda Marina. Bike Walk Alameda's board and advocacy committee have reviewed the plans. We have provided input on the public space and circulation elements of the plan as it has been developed. Revelopment of this site is an important opportunity to improve access, circulation, and safety for people who walk and bike along the Northern Waterfront.

Donna Eyestone
Secretary

The addition of a new, high quality Bay Trail segment throughout the project will be a great improvement over the currently inaccessible site that provides no real recreational access to the waterfront. Connecting this site to the Grand Marina site through the current boat ramp area will be a wonderful change from the status quo. We appreciate that the plan has evolved to the point where the Bay Trail stays along the waterfront, rather than detouring around the commercial component.

Denyse Trepanier
Treasurer

Lucy Gigli

Cyndy Johnson

Pat Potter

Perhaps most importantly, this project provides the perfect opportunity to build the Clement Avenue of the future, one that serves all users safely and efficiently. The improvement of the pedestrian experience and the inclusion of separated, safe bicycling facilities is critical. Seamlessly connecting to the miles of protected bikeways being built from future jobs and transit at Seaplane Lagoon all the way to Fruitvale BART is a must. A fully protected Cross Alameda Trail is how people will feel safe enough to have genuine transportation choices and leave the car behind more often.

Kristen Leckie

We do believe that you can strengthen elements of the plan further. We suggest that a full 16 foot Bay Trail be constructed throughout. The current 12 foot section adjacent to the dry boat storage is insufficient given the complicated mix of uses occurring at that location. Similarly, the 12 foot section between the townhomes connecting Clement Avenue to the graving dock may make the trail feel more like a front porch than a public access point.

We feel that there should be a stronger connection for bikes and pedestrians through the townhomes between corner of the Bay Trail near the stacked flats component (where the trail turns north towards the water) and the southern tip of the graving dock. Along Clement Avenue, the buffer between the potential cycle track and the parking lane should be widened where feasible. Adjacent to buildings 16 and 27, the buffer between the cycle track and the westbound vehicle lane must be widened due to the lack of parking protection. We appreciate the changes that have already been made to improve the plan, and look forward to refining the details as it moves along.

Respectfully,

Brian McGuire

NANCY McPeak

From: Brian McGuire <brianrmcguire@gmail.com>
Sent: Sunday, May 27, 2018 10:54 PM
To: David Mitchell; Sandy Sullivan; Ronald Curtis; David Burton; Alan Teague; Jeffrey Cavanaugh; ANDREW THOMAS; NANCY McPeak
Cc: Trish Spencer; Malia Vella; Marilyn Ezzy Ashcraft; Frank Matarrese; Jim Oddie; Liz Warmerdam; DEBBIE POTTER; LARA WEISIGER
Subject: PB Item 7-A: Alameda Marina
Attachments: Item 7-A_ Alameda Marina comments.pdf

Dear Planning Board Members:

Thank you for considering my personal comments on the Alameda Marina Master Plan Application. As a sailor, employee of Svendsen's Marine, affordable housing advocate, and Bike Walk Alameda President, I have a unique interest in how the Alameda Marina is redeveloped. I want this project to be a success. The community desperately needs the attractive mix of housing and infrastructure investment that the Master Plan provides. Regrettably, the plan falls short in providing the ingredients necessary to host a full service boatyard. The project is likely to move forward at the City Council level. Think strategically. You should demand improvements to the maritime commercial core that will help the economic and cultural resource that is our estuary boating community thrive for the next fifty years.

We should embrace the 103 affordable units that the project provides. The healthy mix of market rate apartments and townhomes will be more attainable to moderate income Alamedans than at recent developments. This housing supply will reduce pressure on a problem we have neglected for two generations. We can not solve our housing shortage overnight, but we have to commit to doing what we can to address the crisis of our time. **The affordable housing plan language needs to be corrected to require ALL affordable units be built in "general size and configuration" to the market rate units.** In order to make the stacked flats meet the "missing middle" housing that you have been working to provide, the square foot range should be reduced from the current 1600-2200 square feet. 1200-1800 would be more appropriate if there is to be any hope of affordability.

The Master Plan fails at delivering a full service boatyard at the the Alameda Marina that the community has depended on for decades. With over 3,000 boats in the estuary, Svendsen's Boatworks has served as the hub of maintenance and repair work for a thriving ecosystem of marine professionals since 1963. In 2017, the boatyard at Alameda Marina represented about two thirds of the capacity in the estuary (Grand Marina approx. 15, British Marine approx. 6). Losing this capacity will have serious consequences for the viability of the "Dockyard" commercial plan presented by the developer and, more importantly, for the health of the boating community in the estuary going forward. Today, sailing is attainable to a wide economic range of people, even me! Permanently losing two thirds of repair space will ensure the estuary boating life is only in reach of high income people, just like our housing.

The Master Plan does have some innovative ideas on what the maritime commercial area could be. Sadly, staff appears to have given up on the possibility of having a full service boatyard at Alameda Marina. The investment required, combined with the regulatory challenges a new boatyard operator would face are not insignificant. The investment can largely be met by having the Master Plan require the "boatyard/flex space" in the plan to be constructed with the elements necessary for a successful boatyard. This investment can be paid off over the life of a long term agreement with a boatyard operator.

After two years of saying that a boatyard will be an approved use, staff is now saying the regulatory hurdles are too high and we should accept what the Master Plan has to offer. You should demand that the Master Plan do what it takes to make a real boatyard viable, and demand that staff and the developer show their work before considering anything less than full service. I have yet to see it. The elements need to be written into the Master Plan. Just like

repairing the seawall, the Master Plan is the only vehicle that can provide the resources and incentives to meet this important community need. **Are the travel lift rails going to be preserved and rehabilitated? Will you require that the concrete pad for the boatyard be built strong enough to handle a loaded travel lift? Should you require a water retention and filtration system be built in order to meet the water quality demands a regulator will place on a boatyard operator?**

The Master Plan does just enough to preserve the “dry boat” sailing community that has relied on this site for access to the sailor’s playground the estuary and S.F. Bay provide. There could always be more and I do worry that some larger boats will be left without a home. It is important to make sure the Master Plan commits to dredging the area near the proposed new hoist before decommissioning the current hoist and storage area.

I encourage you to add the appropriate conditions on this Master Plan and then approve a recommendation to the City Council. By working constructively and then supporting the appropriate plan, you will have the credibility to expect the City Council and developer to include what you know is required for a successful project. If you instead reject the proposal out of hand, you risk having Council pass the project without making the plan better.

Thank you,

Brian McGuire
Renter of homes and sailboats

May 27, 2018

Ronald Curtis
Jeffrey Cavanaugh
David Mitchell
Sandy Sullivan
Alan Teague

Planning Board Meeting May 29, 2018

Agenda Item 7A - Recommendation to Approve the Master Plan, Density Bonus Application, and Environmental Impact Report for the Alameda Marina.

Dear Planning Board Members;

Placing the entire Alameda Marina land on the Housing Element in 2012 was a mistake that has long time range ramification for Alameda and beyond our shores.

Summary of Arguments:

- 1) Loss of the core of our maritime business, the boatyard, that provided much needed services to the entire Bay Area including loss of a major revenue producing industry to the tax base for the City.
- 2) Loss of the businesses that are located in the Marina plus downstream businesses that were supported by the business community at the Alameda Marina.
- 3) Loss of an important part of Alameda's maritime history.
- 4) Traffic problems that cannot be mitigated away.

The Bay West Master Plan is not in the best interest of our city. When the Alameda Marina was placed on the Housing Element in 2012 it was described as vacant and underutilized. From a Google map view of the property, that may appear to be true. A more extensive research into life at the Marina would have discovered a thriving community that reached well beyond the boundaries of Alameda.

Explanation of Arguments 1 through 4

- 1) Loss of the core of our maritime business, the boatyard, that provided much needed services to the entire Bay Area including loss of a major revenue producing industry to the tax base for the City.

Contrary to Bay West's statement, Svendsen's Boat Works provided a full service boatyard that was necessary for the entire boating community in the Bay Area. Alameda is the home to the largest number of boat slips in Northern California and the 2nd largest in the state. A boater could visit the Alameda Marina for service and never have to go to another place; they received "full service". It was all happening here. That's why Alameda is the home to so many sailboats, 6 sailing clubs and sailing schools for beginners.

The boatyard has the necessary equipment for serving boats of many sizes. With 3 hoists, boats could be stored in dry storage on land, and placed back into the water in a short period of time; necessary for race days on the Bay. Svendsen's provided travel lifts to place larger boats into the water. Only 50 to 60 dry storage spaces and one hoist are being planned for. Presently there are approximately 300 dry stored boats at the Marina that need to find a new place outside of Alameda. The Marina also had a "traveler hoist" that could place larger boats into the Estuary.

There was an elevator necessary for servicing Alameda's 40+ houseboat community. The elevator is required for raising houseboats out of the water for underwater hull repairs. This elevator will be removed. Plans for Bay Ship & Yacht to make these repairs in the future is very iffy. BS&Y says it will be necessary to work on 3 houseboats at a time and it will be very expensive. I guess houseboat owners will have to arrange for 3 of them to have leaks at the same time. Houseboats cannot travel through the choppy waters of the Bay to go to another city.

Boat bottom (hull) repairs were a major part of the services at Svendsen's. A boat owner could hire Svendsen's to do the work or rent a space and do the work themselves. This made the job more affordable for the boat owner. The hull of boats need to be scraped and repainted every 2-3 year to remove crud from the bottoms of the boats that sit in the water. This service is not provided in Bay West's Master Plan. The staff report states the boaters can go to other cities for hull work, telling our boating community to take their money to another city because we don't want to provide the service here. In the past, hull work on boats created a polluting problem. Today there are new ways of scraping and painting hulls that are more environmentally safe. The Marina boatyard also had a water retention and filtering system that would capture water from the boatyard area to keep it from running off into the Estuary. Svendsen's had been certified as not polluting the Estuary by Bay Keeper organization since 2012. The water filtration system is being removed by Bay West.



Original foot print of Svendsen's Marine Services

Svendsen's Boatyard was very successful and was always busy as are all boatyards in the Bay Area. There is little doubt of the need for a full service boatyard at the Alameda Marin as it was also one of the largest. There is a continuing need for a full service boatyard here in Alameda. Brooklyn Basin is planning slips for 200 boats and Encinal Terminals is planning for boat slips in the Alaskan Basin. The assumption for these new slips would be that some of the new people moving into the new developments are doing so because of their proximity to a boating community. Since Alameda has had a very successful boatyard, it's safe to assume the service could be returned here without having to tell people to take their business to another city.

The space provided for in the Master Plan is not large enough to provide a viable boatyard. Alameda needs to preserve the space shown above to return the boatyard to Alameda and preserve our maritime business before it is completely destroyed. Alameda's maritime industry depends on it.

2) Loss of the businesses that are located in the Marina plus downstream businesses that were supported by the business community at the Alameda Marina.

The Alameda Marina has been home to 60 business and 284 jobs paying middle class wages. Many of the businesses were related to the maritime industry but many were not. It's easy to think of the business area at the Marina as an island of activity, but all those businesses did business with other companies here in town. The trickle-down loss of taxes at these other business will be felt throughout Alameda.

Many of the businesses previously located at Alameda Marina have already left Alameda as they have been forced out by Pacific Shops, Svendsen's Boat Works being the largest. Svendsen's Boat Works had been in the top 25 sales tax producers for Alameda. In the Alameda Economic Development Strategic Plan, it states "maritime (blue) industries with access to the bay and estuary, Alameda is a competitive location for businesses in the maritime and "blue tech" industries." The problems is, the Bay West Master Plan will not return a viable maritime industry to the Alameda Marina. Many boat owners are already relocating their boats to other cities where they can get the services they need.

In the housing sector portion of the master plan, they allude to the need to provide housing along the northern waterfront in conjunction with strategies 3.2 and 3.3 which are in the blue economy and maritime sector. 3.2 and 3.3 state: *3.2: Update land use and zoning regulations for waterfront sites in Alameda to include objective standards that ensure development projects preserve economically-viable water-dependent maritime uses and maintain adequate access to the waterfront for business operations. 3.3: By 2019, identify waterfront locations and sites best-positioned to serve and retain maritime businesses, and review the adequacy of the existing zoning for these locations to support maritime industrial uses in the future. Provide recommendations on modifications to zoning to preserve these uses by 2020.*

From Blue Tech and Maritime. As a waterfront community with access to the bay and estuary, Alameda is a competitive location for businesses in the maritime and "blue tech" industries. This includes industrial/commercial maritime businesses that provide ship building and repair, recreational marinas, and "blue tech" research and development, engineering, software, and advanced manufacturing. Alameda should actively prioritize and pursue opportunities to support its existing maritime and blue tech businesses, recognizing that they are a core component of Alameda's economy and community identity. Some of the suggested strategies include:

- *Creating and implementing an action plan for job growth in the maritime sector,*
- *Reviewing waterfront land use and zoning regulations to preserve these uses, and*
- *Increasing workforce preparedness for occupations in these sectors.*

As housing developments take over the Estuary waterfront, there is no land left to build a "blue economy". Boatworks will provide zero business space. Shipways will provide zero business space. Encinal Terminals will provide only 10% commercial and, with the new plans to be submitted, commercial may be gone here also. Alameda Maria plans provide an area too small to rebuild the marine industry that is being lost, let alone build new.

3) Loss of an important part or Alameda's maritime history.

Every building presently at the Alameda Marina today is included in this 1946 image. Most people think of the Alameda Naval Air Station at Alameda Point as being the history of Alameda's military effort.



The WWII effort spanned the entire Northern Waterfront. Only Shipways, Alameda Marina, and the warehouses at Alameda Landing Waterfront are still standing. Shipways is totally being replaced with apartment buildings. Some of the warehouses at Alameda Landing Waterfront will be reused by Bay Ship & Yacht but many will be torn down. Only a few of the existing buildings at Alameda Marina are planned to be reused.

The loss of these buildings is a loss to Alameda's history that can never be recovered. Putting up a plaque that says "Here stood a historical building" sure doesn't convey the importance of what was done here. These buildings have been used for a thriving Marina boatyard, related maritime businesses, and non-maritime business and can continue to do so. Alameda has a terrible jobs to housing ratio. Keeping these buildings working for Alameda as a business community will benefit Alameda better than building more market rate homes.

4) Traffic problems that cannot be mitigated away.

Alameda Marina traffic cannot be considered as one development in isolation, yet it was in the EIR. The cumulative effects of traffic have not been adequately considered in any of the EIR's developed for the Northern Waterfront.

As all the major projected developments are located on the North and West sides of Alameda, all construction traffic will travel along Clement Ave, Alameda's truck route, from the Fruitvale Bridge to Clement, to Grand, to Buena Vista, to Sherman, to Atlantic Ave. and for some on to Alameda Point. Most of the projects are major developments that will last from 5 to 10 years. Not only will the traffic from existing and new residents, as units become occupied, cause additional congestion, they will have to travel with major truck traffic. The plans to add the Cross Alameda Bike Trail to Clement will add an additional element to the congestion.

Development Areas in Alameda as of January 2018

Priority Development Area							
Name of Development	Year to be Constructed	Number of Housing	Office/Commercial sq ft	# Very Low	# Low	# Medium	Notes
Alameda Landing (Phase 1)	2018	100	100,000	20	4	16	Under Construction - pending permit
Alameda Landing (Phase 2)	2018	89	100,000	1	1	1	Under Construction - completed permits
Alameda Landing (Phase 3)	2018-19	52		1	1	1	Under Construction
Del Monte	2017	31		10	10		Under Construction
Del Monte (Senior Housing)	2017-18	380 + 1		17	14	20	20 units missing in construction/Approved
Encinal Terminals	2018-20	182		18	1		Approved - pending law suits
Boatworks	TBA	182	10,000				Approved
Boatworks (Senior Housing)	TBA	1	10,000				Ground Floor construction - Approved
2100 Clement	TBA	11				1 or 2	Approved
Alameda Marina	2017-17	760	800,000 Marina	48	40	70	Approved - in default of agreement
Alameda Marina (Phase 2)	TBA-2018	760	Commercial, TBA	Marina and 94 units/units			Developer now requesting TBA Units
Alameda Marina (Phase 3)	TBA	589					589 units approved by city/county. May be lower
Alameda Marina (Phase 4)	TBA	706					15-17 acres developed in 3 years
Alameda Marina (Phase 5)	TBA	589	40,000 sq ft		200		Reserve 200 existing units (100,000 sq ft)
Alameda Marina (Phase 6)	TBA	761	apartments				waiting for planning
Alameda Marina (Phase 7)	TBA	875	Commercial, Retail				Development agreement changed to sublet
Alameda Marina (Phase 8)	TBA	20					Planning - Additional
Alameda Marina (Phase 9)	TBA	20					48 existing replaced with 88 new
Alameda Marina (Phase 10)	TBA	1,000 (approx)					**Must match units, single lot/s with common lot/block area
Total Units in Progress		4972					
Alameda Marina (Phase 11)	TBA		117 waterfront				Unit count may be lower per Planning Board
Alameda Marina (Phase 12)	TBA		40 waterfront				general study report/awaiting funding
Alameda Marina (Phase 13)							
Alameda Marina (Phase 14)							
Alameda Marina (Phase 15)							
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Alameda Marina (Phase 99)							
Alameda Marina (Phase 100)							
Total Future		74					No development plans right now
Total All Development Units		5046		100	100	100	
		Number is Approximate					* Actual # dependent on density
							** Best Case Scenario
							Alameda Average Population Unit = 2.57

Proposed Developments On Clement Ave.

Between Sherman St and Park St. (some completed or under construction)
 One and one half mile equals new 1408 units per mile adding to the density which is one of the highest in the Bay Area.

- Marina Shores 89
- Del Monte380
- Senior Housing 31
- Encinal Terminals589 (may come back less)
- Boatworks182
- 2100 Clement 52
- Alameda Marina 760
- Penzoil (future) 18 (could increase)
- Ron Goode (future) 11 (could increase)
- Total units 2112

The odds of each one of these units owning a car is a given. Many will add a second car per unit.

Ignored by the EIR was traffic impacts that would occur elsewhere on the island. The DEIR grossly underestimated the impacts at the Blanding/Park and Clement/Park intersections which were already studied to be at maximum with the EIR for Boatworks with only 182 units being added. The Boatworks EIR was studied long before any of the major developments on Clement Avenue were planned. The addition of the traffic light across the Park Street Bridge has

already backed traffic into Alameda during AM commute time. Present plans to synchronize the traffic lights on Park Street will help but will not mitigate the problem to an acceptable level.

Both South-bound traffic added to Clement Avenue and North-bound traffic added to the Posey Tube from 760 Alameda Marina units were not properly studied. The EIR properly states that Park Street traffic at Clement and Blanding cannot be mitigated to any acceptable level. Other intersections will experience the same problem. The EIR's continue to document traffic delay and the resulting impacts are grossly incorrect and below today's congestion levels. Some new residents will use bus, bikes, and ferries but not enough to make any appreciable difference in the congestion that will result from the Alameda Marina and the other planned developments.

South bound traffic that will be diverted from the 3 congested bridges to the San Leandro Bridge and down Doolittle Drive was ignored and not studied in the EIR. Congestion on city streets was not studied to any acceptable level. With all the developments being concentrated on the Northern side of Alameda, where the bridges and tube are located, traffic will back up across Alameda behind those already cued in line. Many will overflow onto other city streets to find a way to circumvent the lines. This was experienced recently when a sink hole closed the Posey Tube into Oakland.

The people of Alameda deserve to know the actual traffic levels that will result from a development before it is approved. We need to have a new study that will report the traffic service levels accurately. The Alameda Marina EIR does not do this.

Date of Hearing: May 29, 2018

Planning Board
City of Alameda
2263 Santa Clara Avenue
Alameda, CA 94501

Re: Item 7-A, Alameda Marina Project

Dear Planning Board,

You are being asked to approve the master plan, density bonus application, and environmental impact report for the Alameda Marina property located at 1815 Clement Avenue.

I recommend postponing a decision on the master plan until the climate adaptation measures are better addressed.

According to the proposed master plan, the elevation of the project will provide built-in protection against a minimum of 36 inches of sea level rise. In the event sea level rise exceeds 36 inches, an “adaptive management design strategy” will be implemented along the shoreline perimeter of the site. The plan states that “land and right of way shall be preserved along the shoreline to accommodate further elevated shorelines and/or floodwalls in the future should it be necessary.”

It’s good that the master plan attempts to address sea level rise, but unfortunately it relies on an out-of-date standard. Based on recent projections by the State of California (*Rising Seas in California, An Update On Sea Level Rise Science*, April 2017), the master plan should instead anticipate six feet of sea level rise.

The project should provide built-in protection against more than 36 inches of sea level rise. It will be less costly and more prudent to build it now rather than retrofitting later. The master plan gives no indication of who is responsible for upgrades, repairs and maintenance of the adaptive measures in perpetuity.

It is important to design the project to meet current sea level rise projections and climate change impacts and to know who will pay for any future needs to the shoreline infrastructure.

I urge you to ensure that these issues are included in the master plan before approving it.

Sincerely,

Irene Dieter



SPRAWLDEF

Sustainability, Parks, Recycling And Wildlife Legal Defense Fund

802 Balra Drive, El Cerrito, CA 94530
510 526-4362 www.sprawldef.org n.laforce@comcast.net

May 29, 2018

Planning Board
City of Alameda
2263 Santa Clara Avenue
Alameda, CA 94501

Re: Item 7-A, Alameda Marina Project

Dear Planning Board,

You are being asked to approve the master plan, density bonus application, and environmental impact report for the Alameda Marina property located at 1815 Clement Avenue.

SPRAWLDEF ecommends postponing a decision on the master plan until the future climate adaptation measures are better addressed.

The proposed elevation of the project will provide built-in protection against a minimum of 36 inches of sea level rise. In the event sea level rise exceeds 36 inches, an “adaptive management design strategy” will be implemented along the shoreline perimeter of the site. The plan states that “land and right of way shall be preserved along the shoreline to accommodate further elevated shorelines and/or floodwalls in the future should it be necessary.”

It is important to know what the strategy is at the outset of the project. Based on recent projections by the State of California (*Rising Seas in California, An Update On Sea Level Rise Science*, April 2017), the master plan’s 36-inch projection is out of date, and the master plan should instead anticipate six feet of sea level rise. The handling of expected storm surges is also of concern. The master plan gives no indication of who is responsible for upgrades, repair and maintenance of the adaptive measures in perpetuity.

For further consideration, the Bay Conservation and Development Commission is discussing what new standards to adopt for shoreline development based on sea level rise and climate impacts. The city is also developing a climate adaption plan. The city should utilize both documents and adopt a citywide climate adaptation master plan.

SPRAWLDEF also joins in the very detailed comments that Citizens for East Shore Parks have sent in. This project needs to go back to the drawing board.

Sincerely yours,

Norman La Force

Norman La Force
President

NANCY McPeak

From: Angela McIntyre <angelamcintyre@mindspring.com>
Sent: Monday, May 28, 2018 11:06 AM
To: NANCY McPeak; ANDREW THOMAS
Subject: Your approval of the Alameda Marina Project

Follow Up Flag: Follow up
Flag Status: Flagged

Dear City of Alameda Planning Department:

I am writing in support of granting Alameda Marina the ability to build on the area once known as Svendsen and associated land.

In the past, I, as a Barnhill Marina resident, was concerned that if the land was developed, Svendsen would no longer be available to us. My primary concern was that the homeowners, at Barnhill Marina, who have wood/fiberglass hulls on their floating homes would be without a source for repair or maintenance.

During the last few years, most of those homeowners of wood/fiberglass hull floating homes have had their hulls repaired or maintained by Svendsen. With the work that was done, our hulls should be good for several years!

In addition, I have been informed that Bay Ship, the company that purchased Svendsen, has the ability to continue to maintain/repair these hulls – albeit in groups, as opposed to being done individually.

This assurance has alleviated my concerns of what will happen to our floating homes, in the future, without Svendsen.

I now feel that the plans being submitted for development of the Alameda Marina should be approved. Pacific Shops, Inc., seems to have an in-depth knowledge of marinas and housing developments, they are familiar with Alameda, and seem to be dedicated in creating an exciting area that will enhance our beautiful Island.

Please vote for the adoption to this exciting new development.

Thank you for your consideration.

Angela McIntyre, Alameda resident since 1989
2394 Mariner Square Drive #112
Alameda, CA 94501
510-377-7733

NANCY McPeak

From: David Herrigel <dherrigel@gmail.com>
Sent: Monday, May 28, 2018 1:00 PM
To: Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; NANCY McPeak; spencer@alamedaca.gov; Malia Vella; Frank Matarrese; Marilyn Ezzy Ashcraft; Jim Oddie; LARA WEISIGER; Liz Warmerdam; City Manager; Janet Kern; info@sawwaction.org
Subject: No to Bay West Proposal at Alameda Marina

I am opposed to the BAY WEST Proposal for Alameda Marina. The Alameda Marina is a Bay-Area-Wide asset and the businesses housed there are vital to the greater Bay Area boating community. The Marina provides a site where scores of jobs are available for local residents. There are other places on Alameda where housing can be built.

The boatyard proposal is inadequate to support the existing boating community. Svendsens Marine was a prosperous and thriving business until poisonous relations with the developer forced the Svendsens family to sell. Bay Ship has closed operations simply because working with Bay West and Pacific Shops is so toxic. DOER Marine has already been forced out. The Bay Area boating community needs a yard at the Marina with travel lift and the elevator as it currently exists. Jobs have been lost to Svendsens being sold and the resulting closing of the yard. Those jobs should come back.

The proposed dry storage area is preposterously inadequate in every way. The number of dry storage spaces is a fraction of what is needed, but worse, the size of the spaces is so tiny that almost no boats currently in storage there will be able to continue. The Alameda Marina has the only...THE ONLY remaining three ton hoist between Los Angeles and Portland. The proposed plan will remove it. Dry boat storage is more environmentally friendly and more affordable than in-water storage. Bay West's plan may look good to someone who doesn't launch a boat, but it is in fact completely, preposterously inadequate.

The entire issue of parking and transportation to the Marina and within the Marina, both for boaters and residents is wildly underestimated. NO provision is made for boat-owner parking. None.

I am a former (rental) resident and current employee of a manufacturing based business at Alameda Point. I am also an avid boater and patron of many of the businesses at Alameda Marina. It pains me that many of my sailing associates no longer come to Alameda regularly as much of the incentive to do so has been steadily eroded by BAY WEST's poor management and aggressive development.

I am not insensitive to the issues of housing in Alameda; in fact I was forced to look to West Oakland for an affordable home to purchase 7 years ago. BAY WEST's proposal does nothing to address housing costs.

I strongly advise you to NOT APPROVE Bay West's Master Plan for the Alameda Marina.

--

David Herrigel
510-717-9271 [m]
dherrigel@gmail.com

NANCY McPeak

From: Dorothy Freeman <dfreeman@pacbell.net>
Sent: Monday, May 28, 2018 4:32 PM
To: Ronald Curtis; Jeffrey Cavanaugh; dmitchel@alamedaca.gov; Sandy Sullivan; Alan Teague
Cc: Trish Spencer; Frank Matarrese; Marilyn Ezzy Ashcraft; Jim Oddie; Malia Vella; LARA WEISIGER; Liz Warmerdam; NANCY McPeak
Subject: Agenda Item Item 6C Alameda Economic Development Strategic

May 28, 2018

Planning Board Meeting May 29, 2018

Agenda Item Item 6C
Alameda Economic Development Strategic Plan

To: Planning Board Members:
Ronald Curtis
Jeffrey Cavanaugh
David Mitchell
Sandy Sullivan
Alan Teague

Dear Planning Board Members;

This paragraph is from the Alameda Economic Development Strategic Plan
May 10, 2018 Prepared for: City of Alameda

3. Blue Tech and Maritime. As a waterfront community with access to the bay and estuary, Alameda is a competitive location for businesses in the maritime and “blue tech” industries. This includes industrial/commercial maritime businesses that provide ship building and repair, recreational marinas, and “blue tech” research and development, engineering, software, and advanced manufacturing. Alameda should actively prioritize and pursue opportunities to support its existing maritime and blue tech businesses, recognizing that they are a core component of Alameda’s economy and community identity. Some of the suggested strategies include: · Creating and implementing an action plan for job growth in the maritime sector, · Reviewing waterfront land use and zoning regulations to preserve these uses, and · Increasing work force preparedness for occupations in these sectors.

The following paragraph is from the San Francisco Business Times (Special supplement: Alameda)
May 4, 2018 Alameda geography and history attract blue economy innovators.
Full page follows.

Another strong blue economy sector is the recreational maritime subcategory, which includes businesses related to commercial marinas, yacht sales and recreational use of the water. Alameda is a prime access point to enjoy the largest recreational open space in the region: the San Francisco Bay. Alameda has nearly 3,400 recreational boat slips--the most of any Northern California city. and with the growing regional population, more people will be looking towards the limited supply of waterfront access for recreation and leisure.

THE BLUE ECONOMY

Alameda geography and history attract blue economy innovators

Long before the Bay Bridge and the San Francisco Bay Area, Alameda Island was a natural harbor and a center of maritime activity. From San Francisco and other ports across the bay, East Bay's Alameda Island was a natural harbor and a center of maritime activity. From San Francisco and other ports across the bay, East Bay's Alameda Island was a natural harbor and a center of maritime activity.

Alameda is in the center of one of the world's largest (and) most shipping networks. It's north shore is just a few minutes from the Port of Oakland, the country's 18th largest container port. Its south shore also offers more than 20 miles of shoreline.

Today, San Francisco's international cruise terminal economy has brought more than 100,000 cruise ship passengers to the island annually, making it one of the most visited islands in the Bay Area. The island's location, history and infrastructure make it an ideal site for maritime innovation. The island's location, history and infrastructure make it an ideal site for maritime innovation.

LOOKING TO THE FUTURE

Alameda has served the maritime industry well for over a century. The island's infrastructure, including its deep water harbor, makes it an ideal site for maritime innovation. The island's infrastructure, including its deep water harbor, makes it an ideal site for maritime innovation.

maritime industry in the 2000s has continued to grow. The island's location, history and infrastructure make it an ideal site for maritime innovation.

Another strong indicator of the island's maritime history is the island's business history. The island's location, history and infrastructure make it an ideal site for maritime innovation.

But such is the advantage of the island's location, history and infrastructure make it an ideal site for maritime innovation.

Alameda's location, history and infrastructure make it an ideal site for maritime innovation. The island's location, history and infrastructure make it an ideal site for maritime innovation.

Alameda incubator breathes life into blue economy

Alameda will soon be home to a host of new and innovative maritime businesses. The island's location, history and infrastructure make it an ideal site for maritime innovation.

Alameda and San Francisco share a maritime history. Today, however, the Alameda Island is a maritime business hub. It's Alameda, not San Francisco, that's the maritime business hub.

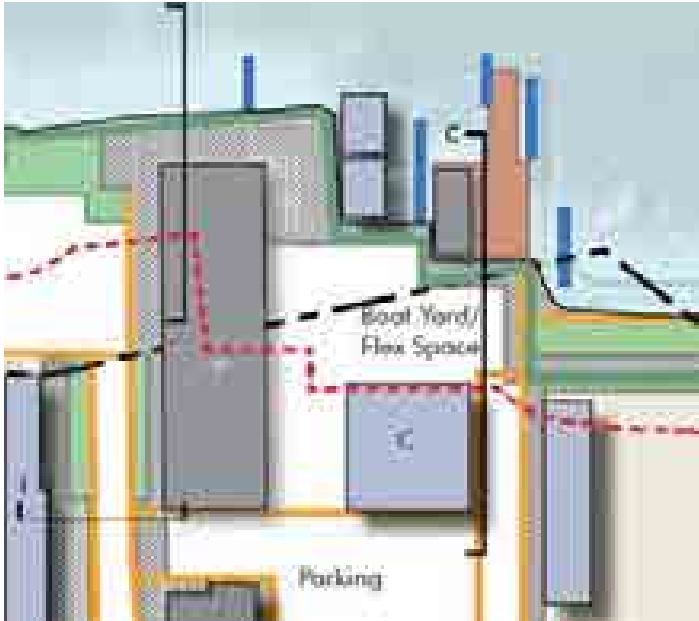


The Alameda Marina has always been a major contributor to the success of the maritime business in Alameda. The loss of Svendsen's Boatyard, at the Marina, has created a situation where much of Alameda's core maritime industry will disappear.

In the 2016 Master Plan, a boatyard was not mentioned in the master plan. At community meetings, attendees were told a boatyard did not fit into the vision of the housing development plan. After the meeting, the boating community reacted to the possible loss of a working boatyard at Alameda. The community was further told, contrary to the above studies, that recreational boating was not a priority to people as it had been in the past so a

boatyard would be a losing industry at the Marina.

The 2018 Master Plan has tried to respond to the outcry regarding the loss of the boatyard. But the approximately .9 acre boatyard that is proposed in the master plan for the Alameda Marina development is not sufficient to sustain a viable boatyard. Contrary to public statements this flex space and seaward provisions will not provide Alameda's boating community with all the services needed and will not return the economic tax base that the Alameda Marina has provided in the past.



Bay West Flex Space

The following proposal by the boating community, has been presented to Alameda City staff. (See Attachment) This proposal will return the space needed to provide a full working boatyard to the Alameda Marina. It will also be an economic win for both the developer and the City of Alameda. It will not interfere with the housing development in any way but will free up Tide Land area for better use by the developer.

A viable business has to provide the services that are needed by the customer. If it does not, it will fail. Many of Alameda's boating community are already moving their boat out of Alameda. This will continue unless the needed services are returned to the community. With a full working boatyard, Alameda will not have to tell it's resident boat owners to take their business, and their dollars to another city.



Space for full working boatyard

Residential boating has always been a part of Alameda's character. Retaining a working boatyard will help retain this part of the "blue economy" in Alameda. The downstream economy supported by a full working boatyard in Alameda will flourish also, instead of leaving the island.

Respectfully
Dorothy Freeman

cc: Mayor Spencer
Vice Mayor Vella
Councilmember Matarrese
Councilmember Ashcraft
Councilmember Oddie
Acting City Manager Warmerdam
PB Staff: Nandy McPeak
City Clerk Weisiger

Attachment: Proposal For Retaining An Alameda Marina Boatyard

Why do we need a replacement tenant for Svendsen's?

- Over 3600 boats in Alameda need maintenance.
- Boats are also brought to Alameda from other areas for repair.
- The elevator on Tidelands for floating homes must be maintained.
- Good paying jobs needed in Alameda must be maintained.

- Other companies that provide services to the boatyard should be protected.

The City of Alameda has expressed the desire to retain a working boatyard at the Alameda Marina. At this point in time there is no longer a working boatyard because Svendsen's Boatyard has been sold and has shut down facilities at the Alameda Marina and moved to Richmond.

This article is a prospectus of what it would take to reestablish a working boatyard at the Marina. Also, this paper addresses only the possibility of a boatyard, but does not address other items such as the dry storage and additional hoists necessary to serve the Alameda boating community at Alameda Marina.

Currently all boatyards in the SF Bay Area are operating at capacity. Waiting lists are common.

We believe the lease for Svendsen's is current through next June.

Alameda has approximately 3600 boat slips which are mostly occupied. Alameda has 6 active yacht clubs: Island Yacht Club, Alameda Yacht Club, Aeolian Yacht Club, Ballena Bay Yacht Club, Oakland Yacht Club, and Encinal Yacht Club. Four of these are on the Estuary waterfront. Alameda also has the Alameda Community Sailing Center (ACSC) which instructs young people who are learning to sail. ACSC has 500 young people and 12 boats. They also teach sailing to adults. There are 4 sailing charter/sailing schools on the Estuary (Club Nautique has approximately 40 boats in it's fleet).

Alameda is the second largest sailing community in California, second in size only to Marina Del Rey. The Encinal Terminals project is proposing an additional 160 slips.

We have not only lost a boatyard, which has relocated to Richmond, we are losing the jobs and possibly the associated boating businesses in Alameda. The two remaining boatyards, only one Alameda, combined only have room for approximately 30 haul outs. This capacity is too small to meet demand and will force local boaters to take their boats elsewhere. Point of information, one yard of the two in Marina Del Rey has 30 haul outs.

Boatyard Prospectus

The following is a prospectus of what would be needed to reestablish the working boatyard at the Alameda Marina.

Figure 1 shows the foot print of the preferred location East of the Alameda Marina building. The total square footage of the preferred plan is 129,839.8 square feet per Google Earth calculation (2.98 acre). The building number #12 would be repurposed for the active Marina Boatyard operations and provide 13,778 square footage of indoor space.

Fig 1



The following figure #2 shows why this facility and the surrounding area is the preferred location for the working Marina due to the location of existing infrastructure. If the working Marina were located at a different location, existing infrastructure would have to be recreated at great expense.

Fig 2



1. Main building #12: Interior includes chandlery, rig shop, metal shop, and storage at a total of 13,778 sq ft. This building location is large enough to provide all necessary support plus space to rent for indoor boat repairs.
2. Hoist & travel lift included at the waterfront. The travel lift is required to provide service to the existing Alameda house boat community.
3. Slips at waters edge. These slips are necessary to provide parking for boats waiting for the hoist and for overnight boat customer parking.
4. Includes existing paint shed.
5. This location provides use of the existing water trapping system. By law all waters from the initial washing of hauled out boat and drainage water associated with boat's refinishing must be captured and filtered. This water trapping system is critical. It prevents polluted water from refinishing work entering the Estuary and Bay. This space would be preserved for the type of boat repairs requiring extensive boat haul work. This system also captures storm water runoff and filters it and is also regulated. Baykeeper has offered their

guidance in this area.

This area also includes a keel trench so boats can be lowered down to ground level to work on. This is a safety issue that helps eliminate falls from high levels.

6. Access through Shiller St. existing gate. The existing Shiller Street entrance would provide the most direct entrance into the boatyard.

7. Provides parking. The parking spaces on the East of the building #12 would provide employee/customer vehicle and customer boat parking including yard space available to rent to persons working on boats. Only boat repair work that does not cause pollution would be allowed in this area. Examples are engine replacement, sail and mast placement, interior work, electronic systems, etc.

8. A necessary consideration is that marina work needs to be located away from the new residence locations, especially those to the East that will be the higher priced units. Placing the more polluting and more noisy work to the West of building #12 and East of building #19, which will be retained, provides a buffer zone isolating this work from the residential areas. Noise from the East side of building #12 should be minimal but can be restricted by day and time.

The center of building #12 is located 945' from the West side of the property and 1215' from the East side of the property, placing this proposed boatyard near the center of the present Alameda Marina property.

There are obstacles to be overcome to reestablishing a working boatyard at Alameda Marina. The following are just a few of them.

a. Lack of stability in building plans. The lack of specific plans on what is happening to the property in the next couple years would make it very difficult to find a new business to lease the proposed boatyard property at this time.

b. SAWW members have talked to several individuals who might be interested in providing a working boatyard at the Alameda Marina. Some of the issues are listed here regarding negative aspects about the possibility of taking over the boatyard in the Marina. Rebuttal notes have been included following each comment.

1. Regardless of the documented environmental issues with the soil on the property, it becomes a liability to any boatyard operator who might lease the space. The land owner can always come back and say to the operator that they are responsible for any contamination, whether or not the yard operator was responsible for the existing contamination.

See item a. above

2. The new operator could not buy existing permits. The hardest permit to obtain (which a new operator would not even know if they could obtain it until all improvements were made), is the air quality permit. See item 8 above.

3. The economics of the boatyard business are not that good. Millennials are not buying boats and there is not enough business to support another boatyard even with the number of boats we have in the bay. Marina Del Rey only had 2 yards for all the boats down in that area.

Note: As stated earlier, all working boatyards in the Bay Area are at capacity. Marina Del Ray is a very compact marina. The boats are not spread out over a large area like the San Francisco Bay. It is

approximately 1 mile by 1.5 miles with housing and commercial land spaces in between the slip placements. The nearest other marina to Marina Del Rey is Long Beach 19 miles away so Marina Del Rey is responsible only to serve the local marina community.

Alameda citizens, the boating community and the Alameda city government want to have a working boatyard at the Alameda Marina. Svendsen's was a very successful boatyard so the probability of a replacement boatyard being successful is good. To replace Svendsen's with a new boatyard management would require getting a replacement soon before the existing customer base relocates out of necessity. We have received a firm commitment from Alameda City staff. A firm commitment from City Council would be required to eliminate the present uncertainty associated with the development.

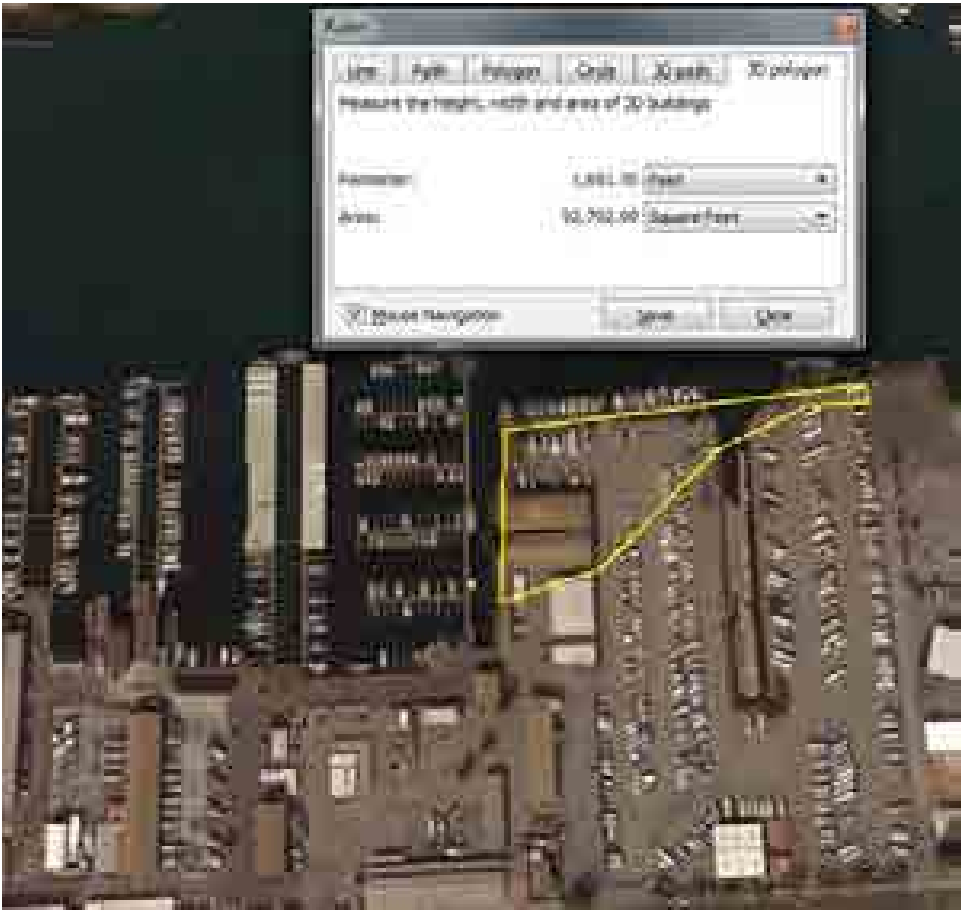
Tide Land Swap Addendum

The Alameda Marina Boatyard would be protected into the future if the City of Alameda could be owner of the land through a Tide Land swap.

Alameda City could obtain ownership of the land designated for the boatyard by using a Tide Land Swap. The Tide Lands in the North East section of the project area are in an inconvenient place for the development. The Tide Lands in this location will be difficult to continue to employ as productive maritime usage as the developments create housing units directly adjacent.

Swapping the Tide Lands for the boatyard area between building #19 and both sides of building #12 would be close to equal square footage of land swap. This swap would guarantee the land for the boatyard and also assure the Tide Lands would be preserved for their intended maritime use.

Present Tide Lands for proposed land swap.



Proposed space necessary to have a viable boatyard. This space would become Tide Lands.



NANCY McPeak

From: alanheb <alanheb@protonmail.com>
Sent: Monday, May 28, 2018 12:34 PM
To: Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; NANCY McPeak; spencer@alamedaca.gov; Malia Vella; Frank Matarrese; Marilyn Ezy Ashcraft; Jim Oddie; LARA WEISIGER; Liz Warmerdam; City Manager; Janet Kern
Subject: Alameda Marina Master Plan

I am opposed to the BAY WESTS Proposal for Alameda Marina. The Alameda Marina is a Bay-Area-Wide asset and the businesses housed there are vital to the greater Bay Area boating community. The Marina provides a site where scores of jobs are available for local residents. There are other places on Alameda where housing can be built.

The boatyard proposal is inadequate to support the existing boating community. Svendsens Marine was a prosperous and thriving business until poisonous relations with the developer forced the Svendsens family to sell. Bay Ship has closed operations simply because working with Bay West and Pacific Shops is so toxic. DOER Marine has already been forced out. The Bay Area boating community needs a yard at the Marina with travel lift and the elevator as it currently exists. Jobs have been lost to Svendsens being sold and the resulting closing of the yard. Those jobs should come back.

The proposed dry storage area is preposterously inadequate in every way. The number of dry storage spaces is a fraction of what is needed, but worse, the size of the spaces is so tiny that almost no boats currently in storage there will be able to continue to use it. The Alameda Marina has the only...THE ONLY remaining three ton hoist between Los Angeles and Portland. The proposed plan will remove it. Dry boat storage is more environmentally friendly and more affordable than in-water storage. Bay Wests plan may look good to someone who doesn't launch a boat, but it is in fact completely, inadequate. The very fact that such a ridiculous proposal was even submitted reveals the complete lack of understanding or interest in the developer to work in any way with the existing boating community.

The issue of parking and transportation to the Marina and within the Marina, both for boaters and residents is wildly underestimated. NO provision is made for boat-owner parking. None.

Until the current owner made it impossible for me to dry-store my boat at the Alameda Marina, I was a regular visitor to Alameda. I bought supplies at Pagano's Hardware, bought lunches at Delis on the island, had breakfast at restaurants on the island. I filled up my truck gas tank at the Chevron on Buena Vista a hundred times, spent money at the West Marine Store on Alameda. Since the ongoing destruction of the Alameda Marina Environment has begun, I have yet to be back to the island. I'm spending my money elsewhere. I'm not the only person who has done this. When you vote, you could change that, and bring business and jobs back to Alameda..

I strongly advise you to NOT APPROVE Bay Wests Master Plan for the Alameda Marina.

Alan Hebert

Sent with [ProtonMail](#) Secure Email.

NANCY McPeak

From: junedj@juno.com
Sent: Monday, May 28, 2018 5:11 PM
To: Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; NANCY McPeak; spencer@alamedaca.gov; Malia Vella; Frank Matarrese; Marilyn Ezzy Ashcraft; Jim Oddie; LARA WEISIGER; Liz Warmerdam; City Manager; Janet Kern; info@sawwaction.org
Subject: No to Bay West Proposal at Alameda Marina

I am opposed to the BAY WEST Proposal for Alameda Marina.
Alameda Marina is a regional asset and an essential part of the greater boating community ecosystem.

1- The boatyard proposal is inadequate to support the existing boating community let alone growth with additional marinas along the estuary and regionally. We need a functional equivalent to Svendsen's with travel lift haul outs and the elevator for floating homes and boats too large for the travel lift. Boatyards are disappearing at an alarming rate and are nearly impossible to regain. Do it yourself boatyards such as Svendsen's are even rarer and help keep water access affordable.

2- The mast-up dry storage area is inadequate in space size and quantity. The logistics of the proposed layout do not seem feasible from an operational point of view- especially during surge times of race days, weekends, and holidays. Mast up dry storage is more environmentally friendly and more affordable. The at the location on the west end shown in the plan is extremely shallow and would require massive dredging efforts with possible toxic environmental issues due to previous use as military shipyard. The logistics of boats queuing up on land and on water are challenging, at best, in the proposal. These logistics issues are readily addressed by the existing 3 ton hoist area and the previous 2 ton hoist area with an inbound and outbound lane to the hoists and large adjacent side tie docks. The dry storage mast up area should be comparable to the capability of mid 2015 with 2 hoists (2 ton and 3 ton). For improved logistics each should be 3 ton capacity.

3-Marina Wet berth vehicle parking quantity and logistics seem inadequate to support the marina use. It is common for boaters to transport boating gear, food, foul weather clothing to/from boat and vehicle.

Thanks for your attention and this opportunity to support affordable water access.

June Johnson
920 Santa Clara Ave

NANCY McPeak

From: Pat Lamborn <patricia.lamborn@aol.com>
Sent: Monday, May 28, 2018 9:15 PM
To: NANCY McPeak
Subject: Tuesday May 29th Alameda Planning Board Decision on Alameda Marina

Please share with Alameda Planning Board Members

- >
- > No to Bay West Proposal at Alameda Marina
- >
- > I am opposed to the BAY WEST Proposal for Alameda Marina. Alameda Marina is a regional asset and an essential part of the greater boating community ecosystem.
- >
- > 1- The boatyard proposal is inadequate to support the existing boating community let alone growth with additional marinas along the estuary and regionally. We need a functional equivalent to Svendsen's with travel lift haul outs and the elevator for floating homes and boats too large for the travel lift. Boatyards are disappearing at an alarming rate and are nearly impossible to regain.
- >
- > 2- The mast-up dry storage area is inadequate in space size and quantity. The logistics of the proposed layout do not seem feasible from an operational point of view- especially during surge times of race days, weekends.
- >
- > 3-Marina vehicle parking quantity and logistics seem inadequate to support the marina use. It is common for boaters to transport boating gear, food, foul weather clothing to/from boat and vehicle.
- >
- > 4. I support a reasonable # of houses to balance jobs, marina use, and housing. Please honor the #s committed to in our Alameda Housing Element with a reasonable commitment to affordable housing.
- >
- > Thanks for your attention and this opportunity to support affordable water access.
- > Sincerely, Pat Lamborn
- >
- >
- >
- > SAWW info@sawwaction.orgo

NANCY McPeak

From: Kathy <Gregkathsmith@att.net>
Sent: Monday, May 28, 2018 8:45 PM
To: NANCY McPeak
Subject: FW: Alameda Marina Must Include Substantial Marine Services & Haul OutFacilities

Sent from [Mail](#) for Windows 10

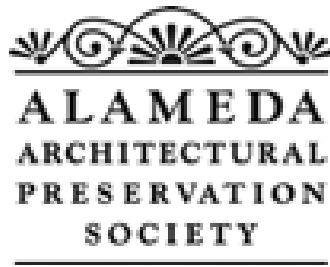
From: [Kathy](#)
Sent: Monday, May 28, 2018 8:39 PM
To: tspencer@alamedaca.gov
Subject: Alameda Marina Must Include Substantial Marine Services & Haul OutFacilities

I have worked in the Alameda Marine Industry for 35 years. The fact that we are down to one small boat yard considering the number of marinas and boats in Alameda is a very troubling situation. With the consideration of the tax base, money spent by recreational boaters this proposal lacks of common sense and shows the greed of developers just to build more houses. It's hard enough to get on & off the island morning and evening.

Fortunately I live and work on the island. We need more not less recreational marine services on this island. It is a great disservice for the many boat owners and berthers in Alameda to have to go all the way to Richmond for haul out services.

Sincerely,

Greg R. Smith



May 28, 2018

(By electronic transmission)
Planning Board
City of Alameda
2263 Santa Clara Avenue
Alameda, CA 94501

Subject: Proposed Recommendation to Approve the Master Plan, Density Bonus Application and Environmental Impact Report for the Alameda Marina Property Located at 1815 Clement Avenue (Item 7-A on Planning Board's 5-29-18 agenda)

Dear Planning Board Members:

The Alameda Architectural Preservation Society (AAPS) would like to thank the Planning Board for this opportunity to comment on the Master Plan and Environmental Impact Report for the Alameda Marina project. However, we have concerns regarding the demolition of the majority of buildings located in the Alameda Marina Historic District and Cultural Landscape.

Per the Staff Report: “*Cultural Resources*: The EIR found that redevelopment of the property and demolition of existing structures would result in a significant impact on the historic resource. The Alameda Marina property is a historic resource because of the role the site and the collection of remaining buildings played in the shipbuilding industry during the Second World War. The Historical Advisory Board has designated seventeen buildings, Buildings 1, 4, 6, 12, 15, 16, 17, 19, 21, 22, 27, 28, 29, 31, 32, 33, 34, and the graving dock as contributing buildings/features to the “Alameda Marina Historic District,” which has been included on the City’s Historical Building Study List. Three of the seven commercial buildings (Buildings 16, 19, and 27) have been determined to be individually eligible for the National Register of Historic Places. The Master Plan will retain six (6) of the contributory buildings, the graving dock, and the three individually eligible buildings. The Master Plan will also retain five (5) additional existing buildings. All exterior modifications to any contributory buildings that will be retained within the Alameda Marina Historic District will be rehabilitated consistent with the Secretary of Interior Standards.”

The Alameda Historical Advisory Board created the District when it designated 17 of the 37 historic buildings and the graving dock as an Alameda Historic District. In addition, all the buildings at Alameda Marina were built pre-1942 and are therefore protected by Alameda’s Historic Preservation Ordinance in Chapter 13 of the Alameda Municipal Code under “Interim Review”.

We strongly urge the developer to adapt the Preservation Alternative and/or the Extensive Adaptive Reuse Alternative described in the Draft EIR, both of which will essentially preserve the Alameda Marina Historic District and the buildings within it. The buildings can then either continue to be used as they have been since the 1960s for light industrial and other commercial/retail uses and/or

adaptively reused for residential purposes. The Preservation Alternative allows the creation of 475 housing units, well above the 396 units required by the Regional Housing Needs Allocation assigned to this property and consistent with Alameda's Housing Element. The number of residential units is increased to approximately 550 units under the Extensive Adaptive Reuse Alternative.

Per the Draft EIR, both Alternatives result in "Less than Significant Impacts" for Aesthetics, Air Quality and Climate Change, Biological, Geology, Soils and Paleontological Resources, Hazards and Hazardous Materials, Hydrology and Water Quality, Land Use and Planning, Noise and Vibration. Both Alternatives would also better address population and employment, public services and recreation and traffic issues over the developer's master plan. Variations of these Alternatives that further increase the number of residential units up to the proposed 779 units while still preserving the historic buildings, should be investigated.

Although the Final EIR states that the State Housing Accountability Act (HAA) and State Density Bonus Law (DBL) require the city to approve the proposed 779 units, it is unclear whether these statutes apply to the project as discussed in Item 3 below.

In addition, we have the following concerns with the approval of the EIR:

1. **Do not irreversibly destroy the interior of Building 19.** This structure retains a very high degree of integrity from the WWII period with few alterations. It is individually eligible for the National Register of Historic Places due to its integrity. The developer proposes to erect 3-4 floors in its interior to divide the space into multiple commercial spaces. The method of making this alteration should be able to be reversible as per the Secretary of the Interior's Standards for Rehabilitation, without damaging the structure of the building to restore it back to its current state, allowing the building to retain its individual eligibility for the National Register.
2. **Multiple Federal agency approvals will be necessary to complete the dredging portion of the Master Plan and would therefore invoke both the National Environmental Policy Act (NEPA) and Section 106 of the National Historic Preservation Act (NHPA).** These Federal approvals could affect the treatment of the historic buildings. Dredging will be required due to the hoist relocation and as a condition of Pacific Shops' lease for Tideland Trust property at Alameda Marina with the City of Alameda. The DEIR states dredging may occur to accommodate current and projected use of the slips. The Federal approvals could result in significant project modifications. Approving the EIR now separate from the NEPA and Section 106 processes will prevent adequate coordination of these processes with the EIR process as well as the city approval process. Normally, CEQA and NEPA reviews are concurrent. Why is that not the case for this project?
3. **The city has not completed sufficient transportation and infrastructure studies of the cumulative effects of each development planned along the northern waterfront and citywide.** Citywide computer models are needed to adequately assess cumulative citywide transportation impacts of this and other planned, but unbuilt, projects on transportation and other infrastructure, including scenarios such as an earthquake or other emergency that may require mass evacuation from the island using existing bridges and the tube. In addition to the EIR-related inadequacies, the lack of this cumulative impact analysis also precludes adequate evaluation of health and safety issues that may disqualify this project under the HHA and under Sections 65915(d)(1)(B) and 65915(e)(1) of the DBL. Until these cumulative impact analyses are completed, it is premature to assume that the HHA and DBL mandates requiring approval of projects that conform to zoning and the general plan apply.

Once completed, the computer models could be used to assess cumulative impacts of future projects and improve the city's overall planning for future projects.

Sincerely,

Christopher Buckley, President
Alameda Architectural Preservation Society

cc: Mayor and City Council (by electronic transmission)
Nancy McPeak (clerk) Andrew Thomas and Debbie Potter (by electronic transmission)
AAPS Board and Preservation Action Committee (by electronic transmission)

NANCY McPeak

From: Robert Thompson <trisailorbob@gmail.com>
Sent: Monday, May 28, 2018 10:05 PM
To: Ronald Curtis; Jeffrey Cavanaugh; Sandy Sullivan; David Mitchell; Alan Teague; NANCY McPeak
Cc: spencer@alamedaca.gov; Malia Vella; Frank Matarrese; Marilyn Ezzy Ashcraft; oddie@alamedaca.gov
Subject: NO to Bay West's current proposal for the Alameda Marina!!!

Dear Esteemed Alameda Planning Board Members,

As both an Alameda resident, as well as a 25 year tenant of the Alameda Marina Dry Storage, I am strongly opposed to Bay West's current proposal for the Alameda Marina sight, and am urging you to vote NO at the Planning Board meeting on May 29th.

The Alameda Marina is a regional asset that is an essential, integral part of the greater San Francisco Bay boating community. And the loss this irreplaceable asset will have a severe negative impact on both Alameda Boaters as well as those from outside the area that rely on this facility to maintain their boats.

In reading through the current proposal, it is clear that the Boat Yard portion is grossly inadequate to support the existing boating community. Alameda needs a fully functional, full service boat yard, similar to the one that Svendsen's Marine offered. This includes Travel Lift facilities, an Elevator for larger boats and the floating homes of Alameda, etc. The suggestion that somehow, floating repair facilities can replace much of the land based need is ridiculous, and demonstrates how short-sighted those behind this proposal are. Boatyards in the greater San Francisco Bay Area are disappearing at an alarming rate, and would be completely impossible to ever reestablish. Additionally, "Do it Yourself" boat yards such as the one that was offered by Svendsens are even more important, as they provide a lower cost alternative to those with limited means. Doing away with this important capability will create an even larger hurdle towards boat ownership, and therefore access to the water for many.

Additionally, an area that affects me directly is Bay West's recommendation for the Mast Up Dry Storage. Simply put, the mast-up dry storage area is inadequate in space size and quantity. The logistics of the proposed layout are not feasible from an operational point of view- especially during surge times of race days, weekends, and holidays. Mast up dry storage is more environmentally friendly and more affordable. The at the location on the west end shown in the plan is extremely shallow and would require massive dredging efforts with possible toxic environmental issues due to previous use as military shipyard. The logistics of boats queuing up on land and on water are challenging, at best, in the proposal. These logistics issues are readily addressed by the existing 3 ton hoist area and the previous 2 ton hoist area with an inbound and outbound lane to the hoists and large adjacent side tie docks. The dry storage mast up area should be comparable to the capability of mid 2015 with 2 hoists (2 ton and 3 ton). For improved logistics each should be 3 ton capacity.

And finally, the number of homes proposed, and the additional traffic that will be created are unsustainable. If the proposal, as written is passed, then the already overburdened exits from the island will become impossible choke points. The decline in quality of life that the overpopulation of Alameda Island is real, and it is irresponsible to believe that you can add this lever of population increase, without creating new exit points in the form of bridges and tunnels. Failure to require the traffic infrastructure be put in place prior to the building of high density housing is irresponsible. And, if the planning board approves this proposal, I strongly suggest that your term in office will be limited, as the residents of Alameda have grown tired and frustrated with these short sighted decisions by our elected leaders.

With Regards,

Robert Thompson

NANCY McPeak

From: Thom Mantooth <thomscorpio@att.net>
Sent: Monday, May 28, 2018 11:17 AM
To: David Mitchell; Sandy Sullivan; Alan Teague; NANCY McPeak; spencer@alamedaca.gov; Malia Vella; Frank Matarrese; Marilyn Ezzy Ashcraft; Jim Oddie; LARA WEISIGER; Liz Warmerdam; City Manager; Janet Kern; info@sawwaction.org
Subject: NO to Bay West proposal for Alameda Marina

I am opposed to the BAY WEST Proposal for Alameda Marina. Alameda Marina is a regional asset and an essential part of the greater boating community ecosystem.

1- The boatyard proposal is inadequate to support the existing boating community let alone growth with additional marinas along the estuary and regionally. We need a functional equivalent to Svendsen's with travel lift haul outs and the elevator for floating homes and boats too large for the travel lift. Boatyards are disappearing at an alarming rate and are nearly impossible to regain. Do it yourself boatyards such as Svendsen's are even rarer and help keep water access affordable.

2- The mast-up dry storage area is inadequate in space size and quantity. The logistics of the proposed layout do not seem feasible from an operational point of view- especially during surge times of race days, weekends, and holidays. Mast up dry storage is more environmentally friendly and more affordable. The at the location on the west end shown in the plan is extremely shallow and would require massive dredging efforts with possible toxic environmental issues due to previous use as military shipyard. The logistics of boats queuing up on land and on water are challenging, at best, in the proposal. These logistics issues are readily addressed by the existing 3 ton hoist area and the previous 2 ton hoist area with an inbound and outbound lane to the hoists and large adjacent side tie docks. The dry storage mast up area should be comparable to the capability of mid 2015 with 2 hoists (2 ton and 3 ton). For improved logistics each should be 3 ton capacity.

3-Marina Wet berth vehicle parking quantity and logistics seem inadequate to support the marina use. It is common for boaters to transport boating gear, food, foul weather clothing to/from boat and vehicle.

Thanks for your attention and this opportunity to support affordable water access
Thom Mantooth



Virus-free. www.avast.com

NANCY McPeak

From: Eileen <Eileen@alamedamarina.net>
Sent: Tuesday, May 29, 2018 8:42 AM
To: NANCY McPeak; Becca Perata
Subject: I Support Plans to Revitalize Alameda Marina

Dear Alameda Planning Board,

The proposed Master Plan for Alameda Marina is the culmination of more than two years of work with City planners and the community. The new plan will:

- Bring hundreds of new housing units, including affordable housing
- Preserve and enhance a commercial maritime core and bring new jobs
- Open up waterfront access to all Alamedans — a new aquatic park, extension of the Bay Trail and open space for public enjoyment

- Invest \$35M to rebuild a crumbling seawall
- Include a modern, efficient dockyard for land- and water-based marine services, including boat repair and maintenance

After more than two years of public review and changes, I recommend you move this plan forward so Alameda has a waterfront that works!

NANCY McPeak

From: Judith Lynch <judithlynch7@gmail.com>
Sent: Tuesday, May 29, 2018 10:40 AM
To: Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague; NANCY McPeak; spencer@alamedaca.gov; Malia Vella; Frank Matarrese; Marilyn Ezzy Ashcraft; Jim Oddie; LARA WEISIGER; Liz Warmerdam; City Manager; Janet Kern; info@sawwaction.org
Subject: for Planning Board session Alameda Marina 5-29 2018
Attachments: Bldg 19 inside Judith.jpg

City of Alameda Planning Board,

City Council, Historic Advisory Board

and all other interested parties:

Headline: Do not approve the Bay West “Master Plan” for the Alameda Marina

Part of Alameda’s dwindling maritime heritage still lives at the Alameda Marina, a major ship building center since the 1880s, according to Save Our Working Waterfront (SAWW) citizens organizing to save both the nautical present and the architectural heritage of the thirty plus historic buildings there. I support the SAW initiative to derail the “plan” currently being promulgated by developers Bay West. The AAPS-SAWW letter is thoroughly detailed about aspects of the nautical value of the place and its significance to the boating community and subsidiary craftspeople, businesses, and organizations, both non-profit and social.

However another important aspect of the value of the Alameda Marina is the history endowed by its yesteryear structures. Some people who love old buildings are befuddled by the Marina because these solid workaday structures don’t bewitch with beguiling details like easy to embrace Victorians. However the Marina has stalwart functional places with calm demeanor and consistency of materials and color. These buildings are what historian Woody Minor so fondly calls “the Chorus Line.” Their quiet elegance provides back up support for the architectural celebrities that abound elsewhere on the Island.

Much the same bewilderment applied years ago when the Alameda Point--Naval Air Station (NAS) was decommissioned. These pre-mid-post WWII buildings were initially unappealing, with nary a bracket, to keep your eyes from sliding right off. But more careful study with sympathetic hearts revealed the place as a cultural treasure, with uniformity of decoration, understated but endearing lines, open spaces enriched with statuary, and a mess hall entrance enlivened with oval columns. Use that same embracing gaze on the Alameda Marina, which exhibit similar uniformity of materials, massing, and size, constituting a utilitarian chorus line that still sings a fine tune.

Sincerely, Judith Lynch
AAPS member
HAB member 2004-2014 (or so)

Image attached: Building 19 is the multi-story showpiece of the Alameda Marina that offers tangible proof that “We are bigger inside!” Marvel at its vast interior space, designed by architect Alben R. Froberg. Three stories high and providing thousands of square feet of space, it is both the tallest and the largest at the Marina. Credit: Woody Minor



NANCY McPeak

From: Keith Mccoy <keith@urbanmixdevelopment.com>
Sent: Tuesday, May 29, 2018 10:28 AM
To: NANCY McPeak
Subject: I Support Plans to Revitalize Alameda Marina

Dear Alameda Planning Board,

Unfortunately I can't attend the Planning Board mtg this evening due to a conflict. However, I strongly urge you to approve this well thought out and much needed project.

The proposed Master Plan for Alameda Marina is the culmination of more than two years of work with City planners and the community. The new plan will:

- Bring hundreds of new housing units, including affordable housing
- Preserve and enhance a commercial maritime core and bring new jobs
- Open up waterfront access to all Alamedans — a new aquatic park, extension of the Bay Trail and open space for public enjoyment
- Invest \$35M to rebuild a crumbling seawall
- Include a modern, efficient dockyard for land- and water-based marine services, including boat repair and maintenance

After more than two years of public review and changes, I recommend you move this plan forward so Alameda has a waterfront that works!

Keith McCoy
1007 Morton St
Alameda, CA 94501

NANCY McPeak

From: Marcel Sengul <marcelsengul@gmail.com>
Sent: Tuesday, May 29, 2018 11:24 AM
To: NANCY McPeak
Cc: ANDREW THOMAS
Subject: Alameda Marina

Nancy/Andrew,

Please forward this email as a letter of support to the Planning Board. Thank you.

Members of the Planning Board,

As an Alameda local and avid boater, I would like to express my full support of the Alameda Marina development. Personally, I have spent many days at Alameda Marina, mostly tinkering with my own boating hobby, but also seeing the space with all of its future potential.

Although historically the marina has been functional as a maritime industry property as well as hosting other types of businesses, the space has been grossly underutilized for decades. The time has come to make full use of the acreage. Our housing needs are growing on the island and the commercial component of this property needs to use space a bit more efficiently. Coupled with the improvements to the marina and sea wall repairs, this development is poised to improve a very large chunk of our waterfront for the benefit of residents, businesses, and boaters alike.

Understanding that traffic may be a concern for some, I would like to point out San Francisco as our neighbor and other densely packed cities such as New York, Chicago, Boston, etc. New developments are continually going up in those cities yet they still find a way to get around. The developer has taken great care to address traffic concerns which is appreciated. At the end of the day, Alamedan's have to come to grips with reality that population growth will only continue to get exponentially stronger, forcing new housing. Along with new housing there may be some additional traffic that just might possibly slow down commutes a little bit more. However, please do not underestimate the new generation of public transport and 'ride share' obsessed citizens. More and more of our population is shying away from driving and utilizing the many transport services available at our fingertips. Once the dust settles, I truly believe the traffic will be a much smaller concern.

Please accept this letter of support in lieu of my attendance this evening.

Thank you.

--

Marcel Sengul
415-503-8629

29 May 2018

Dear planning board members

It has come to my attention that some of you have received information about the circumstances surrounding DOERs eviction from Alameda Marina that may have caused you to discount or dismiss both the business and my personal credibility. In the interest of fairness, I ask that you read and consider my side of the story as you weigh the master plan before you this evening.

In July of 2017, Sabrina Svendsen and Bay West publicly lied about DOER, stating that the company failed to pay almost \$200k in rent and utilities. They further untruthfully asserted that more than \$150k in rent was forgiven in settlement. The reality is that DOER was targeted for removal in order to free up Building 19 for adaptive reuse. By ousting DOER, scaled down maritime services could theoretically be consolidated into one multi story building. This strategy is prominently featured in the master plan with Building 19 as a focal point for the proposed lightweight dockyard, "maritime core". It destroys the integrity of building 19 which is both a historic building and historic crane.

DOER and boaters were squeezed by the closure of boat hoists proximate to our building. It eliminated our ability to carry out in water testing on site. We incurred financial burdens having to transport equipment and personnel for off-site testing. In addition our use of Building 20, was restricted to storage as our efforts to bring in paddle sports, a dive shop, education center, and nonprofit collaborators were rebuffed. We paid tens of thousands of dollars in rent on this space while being actively blocked from using it to strengthen and diversify our business. Now some of these same ideas are being presented in the master plan as "vibrant additions" as if Bay West thought of them.

The flat refusal to extend our lease despite having no approved development plan caused delays and cancellation of contracts for DOER. Our cash flow was disrupted. We were forced to release more than half our employees and were late on paying rent several times as a direct result of these pressures. Despite this, all rent was paid through the end of our lease in 2016. In January of 2017, we were served with an eviction notice that gave us just five days to vacate. The rent check we submitted for December was not cashed. It was the culmination of a years- long, engineered effort to weaken DOER while building a case for eviction.

While Bay West has no maritime or marine construction experience, they are extremely experienced at eviction. This became apparent when we went to court and were faced with a gaggle of lawyers representing Bay West. We asked for a year extension to our lease to give us time to continue our search for an alternative site with water access. This was denied. We eventually made the best deal we could but still paid \$40k towards 2017 rent plus all of our legal costs and lost all security deposit funds. Bay West did cede a bit of time, giving us until September 2017 to vacate; hardly a \$150k "forgiveness" and no back rent was owed. The damage done by the false assertions from Ms Svendsen and Bay West made prospective landlords leery. Plans with Catellus, which included an ocean exploration center and water taxi fell through as a result. We were blessed to find a temporary solution at Alameda Point but we still lack working access to the water.

Bay West continues to lie about DOER in the press, most recently in the 1 May edition of the SF Business Times. Shortly thereafter, a consultant from Bay West paid a visit to our temporary location to deliver a personal message for me to "back off", "move on", and to "stop commenting about the plan on social media".

I want to be crystal clear that I do not oppose building new housing in Alameda, not even at Alameda Marina. Before cancer took Svend Svendsen, I was in full support of his plan for retaining all existing businesses and historic buildings, expanding maritime and adding workforce housing as "bookends" to the property. We looked forward to being a part of the next chapter at Alameda Marina. The wholly reasonable approach preserved the working waterfront while adding our most needed type of housing. It was a plan that holistically worked with the Northern Waterfront without overly impacting the surrounding neighborhood or truck route.

The argument that 760 units of housing are needed to fund \$35M in tidelands repair does not hold water. The \$3M spent by PSI since 2006 has been towards floating docks and some pier pilings; mostly floating assets that can be resold, just like the barge they propose now in lieu of a full service boat yard. The "condemned" hoists that cut off tenant access to the water remain in use even now by PSI staff. The deferred maintenance plan is real and was as strategic as the ousting of DOER and Svendsen's Boat Works. Both companies were called "incompatible with the vision" at the November 2015 tenants and neighbors meetings. As stated before, Bay West is skilled in marketing spin and eviction, not maritime and marine construction.

Please reject the master plan as currently proposed and obtain three independent bids from certified marine construction firms to competitively rebid the tidelands work per the 2010 document noting what recommendation were and were not followed. Ask the city attorney to review the matter for tidelands lease compliance and accountability in maintaining these public trust areas, particularly in regard to BCDC rules. Please listen to the many voices that have actual maritime and boating experience about the future of Alameda Marina. Please read the articles and outline for blue economic development I've provided in earlier correspondence and shared with Economic Development staff. Please look at the economic development staff's recommendations for blue tech and maritime. Once the unique and historic assets of Alameda Marina are gone, they will be gone for good, forever altering the character and the utility of the public tidelands. Please don't aid and abet bullying by rewarding Bay West with an approval of the current master plan.

Thank you

Liz Taylor
DOER Marine
650W Tower Ave
Alameda CA 94501

cc: Mayor Spencer, City Council, City Attorney

NANCY McPeak

From: Vicki Sedlack <vsedlack@gmail.com>
Sent: Tuesday, May 29, 2018 12:57 PM
To: David Burton; Ronald Curtis; Jeffrey Cavanaugh; David Mitchell; Sandy Sullivan; Alan Teague
Cc: NANCY McPeak; ANDREW THOMAS
Subject: Please approve Alameda Marina master plan

May 29, 2018

Dear Members of the Alameda Planning Board,

I am writing to strongly support the redevelopment plans for the Alameda Marina.

It was just recently that I discovered there was a whole world beyond the Clement St. wall. With the proposed Alameda Marina plan, other Alamedans will discover this world, too, thanks to opened waterfront access. Alameda will be able to enjoy a new aquatic park, extension of the Bay Trail, and open space. In addition, the plan will invest \$35,000,000 to rebuild the crumbling sea wall.

Alameda desperately needs additional housing, which Alameda Marina will address with new units, including affordable units. Preservation of the existing maritime core and creation of new jobs are a critical part of the plan, as is the creation of a modern, efficient dockyard.

I urge you to move forward with the Master Plan.

Sincerely,

Vicki Sedlack
vsedlack@gmail.com
510-459-5204

NANCY McPeak

From: LARA WEISIGER
Sent: Tuesday, May 29, 2018 1:15 PM
To: NANCY McPeak
Subject: Fwd: Oposed to the Alameda Marina Development

Follow Up Flag: Follow up
Flag Status: Flagged

Begin forwarded message:

From: Grant Hayes <grant.l.hayes@gmail.com>
Date: May 28, 2018 at 5:42:47 AM PDT
To: "spencer@alamedaca.gov" <spencer@alamedaca.gov>, "mvella@alamedaca.gov" <mvella@alamedaca.gov>, "fmatarrese@alamedaca.gov" <fmatarrese@alamedaca.gov>, "mezzashcraft@alamedaca.gov" <mezzashcraft@alamedaca.gov>, "mezzashcraft@alamedaca.gov" <mezzashcraft@alamedaca.gov>, "joddie@alamedaca.gov" <joddie@alamedaca.gov>, "lweisiger@alamedaca.gov" <lweisiger@alamedaca.gov>, "lwarmerdam@alamedaca.gov" <lwarmerdam@alamedaca.gov>
Subject: **Oposed to the Alameda Marina Development**

Respected Council Members,

I have been an Alameda resident for over 20 years, I am a boat owner, and a major proponent of Alameda. When I moved to Alameda the base had just closed and Alameda's fate was in question. Alameda survived the base closure and it prospered. I am not opposed to progress and change but I have my concerns regarding the scope of the recent changes.

The Master Plan for Alameda Marina is simply too much.

Bay Wests intent to build 760 housing units is grossly excessive. The subsequent traffic nightmare that will occur on Clement Street will make driving impossible between Park and Grand Ave.

In the last year traffic has increased significantly throughout the Island. Even worse than traffic, driver courtesy and observation of basic traffic rules and the 25 MPH has declined to dangerous levels. As a driver I have been passed both on the right and the left hand side while proceeding down Clement Street.

The development also reduces the number of good paying maritime jobs and erodes our maritime history. The loss of Swenson's greatly effects the boating community and the ability to maintain boats. The Bay Area already has a shortage of boat maintenance facilities. To assume Grand Boat Yard can absorb the maintenance requirements of the Alameda boating community is absurd. Imagine Alameda with one gas station.

Bay West's project does not provide enough parking to meet the needs of the new residents, the boating community and visitors. Parking is extremely difficult at the Main Street ferry terminal. What

protections are in place to ensure Clement and the Marina do not mirror the problems of the ferry terminal?

Let's work together to make Alameda a better community, not just a denser community.

Respectfully,
Grant Hayes
1331 Weber St.
Alameda, CA 94501

Sent from [Mail](#) for Windows 10

NANCY McPeak

From: beth kenny <bethkenny411@gmail.com>
Sent: Tuesday, May 29, 2018 3:22 PM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: Support Housing at Alameda Marina!

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Alameda Planning Board,

I urge you to approve the master plan for Alameda Marina. Alameda desperately needs more housing. Alameda Marina is an excellent opportunity to increase housing and create an exciting new environment along the estuary. If approved this project will create a significant amount of universally designed units in accordance with Alameda's Universal Design Ordinance. Creating 200+ new housing units that are easily adaptable to become accessible would drastically improve the chances of people with disabilities and senior residents obtaining suitable housing.

Sincerely,
Beth Kenny

Sent from my iPhone

NANCY McPeak

From: Sarah Sieloff <sarah.sieloff@cclr.org>
Sent: Tuesday, May 29, 2018 4:40 PM
To: NANCY McPeak; Becca Perata
Subject: I Support Plans to Revitalize Alameda Marina

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Alameda Planning Board,

I write in support of plans to revitalize the Alameda Marina. This project will provide much-needed investment to rebuild a crumbling sea wall, increase the city's housing stock, and enhance the existing maritime economy while creating new jobs. In short, plans for the redevelopment of the Marina accomplish what a good infill project should -- they provide the city and the surrounding neighborhood with an opportunity to build for a vibrant, prosperous and sustainable future.

Alameda has no room to sprawl, as you surely know, and infill is therefore the island's single best way to meet its current and future needs. In addition, redevelopment of the Marina will remove contamination left on the site by past industrial activity. Removing this contamination will protect the health of Alameda's water, soil and air, in addition to the health of its people. Especially as sea level rise threatens, communities around the country are having to ask themselves hard questions about the risk this development poses for existing brownfields, or even sites that were once remediated but are now threatened by the advancing waters. Alameda is fortunate to have the opportunity to remediate the site and remediate it correctly.

Speaking strictly now from my personal perspective, and not on behalf of my organization, I lived in Alameda for three years, and moved in March to keep my rent from skyrocketing. The city needs more housing. Neighbors who oppose the development of new housing at the Alameda Marina are looking backward, and that's not an advisable position from which to shape public policy. Those who would oppose this project are holding Alameda's future hostage. Your responsibility as planning commissioners is to make a decision for the public good, and the public good requires environmental cleanup, housing, jobs, waterfront access, solid infrastructure and protection against sea level rise.

After more than two years of public review and changes, as land reuse professional and a former resident, I recommend that you move this plan forward.

Sincerely,

Sarah Sieloff



Sarah Sieloff | Executive Director
www.cclr.org | 415-398-1080 x 110
[200 Frank Ogawa Plaza, 5th Floor, Oakland, CA 94612](https://www.cclr.org/200-Frank-Ogawa-Plaza-5th-Floor-Oakland-CA-94612)



NANCY McPeak

From: Amy Wells <wingit74@yahoo.com>
Sent: Tuesday, May 29, 2018 4:43 PM
To: NANCY McPeak
Subject: I Support Plans to Revitalize Alameda Marina

Dear Alameda Planning Board,

The Alameda Marina plan has evolved over time and I believe the plan, in it's current form, meets the needs of the existing boating community, including active dry storage sailors, the Island Yacht Club and active marine repair services and sail lofts located onsite at Alameda Marina.

My dry stored trimaran has been a resident of Alameda Marina for the past fifteen+ years. I am a member of Island Yacht Club. I am commodore of Bay Area Multihull Association, and BAMA's fleet makes up a large part of the dry storage active sailed boats.

It is confusing to me why there is so much opposition to the build. The SAWW emails are not signed by anyone, and the ideas expressed therein don't match the sentiment of the business people and yacht club members existing on the property. I am left to conclude that SAWW is made up of a small number of people who are gifted at being oppositional.

It seems like upgrades to the marina should be possible even in the face of such opponents and I hope the build is eventually approved as planned.

Best regards,

Amy Wells
F-27 Wingit
(831) 252-4274

NANCY McPeak

From: EDWARD W CALLENDER <springy@sbcglobal.net>
Sent: Wednesday, May 30, 2018 7:37 AM
To: NANCY McPeak; becca@voxpathulipr.net
Subject: I Support Plans to Revitalize Alameda Marina

Dear Alameda Planning Board, The proposed Master Plan for Alameda Marina is the culmination of more than two years of work with City planners and the community. The new plan will: - Bring hundreds of new housing units, including affordable housing - Preserve and enhance a commercial maritime core and bring new jobs - Open up waterfront access to all Alamedans — a new aquatic park, extension of the Bay Trail and open space for public enjoyment - Invest \$35M to rebuild a crumbling seawall - Include a modern, efficient dockyard for land- and water-based marine services, including boat repair and maintenance After more than two years of public review and changes, I recommend you move this plan forward so Alameda has a waterfront that works!

WE NEED MAINTENANCE FACILITIES WHERE OUR VESSELS CAN BE HAULED OUT AND SERVICED FOR THEIR YEARLY MAINTENANCE IN THE ALAMEDA ESTUARY. THE BOATERS NEED GATED FACILITIES TO KEEP OUR VESSELS SAFE AND PLEASE DO NOT ALLOW ANY TYPE OF LANDSCAPING WITH THROWABLE ROCKS OR OBJECTS.

IF YOU WERE FORCED TO ONLY BUY FUEL ONLY IN RICHMOND OR ALL OF YOUR VEHICLE MAINTENANCE WAS REQUIRED TO GO TO RICHMOND WHAT WOULD THE REACTION OF THE RESIDENCE BE ??

WHO IS GOING TO BE RESPONSIBLE FOR KEEPING OUR BERTHING MAINTAINED AND DREGGED SO ACCESS TO BOATS CAN BE HAD AT LOW TIDES??

WHERE CAN VESSELS BE HAULED OUT IN EMERGENCIES ??