

CITY OF ALAMEDA RESOLUTION NO. _____

APPROVING TENTATIVE MAP TRACT 8060 PLN 20-0118, DENSITY BONUS APPLICATION PLN 20-0119, DEVELOPMENT PLAN PLN 20-0120, AND OPEN SPACE DESIGN REVIEW PLN 20-0121 FOR DEVELOPMENT OF THE 9.48 ACRE PROPERTY LOCATED AT 2229 - 2235 CLEMENT AVENUE

WHEREAS, Boatworks, LLC ("Boatworks") is requesting approval of a Tentative Map Tract 8060 PLN 20-0118 (Exhibit A), Density Bonus application PLN 20-0119 (Exhibit B), Development Plan PLN 20-0120 (Exhibit 3), and Open Space Design Review PLN 20-0121 (included in Exhibit C) for the construction of 182 residential units, approximately 1.5 acres of publicly accessible open space, and associated parking, landscaping and other improvements at the property owned by Boatworks, LLC located at 2229 - 2235 Clement Avenue (APN 071-0290-001-00 and 071-0289-005-00), also known as "the Property"; and

WHEREAS, the Property is designated for Mixed Use in the General Plan Diagram; and

WHEREAS, the Property is located in a R-2/PD, Two Family Residential/Planned Development Zoning District and Open Space Zoning District; and

WHEREAS, on October 5, 2010, by Resolution No. 14494, the City of Alameda certified an Environmental Impact Report (EIR) evaluating 242 residential units on the Property consistent with the California Environmental Quality Act (CEQA); and

WHEREAS, the existing zoning for the property allows a base density of 140 residential units; and

WHEREAS, Boatworks is proposing to provide a total of thirteen (13) units (or 9%) affordable to very low income households and eight (8) units (or 6%) affordable to moderate income households, for a total of twenty-one (21) affordable units, which qualifies the project for a 30% density bonus (for a total of 182 residential units) and one concession or incentive under State Density Bonus Law and the City's density bonus ordinance; and

WHEREAS, subdivision of the property will facilitate the development of the property consistent with the Development Plan for 182 residential units; and

WHEREAS, the Planning Board held a duly noticed public hearing on March 23, 2020, examined pertinent maps, drawings, and documents, and unanimously recommended approval of Tentative Map Tract 8060 PLN 20-0118 (Exhibit A) to allow the subdivision of a 9.4 acre site into 153 residential lots, Density Bonus application PLN

20-0119 (Exhibit B), Development Plan PLN 20-0120 (Exhibit 3), and Open Space Design Review PLN 20-0121 (included in Exhibit C) for the construction of 182 residential units, including 21 affordable units, approximately 1.5 acres of publicly accessible open space, and associated parking, landscaping and other improvements at the property owned by Boatworks, LLC located at 2229 - 2235 Clement Avenue; and

WHEREAS, on April 7, 2020, the City Council conducted a duly noticed public hearing, reviewed the Density Bonus application, Development Plan, Open Space Design Review, and Tentative Map Tract 8060 and all pertinent maps, documents and exhibits, and determined the entitlements to be consistent with state law, the General Plan, Housing Element, Open Space Element, and Alameda Municipal Code.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of Alameda makes the following findings pertaining to the Density Bonus Application (Exhibit B):

1. **The proposal qualifies for a 30% density bonus pursuant to California Government Code Section 65915 and City of Alameda Municipal Code (AMC) Section 30-17.** The applicant is proposing to provide 13 very low income units, or 9% of the base project of 140 units for very low-income households, which qualifies the project for a 30% density bonus for a total project size of 182 units.
2. **The proposal qualifies for waivers from City of Alameda development standards that physically preclude the construction of the development at the densities permitted pursuant to AMC Section 30-17.** The applicant has requested waivers from AMC Section 30-53 (prohibiting multifamily housing) and AMC Section 30-4.2 (prohibiting parcels less than 2,000 square feet in size), both of which physically preclude the construction of the development at the densities permitted.

The applicant's request for a waiver of the City's Universal Design requirements is premature and may be considered at a later date when the applicant presents evidence (e.g., floor plans for Design Review approval) that demonstrates the development standards physically preclude development of the project at the permitted density. The applicant may request a waiver under Density Bonus law or under the City's local ordinance (AMC Section 30-18.5) for review by the Disability Commission and Planning Board.

The applicant's request for waivers of the parking, open space, height and setback requirements in the R-2 zoning ordinance are not necessary because (1) the Planned Development zoning allows the City to approve the requested building heights, setbacks, and parking plans as shown in the Development Plan, without the need for density bonus waivers, and (2) the Development Plan demonstrates that the proposed development will meet the open space zoning requirements of 600 square feet of open space per unit.

3. **The proposal qualifies for one incentive or concession reducing or modifying development standards that result in identifiable and actual cost reductions to provide for affordable housing costs.** The applicant has requested a reduction or modification of local development standards (AMC Section 30-17.8) that require that the affordable housing be reasonably dispersed throughout the project and that the 21 deed restricted housing units reflect the range and number bedrooms provided in the project as a whole, which the applicant claims results in identifiable and actual cost reductions to provide for affordable housing costs.
4. **The recommended development standard waivers and concession will not have a specific, adverse impact, as defined in paragraph (2) of subdivision (d) of section 65589.5, upon public health, safety, or physical environment, and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact.**
5. **The recommended development standard waivers and concession are not contrary to state or federal law; and**

BE IT FURTHER RESOLVED, that the City Council makes the following findings pertaining to the Tentative Map Application (Exhibit A):

1. **The proposed subdivision, together with the provisions for its design and improvement, is consistent with the General Plan and Zoning for the property.** The proposed tentative map is consistent with the property's Mixed Use General Plan designation, the Open Space Element of the General Plan, and the R-2-PD and Open Space zoning designation. The subdivision will create 153 residential lots, rights of way for streets, and a parcel for waterfront open space. The proposed subdivision is designed to ensure lots face streets and parks, and that the parcels are adequately accessed by a system of internal streets, alleys, paseos, and sidewalks.
2. **The site is physically suitable for the type of development.** The tentative map facilitates the development of the property for residential use and waterfront open space on this waterfront property that is located one block from the Park Street corridor, major transit facilities, and services.
3. **The site is physically suitable for the density of the development.** The proposed subdivision provides a residential density of 19 units per acre, which is consistent with and within the density limits established by the property by the General Plan and Alameda Municipal Code. The proposed subdivision will not exceed the planned density proposed in the Development Plan.
4. **The proposed design of the subdivision and improvements, as conditioned, will not cause environmental damage or substantially and avoidably injure fish or wildlife or their habitat.** All future improvement on the property will be subject to the

environmental protections and mitigation measures imposed by the Final EIR for the project. The project as proposed does not require any work in the Oakland Estuary.

5. **The design of the subdivision and its improvements will not conflict with easements acquired by the public at large for access through or use of property within the subdivision.** All future improvements on the site will be subject to compliance with the existing and proposed easements for the access to or use of the property.
6. **The design of the subdivision and its improvements will not cause serious public health problems.** Subdivision of the property will facilitate investment in the property, which is necessary to upgrade the existing substandard infrastructure and preserve public health and will comply with all conditions of approval and mitigations imposed by the FEIR to protect the environment and public health.
7. The subdivision will not result in the discharge of waste into the sewer system that would violate regional water quality control regulations.
8. In approving the tentative map, the City Council has considered its effect upon the housing needs of the region, balanced with the public service needs of Alameda residents and available fiscal and environmental resources; and

BE IT FURTHER RESOLVED, that the City Council makes the following findings pursuant to AMC Section 30-4.20 relative to proposed Development Plan (Exhibit C):

1. **The Development Plan satisfies the purpose of the Planned Development regulations under AMC Section 30-4.20.** The proposed Development Plan is consistent with the Alameda General Plan and Zoning Ordinance for construction of multi-family residential units and open space at this location. The Development Plan qualifies for approval under AMC Section 30-4.20(g) and satisfies the purpose of the regulations to facilitate multifamily development consisting of 182 multifamily and single family homes and waterfront open space.
2. **The Development Plan is designed in a manner compatible with existing and potential contiguous uses.** The Development Plan also provides a sufficient vehicular and non-vehicular circulation system within the project with the least amount of duplication, and the Plan provides the best interface with other systems. The Development Plan is compatible with existing surrounding uses and provides coordinated travel ways for vehicular and non-vehicular circulation. Landscaped pathways and Bay Trail connections serve public access to the site and the waterfront open space. A variety of paseos, pathways, and other landscaped areas facilitate harmonious transitions between the residential buildings and immediate surroundings.
3. **The Development Plan provides a sufficient vehicular and non-vehicular circulation system within the project with the least amount of duplication; and**

the best interface with other systems. The proposed design provides an extension of Blanding Avenue and Elm Street into the project site to access the residential units and the waterfront open space. Pedestrian paseos and sidewalks are provided for non-vehicular access.

- 4. The Development Plan provides and maintains adequate landscaping using, where appropriate, native plants and taking maximum advantage of the screening capabilities of landscaping.** The proposed landscape plan includes native plants and Bay Friendly-compliant landscaping that also achieves state water efficient landscape requirements. The landscaping provides a good transition between the public sidewalks and private open spaces used by residents in the townhomes.
- 5. The amount of land proposed for any particular use can be marketed for that use within a reasonable time after development is complete.** This plan provides for the development of 182 residential units that are necessary to address a severe housing shortage in the community. These critically needed housing units include affordable housing units that are expected to be occupied within a reasonable time.
- 6. The Development Plan provides sufficient area, and encourages adequate public accessibility and usage of the water/land interface.** A network of pedestrian pathways and roadways provide public access through the site to the publicly accessible waterfront open space and Bay Trail and Clement Avenue. This network of pedestrian paths and other common landscaped open space facilitate harmonious transitions between the development, the waterfront, and the immediate surroundings.
- 7. The Development Plan provides a comprehensive, coordinated, controlled system of informational and directional graphic signage throughout the development.** The Development Plan consists of a variety of connected streets, private driveways and non-vehicular paths. A wayfinding signage program will be incorporated into the building plans as a condition of approval. Bay Trail-compliant signage will also be provided according to the Bay Trail program.
- 8. The Development Plan demonstrates progressive techniques for the conservation of, and decreased consumption of, nonrenewable energy.** The proposed project will meet the requirement of the 2020 California Building Code, which requires that all new buildings be solar ready; and

BE IT FURTHER RESOLVED, that the City Council makes the following findings pursuant to AMC Section 30-37.5 relative to the Open Space Design Review application:

- 1. The proposed waterfront open space design is consistent with the General Plan, Zoning Ordinance, and the City of Alameda Design Review Manual.** The proposed waterfront open space design is consistent with General Plan, Zoning, and Design Review manual standards for publicly accessible open space.
- 2. The proposed waterfront open space design is appropriate for the site, is compatible with adjacent or neighboring buildings or surroundings, and promotes harmonious transitions in scale and character in areas between**

different designated land uses. The open space design provides connections to the adjacent neighborhood from an extension of Blanding Avenue through the site to the waterfront, from Blanding and Oak Street via a pedestrian path to the waterfront, from the extension of Elm Street to the waterfront, and from a public pathway through the center of the project from Clement Avenue to the waterfront. The proposed design promotes harmonious transitions in scale and character with the immediate surroundings.

- 3. The proposed design of the open space and landscape materials and pathways are visually compatible with the surrounding development.** The proposed design of the open space will be visually compatible with adjacent open space provided at Park Street landing; and

BE IT FURTHER RESOLVED, that the City Council finds that, based on substantial evidence in the record, the potential environmental impacts of the project have been evaluated and disclosed pursuant to CEQA. On October 5, 2010, by Resolution No. 14494, the City of Alameda certified an EIR in compliance with CEQA that evaluated the environmental impacts of the development of 242 residential units on the property. Consistent with the October 2010 action, the conditions of approval require compliance with, and implementation of, all remaining applicable mitigation measures adopted by the City Council in October 2010. No further environmental review is required for the proposed project as provided by Section 15162 of the California Environmental Quality Act Guidelines because neither the project, nor the circumstances surrounding the project, would result in any new or more severe significant impacts to the environment than were analyzed in the previously certified EIR; and

BE IT FURTHER RESOLVED, that the City Council hereby approves Tract 8060 Tentative Map PLN 20-0118 (Exhibit 1), Density Bonus Application PLN 20-0119 (Exhibit 2), Development Plan PLN 20-0120 (Exhibit 3) and Open Space Design Review PLN 20-0121 (included in Exhibit 3), subject to the following conditions of approval:

Planning Conditions:

1. Subsequent Actions and Approvals. Development of the property and all future City actions and subsequent requests for entitlements and permits by the applicant or property owner shall be in substantial compliance with Exhibits A, B, and C attached to this Resolution. The total number of units developed on the Property shall not exceed 182 residential units. The City shall take no subsequent discretionary action that either reduces the number of units below 182 units or reduces the requested floor area of any of the 182 units provided the proposed plans are in compliance with all Applicable Law as that term is defined in the Development Agreement and in substantial conformance with Exhibits A, B, and C.
2. Regional and State Agency and Utility Company Approvals. Prior to issuance of the first building permit for any phase of the project, the applicant shall acquire the necessary permits and approvals from all other regulatory agencies with jurisdiction

over the project, including the Bay Conservation and Development Commission (BCDC) and the Department of Toxics Substance Control (DTSC) and East Bay Mud (EBMUD) and Alameda Municipal Power (AMP). Applicant/developer shall include in the project's improvement plans any de minimis modifications or improvements recommended by such agencies.

3. **Environmental Impact Report (EIR) Mitigations:** All future development of the site shall comply with all remaining applicable requirements of the Mitigation Monitoring Reporting Program for the site, as summarized below:

- a. Prior to issuance of the first building permit for the first phase of the project, the applicant shall complete Boatworks Project EIR Mitigation Measures: MM 4.B-3a, B-5, B-8b, C-2, C-6a-c, D-1, D-4, E-2, E-3, E-4, F-2, F-4, F-5a, G-1a, G-2, G-3, H-3, I-2a-c.
- b. Prior to the 101th Occupancy Permit, the applicant shall complete Boatworks Project EIR Mitigation Measure MM B-4,
- c. Prior to issuance of the first final inspection or certificate of occupancy for any dwelling unit in the first phase of the project, the applicant shall complete Boatworks Project EIR Mitigation Measures MM B-3a (TDM), MM E-1 (Interpretive history exhibit), and G-1b.
- d. Mitigation Measure 4B-3a to be completed by the applicant prior to issuance of the first Building Permit shall be fulfilled by completion of one of the following two measures:

Option #1: Preparation of a Transportation Demand Management program for review and approval by the City's Planning Board that is designed to reduce peak hour trips by 10%. The program shall include:

- a. Establishment of a Home Owners Association (HOA) and CCRs for the project;
- b. Establishment of an annual assessment on all residential units in the development in an amount determined necessary to fund the following:
 - 1) Two AC Transit Passes for each unit to provide unlimited transit pass, usable on AC Transit buses.
 - 2) Bicycle storage facilities in each unit;
 - 3) Annual car share membership for each unit; and
 - 4) Annual funding for transportation coordination services including, but not limited to, promotional information packages and planning services regarding available transportation options.

- 5) Annual funding for annual monitoring reports to be provided to the City of Alameda on an annual basis documenting the effectiveness of programs and recommended enhancements to meet 10% reduction goal.

Option #2: Establishment of a Home Owners Association (HOA) and CCRs, which provide for:

- a. HOA membership in a Transportation Management Association (TMA), which shall be responsible for providing transportation services for the residents of the project, which shall at minimum include AC Transit Easy Passes for each household, and
 - b. Establishment of an annual assessment in the CC&Rs to provide annual funding to the TMA for transportation services in the amount of: \$550/year/Single Family Detached Unit, \$450/year/Single Family Attached Unit (townhome), and \$350/year/Stacked Flat Unit, The initial annual assessment fee per unit per year established by this condition shall be subject to annual adjustment according to the annual changes in the Bay Area Consumer Price Index.
4. Affordable Housing Phasing. Prior to final map approval, the applicant/developer shall complete and record on the deed of the property an executed Affordable Housing Agreement for the 21 units described on Development Plan page MFB-1. The Certificate of Occupancy permits for the 21 deed restricted residential units shall be issued prior to the issuance of the 101st Certificate of Occupancy permit for the 152 market rate townhome and single family residential units that are not located within the 30 unit multifamily building.
 5. Waterfront Bio retention Lawn Improvement Plans. The improvement plans submitted pursuant to Engineering Condition of Approval #21 below shall include the final storm water retention plans for the Bio retention Lawn shown on Development Plan page L-1 through L5. (as useable open space lawn) and not as shown on Tentative Map page TM 6.0. (an unusable open space). Any deviation from the bio retention plans as depicted on L1-L5 of the Development Plan shall be subject to the review and approval of the Public Works Director and the recreation and parks director.
 6. Waterfront Open Space Play Structures. The improvements plans shall include play structures for young children. The structures should be located outside of the bio retention lawn area and shall be designed in consultation with the Recreation and Parks Director.
 7. Open Space Maintenance and Access. Prior to final map approval, the applicant shall provide to the satisfaction of the Public Works Director a Maintenance Finance Mechanism to ensure maintenance of the open space in perpetuity by the Property

owners at no expense to the City. The approved finance mechanism will be clearly disclosed in the CC&Rs for the project and in any future Homeowners Association documents.

8. Final Map Lots 99-100. To preserve a 25-foot open space between buildings along the public access paseo from Clement Avenue to the waterfront open space as shown on the Development Plan Page DP-4 and TM Page TM-3, prior to final map, the applicant shall deed restrict Lots 99 and 100 so that there will be a "no-build" area of 3 feet on the side of each of those lots facing the public paseo. This deed restricted area will allow for private utility installation.
9. Final Map Easements. The Final map shall grant a permanent Public Access Easement on all lands depicted as public open space on Development Plan page DP-4 and on all internal roads and alleys within the project site.
10. Waterfront Park Construction Phasing. The waterfront improvements depicted on Development Plan pages DP-4, and pages L-1 through L-5 shall be completed to the satisfaction of the Public Works Director prior to the issuance of the 101st Certificate of Occupancy for the 152 market rate townhome and single family homes that are not located within the 30 unit multifamily building.
11. Blanding/Oak Street and Elm Clement Intersections. The improvement plans shall include final striping details for two cross walks across Clement to the project entrance at Elm and the improvements shown on Development Plan page DP-4 at the intersection of Elm and Clement, which shall include four new crosswalks and stop signs as determined necessary for public safety by the Public Works Director.
12. Blanding and Elm Striping. The improvement plans for the Blanding and Elm Street extensions within the project site shall provide a striping plan that provides two 10 foot travel lands and one 6 foot bike lane.
13. Guest Parking. The applicant shall work with city staff and BCDC staff to identify a specific number of visitor parking spaces that will be signed and managed by the Homeowners Association as visitor parking for the park. The improvement plans and Final Map will not include any indication that the public parking on the public right of way on Oak or Clement is dedicated to guest parking.

PUBLIC WORKS CONDITIONS OF APPROVAL

Tract 8060 Specific Improvement Plan Conditions

1. The project shall construct a public sidewalk extending 2 feet onto the project property along Oak Street and Clement Avenue as shown on the Tentative Map. The sidewalk on Clement Avenue shall be 8 feet wide and provide a minimum 48

inch clearance around all joint utility poles that are to remain. The sidewalk on Oak Street shall be a minimum of 5 feet wide. The final width of the Oak Street sidewalk, up to a maximum of 7 feet, will be determined by the City Engineer during review of the Improvement Plans. Either a right-of-way dedication or Public Access Easement shall be granted on the Final Map for the 2 foot wide portion of sidewalk located on the project site.

2. The Blanding Avenue extension and Elm Drive extension shall be private roadways with public access and public utility easements or easements of a form acceptable to the City Engineer dedicated on the final map.
3. Public access and public utility easements of a form acceptable to the City Engineer shall be granted on the Final Map for the Parcel C Open Space.
4. The roadway grades on the Blanding Avenue extension and Elm Drive extension, if feasible, shall be designed to be 5% slope or less within 40 feet of the Oak Street and Clement Avenue intersections, respectively. The final roadway grades or alternative mitigations to provide for a safe intersection shall be to the satisfaction of the City Engineer.
5. Street trees, where required, shall be planted on private property behind the back of sidewalk on Oak Street and Clement Avenue.
6. All permanent retaining walls constructed to create elevated building pads shall be located on private property and not within public right-of-way, access easements or within the Open Space.
7. All utilities shall be undergrounded within the subdivision. Existing utilities along the project frontages, with the exception of the 115kV electrical transmission lines along Clement Avenue, shall also be undergrounded.
8. Street lighting shall be designed in accordance with the City of Alameda Street Lighting Design Guide, latest edition. A photometric study shall be provided with the improvement plans. Any adjustments to bring street lighting up to City standards will be incorporated into final permit drawings. Any new streetlights located on Clement Avenue or Oak Street shall be LED cobra head 30-foot aluminum poles per the City Standard Plans. Streetlights located on the Blanding Avenue and Elm Street extensions shall be post top historic streetlights or as approved by the City Engineer.

Engineering

21. The applicant/developer shall submit construction improvement plans for all on- and off-site improvements, including detailed designs for all wet and dry utilities, landscaping and irrigation, water, grading, drainage, erosion control, paving, and solid

waste storage and recycling areas. The plans shall be prepared, signed, and stamped as approved by a registered civil engineer licensed in the State of California, unless the Public Works Director authorizes them to be prepared by some other qualified professional, and be in accordance with the AMC, the City of Alameda Standard Plans and Specifications, Standard Subdivision Specifications and Design Criteria, and the Master Street Tree Plan. The improvement plans shall be reviewed and approved by the Public Works Director prior to approval of the first final map. The engineer shall also assume responsibility for inspection of the on-site construction work, including but not limited to parking lots, pathways, storm facilities, sewer facilities, etc., and shall certify to the City, prior to acceptance of the work or issuance of any occupancy permit, including a temporary occupancy permit, whichever comes first, that the installation of the on-site work (excluding the building and foundation) was constructed in accordance with the approved plans.

22. Improvement and Grading Plans. Public Improvement and grading plans shall meet Federal Emergency Management Agency (FEMA) and City standards for flood protection and floodplain management in effect at the time of subdivision(s) map approval. Public improvements are to be consistent with the most current BCDC and DTSC requirements. The applicant/developer shall include the recommended improvements into the project's improvement plans prior to approval of the improvement plan and issuance of first (1st) final map.
23. Easements. All existing and proposed easement locations, uses and recording information shall be shown on final subdivision maps. The Owner's Statement shall list all easements to be dedicated and the Certificate of City Clerk shall list all public easements to be abandoned with recording information as part of the Final Map.
24. Landscaping. Prior to issuance of the first Building Permit, the applicant shall submit for Public Works Director review and approval, landscape and irrigation plans for improvements on and off-site signed and stamped as approved, by a licensed landscape architect. The plans shall be in accordance with the most recent version of the "Bay-Friendly Landscape Guidelines" developed by StopWaste.Org and the Bay Friendly Coalition, the Master Street Tree Plan, the AMC, conditions of approval by the Planning Board, and other regional jurisdictions requirements, as applicable.
 - a) Projects required to meet the Bay-Friendly Landscape (BFL) guidelines shall:
 - i. have plans prepared by a qualified BFL design professional;
 - ii. earn, at a minimum, a total of 60 points on the StopWaste.Org "Bay-Friendly Scorecard for Commercial & Civic Landscapes" (Scorecard), as determined by a qualified BFL Rater;
 - iii. complete the nine required practices identified on the Scorecard related to mulch, compost, diversion of construction and demolition

waste, selection of planting materials, and the design and installation of irrigation systems, as determined by a qualified BFL Rater;

- iv. acquire the services of an approved BFL Rater to assist the landscape project design and construction team with gaining BFL qualification, consistent with the BFL qualification requirements of the Bay Friendly Coalition
 - v. pay the required BFL application and final submittal fees via the BFL Rater to the Bay Friendly Coalition
 - vi. Submit to the City written certification from the Bay Friendly Coalition that the design and construction of the landscaping and irrigation meets the most recent version of the "Bay-Friendly Landscape Guidelines" developed by StopWaste.Org and The Bay Friendly Coalition. This certification is required prior to acceptance of the work or issuance of any occupancy permit, including a temporary occupancy permit, whichever comes first.
- b) In addition, the final landscape plans shall ensure that all landscaping and bioswales are compatible with the stormwater treatment measures designed to minimize irrigation and runoff; promote surface infiltration, where appropriate; and minimize the use of fertilizers and pesticides that can contribute to storm water pollution. Also, integrated pest management (IPM) principles and techniques shall be incorporated into the landscaping design and specifications. Where feasible, landscaping shall be designed and operated to treat storm water runoff by incorporating elements that collect, detain, and infiltrate runoff. Plant placement within the site landscape design shall not interfere with the design function of any of the stormwater treatment measures. For example, trees planted in or near a bio treatment swale shall not adversely interfere with the design flow capacity of the swale. The plans shall be reviewed and approved by the Public Works Director prior to approval of the building permit. The landscaped architect shall also assume responsibility for inspection of the work and shall certify to the City, prior to acceptance of the work or issuance of any occupancy permit, including a temporary occupancy permit, whichever comes first, that the installation of landscaping and irrigation in the public right of way was constructed in accordance with the approved plans.

25. To guarantee completion of the required on-site and off-site public improvements the applicant/developer shall enter into an agreement with the City and provide a security in a form and amount acceptable to the City prior to approval of the improvements plans and final map pursuant to Government Code sections 66456.2 and 66499-66499.10.

26. A sanitary sewage flow analysis, acceptable to the Public Works Director, identifying the total peak sanitary sewage flow quantities to be generated by the proposed development shall be prepared by a registered civil engineer licensed in the State of California and submitted as part of the construction improvement plans. The engineer shall sign and stamp the analysis as approved. The applicant/developer will pay for the cost of the analysis. The City will determine the required improvements, if any, to ensure sufficient sewage capacity for this project and anticipated cumulative growth in the associated sewer sub-area. The applicant/developer will include the City and EBMUD recommended improvements, if any, from the sewer study and into the project's improvements plans prior to approval of the first building permit. All permits, easements and/or approvals for modifications to the sewer system required by EBMUD are to be obtained prior to the first Building Permit.
27. Prior to the Final Inspections, the applicant/developer shall comply with the provisions of the EBMUD Regional Private Sewer Lateral Program (which the City is a participant of). Compliance requires that each parcel within the subdivision be issued a Compliance Certificate by EBMUD for the upper lateral. The applicant/developer shall schedule verification testing for the public and private sewer mains, manholes, and other appurtenances in the public or private streets within the subdivision, including lower laterals. The City will provide their acquired test results for sewer infrastructure that is privately-owned to EBMUD so Compliance Certificates can be issued for those parcels (parcels to be owned by Homeowners Association). Review the program requirements and cost for Compliance Certificates at <http://www.easbaypsl.com/>.
28. A storm drainage hydrology analysis, acceptable to the Public Works Director, identifying the total peak drainage flow quantities to be generated by the proposed development shall be prepared by a registered civil engineer licensed in the State of California and submitted as part of the construction improvement plans. The engineer shall sign and stamp the analysis as approved. The City will provide this data to an independent consultant who will assess the impact of the proposed development on the City's storm drainage system using a hydraulic model and determine the improvements, if any, to ensure sufficient capacity for this project and anticipated cumulative growth in the associated drainage basin. The applicant/developer will pay for the cost of the modeling study. The applicant/developer will include the recommended improvements into the project's improvements plans prior to approval of the first Building Permit.
29. Prior to issuance of a final map for the Property, the applicant/developer shall submit a soils investigation and geotechnical report, acceptable to the Public Works Director, in accordance with the AMC and prepared by a registered geotechnical engineer licensed in the State of California, with recommendations based on the findings. The report shall address the structural and environmental analysis of existing soils and groundwater. The improvement plans shall incorporate all design and construction criteria specified in the report. The geotechnical engineer shall sign and stamp the improvement plans as approved and as conforming to their recommendations prior to

approval the first final map. The geotechnical engineer shall also assume responsibility for inspection of the work and shall certify to the City, prior to acceptance of the work that the work performed is adequate and complies with their recommendations. Additional soils information may be required during the plan check of individual house plans. The applicant/developer will include the engineer's recommended improvements into the projects improvement plans prior to approval of the first final map.

30. All required public frontage and street improvements shall be designed, built, and dedicated to the City in accordance with City ordinances and Public Works Department standards and shall include curbs, gutters, sidewalks, street trees, landscaping and irrigation, streetlights, etc.
31. Maintenance of public improvements shall be the responsibility of the new development through an Assessment District and/or Homeowners Association.
32. Applicant/developer shall resurface the existing street pavement whenever a street is cut, either by a longitudinal or transverse cut, for utility or other improvement installations so the street is restored to pre-project conditions. The resurfacing shall extend a sufficient distance beyond any cut to ensure a smooth transition, as determined by the Public Works Director, and shall consist of a one and a half (1½) inch asphalt concrete overlay. Applicant/developer shall also provide digouts and reconstruction of any potholed and/or alligatored areas adjacent to the project.
33. Any retaining walls, which are adjacent to a property line, shall be masonry, metal, or concrete. Wood retaining walls shall not be installed adjacent to property lines. Existing retaining walls are to be evaluated by a geotechnical/structural engineer for integrity. The engineer is to provide design criteria and recommendations for all retaining structures. The applicant/developer will include the engineer's recommended improvements into the projects improvement plans prior to approval of the improvement plan or parcel/final map, whichever comes last.
34. Installation of street paving shall include reconstruction of the existing pavement section to provide adequate conforms. The limits of the reconstruction shall be established by the Public Works Director prior to approval of the improvement plans or parcel/final map, whichever comes first.
35. Exposed soil surfaces shall be periodically sprinkled to retard dust. During construction, the applicant/developer shall ensure that construction crews undertake a program of dust control including, but not limited to, watering soil surfaces as needed to prevent dust blowing, covering trucks carrying materials to and from the site, and frequent clean-up of soil carried by construction vehicle tires from the site onto streets. No City water shall be used for this purpose.

36. All project related grading, trenching, backfilling, and compaction shall be conducted in accordance with City of Alameda Standards and Specifications.
37. Construction activities are restricted to the hours of 7:00 a.m. to 7:00 p.m., Monday through Friday and 8:00 a.m. to 5:00 p.m. on Saturday, unless a permit is first secured from the City Manager or designee based upon a showing of significant financial hardship.
38. Construction equipment must have state of the art muffler systems as required by current law. Muffler systems shall be properly maintained.
39. Noisy stationary construction equipment, such as compressors, shall be placed away from developed areas off-site and/or provided with acoustical shielding.
40. Grading and construction equipment shall be shut down when not in use.
41. During non-working hours, open trenches shall be provided with appropriate signage, flashers, and barricades approved by the Public Works Director to warn oncoming motorists, bicyclists, and pedestrians of potential safety concerns.
42. Prior to trenching within existing street areas, the applicant's engineer shall ascertain the location of all underground utility systems and shall design any proposed subsurface utility extensions to avoid disrupting the services of such system.
43. New street trees shall maintain clearances from utilities as follow: a) Fire hydrant – six feet; b) top of driveway wing – five feet; c) stop signs – 15 feet; d) street/pathway lights and utility poles – 25 feet; e) storm drain, sanitary mains, gas, water, telephone, electrical lines – five feet; f) front of electrical pad-mounted equipment – ten feet. Verify minimum clearance distances of street trees/shrubs from electrical transformers with Alameda Municipal Power (AMP).
44. Approved backflow prevention devices shall be installed on all new and existing domestic, commercial, irrigation, and fire water services and as required by the East Bay Municipal Utility District (EBMUD). These devices must be tested by an AWWA certified tester from a list of testers provided by EBMUD.
45. Costs for inspection by the Public Works construction inspectors during non-working hours shall be at time and one half. Work on Saturdays requiring inspection shall not be done unless approved in advance by the Public Works Director. No work allowed on Sundays. Any work done without inspection may be rejected at the contractor's expense.

Stormwater and Water

46. Prior to issuance of building permits, the applicant/developer shall secure all necessary permit approvals from EBMUD regarding the installation of all water or sewer service connections for the project. Accumulated wastewater must be drained to the sanitary sewer.
47. The applicant/developer shall provide the Public Works Department City Engineer or designee, prior to the issuance of the first combination building/grading permit, a copy of both the WDID# issued to the project site by the State Water Resources Control Board (SWRCB) and the required Storm Water Pollution Prevention Plan (SWPPP). These documents shall provide verification to the City that the project site is in compliance with SWRCB Order No. 2009-0009-DWQ prior to the initiation of construction activity. Information on this State Order is available at: http://www.waterboards.ca.gov/water_issues/programs/stormwater/construction.shtml
48. In compliance with the submittals to the SWRCB, the developer shall submit a Storm Water Pollution Prevention Plan (SWPPP) as part of the improvement plan submittal. The SWPPP shall be reviewed and accepted by the Public Works Director or designee for conformance with the City's erosion and sediment control standards.
49. In compliance with the SWPPP, the developer shall be responsible for ensuring that all contractors and sub-contractors install and regularly maintain all control measure elements required in the project SWPPP during any construction activities.
50. The applicant/developer shall be responsible for ensuring that during any construction activity all contractors and sub-contractors install and regularly maintain erosion control measures and perform Best Management Practices (BMP) as described in the Alameda Countywide Clean Water Program brochures, the San Francisco Bay Regional Water Quality Control Board's Erosion and Sediment Control Field Manual and the California Stormwater Quality Association's Stormwater Best Management Practice, to prevent any pollutants entering directly or indirectly the storm water system. The applicant/developer shall pay for any required cleanup, testing, and City administrative costs resulting from consequence of construction materials entering the storm water system.
51. The construction improvement plans shall incorporate permanent stormwater treatment controls and/or design techniques to manage the quantity and quality of stormwater runoff from a planned development to prevent and minimize impacts to water quality. Efforts shall be taken to minimize impervious surface areas, especially directly connected impervious surface areas. Roof drains shall discharge and drain to an unpaved area wherever practicable. Design techniques may include vegetated swales, vegetated buffer zones, bioretention units, retention/detention basins and

ponds, tree well systems, and the incorporation of pervious surface areas and Low Impact Development (LID) measures. Stormwater treatment measures shall be constructed consistent with the latest version of the Alameda County Clean Water Program's Provision C3 Technical Guidance Manual. Applicants may also refer to the Bay Area Stormwater Management Agencies Association (BASMAA) *Start at the Source Manual* for technical guidance.

52. The landscaping plans shall be designed to minimize runoff, promote surface infiltration where appropriate, and minimize the use of fertilizers and pesticides that can contribute to stormwater pollution. Consideration shall be given to pest-resistant landscaping and design features and the use of integrated pest management (IPM) principles and techniques. Where feasible, landscaping shall be designed and operated to treat stormwater runoff.
53. The applicant/developer shall submit a completed City of Alameda Stormwater Requirements Checklist Form to the Public Works Department for review and approval prior to improvement plan approval. The form shall include a table showing the amount of pervious and impervious area (indicated in units of square feet) prior to and after the proposed development.
54. The applicant/developer shall submit a stormwater drainage management plan showing each storm water LID measure(s) and treatment measure(s) required to be constructed to meet Provisions C.3.c and C.3.d of the City of Alameda's Municipal NPDES storm water permit as part of the improvement plans for the project. This stormwater drainage management plan shall be consistent with the results of the completed City of Alameda Stormwater Requirements Checklist Form, as above. As part of the final improvement plan submittal, the applicant/developer shall submit a stamped, signed certification from a qualified independent civil engineer with stormwater treatment facility design experience, licensed in the State of California, and acceptable to the Public Works Director or designee that indicates the LID and treatment measure design meets the established sizing design criteria for stormwater treatment measures prior to approval of the improvements plans, grading permit, or building permit, whichever comes first.
55. The applicant/developer shall submit a Stormwater Treatment Measures Operations and Maintenance (O&M) Plan as part of the improvement plans for the project. The O&M plan shall include, but not be limited to: treatment measure(s) descriptions and summary inventory; a legible, recordable, reduced-scale (8.5"x11") copy of the Site Plan indicating the treatment measure(s) location(s) and site drainage patterns; treatment measure(s) maintenance requirements and maintenance schedule; detailed description of the integrated pest management principals and techniques and/or Bay Friendly Landscaping Program techniques to be utilized during landscape maintenance to ensure pesticide/herbicide use-minimization in landscaped areas; the approved plant palette list; name and contact information of current maintenance personnel. The O&M Plan shall be reviewed and approved by the Public Works

Director or designee prior to approval of the improvements plans, or grading permit, or building permit whichever comes first.

56. The applicant/developer shall execute a Stormwater Treatment Measures Maintenance Agreement (Agreement) with the City. The Agreement shall include, but not be limited to: the approved O&M plan for all post-construction (permanent) stormwater LID and treatment measures; identification of the party responsible for stormwater LID and treatment measures O&M; assurances of access to inspect and verify the treatment system O&M for the life of the project; and assurances of the submittal of the annual O&M report approved by the City. The Agreement shall be executed between the project owner and the City and recorded prior to the issuance of any occupancy permit, including a temporary occupancy permit. The Agreement shall also be recorded at Project Owner's expense, with the County Recorder's Office of the County of Alameda prior to establishment of the use.
57. The applicant/developer shall submit a certification report (Report) prepared by a registered civil engineer, licensed in the State of California, affirming that all project site stormwater treatment measures have been constructed per the City approved plans and specifications. As appropriate, the Report shall include, but not be limited to, assurances that: imported materials used for the treatment measure(s) are certified by the supplier; installation of these materials is per approved plans and specifications and meets the intent of the design engineer; required on-site testing results conform with approved plans and specifications; treatment measures conform to dimensions, grades and slopes on approved plans and specifications; all structural features of the treatment measures comply with plan specifications; the irrigation system is installed and functions as designed; healthy vegetation/ground cover is installed as shown on plans. The Report shall be submitted in a form acceptable to the Public Works Director or designee, prior to the issuance of any occupancy permit, including a temporary occupancy permit.
58. Fire sprinkler system test water discharges shall be directed to the sanitary sewer system or to appropriately-sized onsite vegetated area(s).
59. All new storm drain inlets shall be clearly marked with the words "No Dumping! Drains to Bay," or equivalent, as approved by the City Public Works Department. Permanently affixed thermoplastic, metallic or plastic laminate style markers are examples of acceptable forms.

Traffic and Transportation

60. For any off-street parking facilities associated with the Development Plan, the applicant/developer shall submit the improvement plans to the Public Works Department for review and approval. The plans shall be prepared by a registered civil engineer licensed in the State of California and shall be signed and stamped as approved. The Public Works Department shall review and approve the parking lot

layout, including number of spaces, dimensions of spaces, and Americans with Disabilities Act (ADA) requirements, and the Public Works Director shall review and approve the internal and external circulation of the parking lot, including lane widths and access points, prior to approval of building permits.

61. Final bicycle parking plan shall be consistent with AMC-30-7.15 Bicycle Parking.
62. At least three weeks prior to the commencement of work within the public right-of-way that affects access for pedestrian, bicyclist, and vehicular traffic, the applicant/developer shall provide a Traffic Control Plan (TCP) to the Public Works Department for review and approval. The TCP shall be prepared by a registered civil engineer or traffic engineer licensed in the State of California, and be in accordance with the California MUTCD standards. The engineer shall sign and stamp the plans as approved. In general, any vehicular, transit, bicyclist, and pedestrian access through and/or adjacent to the project site shall remain unobstructed during project construction or an ADA compliant alternative route established as approved by the Public Works Director. At locations where adequate alternate access cannot be provided, appropriate signs and barricades shall be installed at locations determined by the Public Works Director and Police Chief. Should transit routes be affected, applicant/developer is required to receive prior approval from AC Transit of any proposed traffic detours or temporary closure of bus stops. Constructed work may not commence until the TCP is approved by the Public Works Director.
63. Pedestrian and vehicular lighting at all intersections and project driveways shall meet AMP standards for crosswalks, sidewalks, and intersections.
64. Applicant/developer shall replace any damaged curb, gutter, and sidewalk along street frontages to the satisfaction of the Public Works Director, in accordance with the Public Works Department's Standard Plans and Specifications prior to acceptance of the project.

Environmental

65. If external enclosures are proposed, design of all external enclosures for solid waste, recycling, and organics shall be approved by the Public Works Director prior to approval of the building permit.

These facilities are to be designed to prevent water run-on to the area, runoff from the area, and to contain litter, trash, and other pollutants, so that these materials are not dispersed by the wind or discharged to the storm drain system. External enclosures are to be roofed and/or enclosed. Any enclosures containing food waste shall have floor drains connected to the sanitary sewer system. If the enclosures are attached to buildings they shall have fire sprinklers. Internal collection and storage area(s) and the individual bins and containers provided, shall be adequate in capacity, number,

and distribution to serve the anticipated demand for trash, recycling, and organics as determined by the Public Works Director.

A 40-foot turning radius must be provided for any street that would otherwise require the collection truck to back up a distance greater than 150 feet. The storage area(s) shall be accessible to residents and employees. Each storage area within a residential development shall be no more than 250 feet from each dwelling unit.

66. Driveways or aisles shall provide unobstructed access for collection vehicles and personnel and provide at least the minimum clearance required by the collection methods and vehicles used by the City's designated collector or hauler. In all cases where a parcel is served by an alley, all exterior storage areas shall be directly accessible to the alley.

67. The storage area(s) and the individual bins and containers provided within shall be adequate in capacity, number and distribution to serve the anticipated demand as determined by the Public Works Director.

68. The design and construction of the storage area(s) shall:

- a. Be compatible with the surrounding structures and land uses; and
- b. Be properly secured to prevent access by unauthorized persons. If gates with locks are planned to limit access to the enclosure or to the Property, cards or keys must be provided to the City's franchised waste hauler and recycling collector; at present Alameda County Industries; and
- c. Contain a concrete pad within the fenced or walled area(s) and a concrete apron which facilitates handling of the individual bins and containers; and
- d. Provide a 6-inch wide curb or parking bumpers along the interior perimeter of the enclosure walls to protect them from damage by the dumpster. A 6-inch wide parking bumper, at least 3 feet long, should also be placed between the refuse dumpster and the recycling containers; and
- e. Maintain a minimum space of 12 inches between the dumpster(s) and the walls of the enclosure and the recycling container(s) to allow for maneuvering the dumpster(s); and
- f. Protect the area(s) and containers from adverse environmental conditions, which might render the collected materials non-collectible, noxious, unsafe, or in the case of recyclable materials, unmarketable.

69. Dimensions of the storage area(s) shall accommodate containers consistent with the current methods of collection. The storage area(s) shall be appropriately located and screened from view on at least three sides by a solid wall six feet in height, and on the fourth side by a solid gate not less than five feet in height. The gate shall be maintained in good working order and shall remain closed except when in use. Gates

must open straight out and gates and hinges must be flush with the enclosure wall to allow adequate maneuverability of the dumpster in and out of the enclosure. The design of the wall and gate shall be architecturally compatible with the surrounding structures.

70. A sign clearly identifying each exterior solid waste and recyclable material storage area and the accepted material(s) is required. Each sign shall not exceed two square feet in area and shall be posted on the exterior of the storage area adjacent to all access points.
71. All solid waste containers, including dumpsters and individual cans or carts, must have fitted lids which shall remain closed at all times when the container is not being used or emptied.

ALAMEDA MUNICIPAL POWER (AMP)

72. Concurrent with submittal of Improvement Plans, the Applicant shall coordinate with the AMP regarding power requirements. All submittals shall refer to AMP's "Material and Installation Criteria for Underground Electrical Systems" for minimum clearances of street trees/shrubs from electrical transformers and other utility electrical equipment.
73. The Applicant shall provide and install street lighting consistent with AMP's standard specifications and AMP's "Material and Installation Criteria for Underground Electrical Systems". No signs shall be installed or mounted on street light poles owned and/or maintained by AMP. All new signs shall be installed on dedicated sign posts.
74. The Applicant shall provide all necessary on-site underground substructures, including conduits, pull boxes, transformer pads, etc. per the AMP specifications. AMP will require easements for all transformers, primary and secondary boxes, and conduits. AMP will furnish and install all required transformers, high voltage distribution cables, and secondary cables. The Applicant shall be reimbursed for improvements pursuant to the standard AMP agreement.
75. The Applicant shall furnish and install code-sized service cables in code-sized conduit from each house to the nearest secondary pillbox. AMP will connect the service to the secondary distribution system.
76. The Final Map shall show all necessary easements and access to all electrical utility facilities that are in the private properties, at no charge to AMP.
77. Prior to issuance of Certificate of Occupancy, the Applicant shall furnish and install service equipment for each house. The service equipment shall meet Electric Utility Service Equipment Requirement Committee standards.

78. Concurrent with acceptance of work by City Council, the applicant/developer shall dedicate and AMP shall take over ownership and will be responsible for maintaining all new substructures for under grounding primary and secondary circuits, and distribution transformers once the improvements have been inspected and found to have been properly installed. The Applicant or successor property owners shall be responsible for the service cables and service equipment.
79. The Applicant/developer shall be responsible for all expenses involved in the on-site duct/joint trench system including engineering design, plan check, and electrical construction inspection. The Applicant shall be responsible for the cost of AMP assigned inspector during construction
80. The Applicant/developer shall submit, with the on-site improvement plans, detailed drawings showing the required on-site electric utility facilities.

ALAMEDA FIRE DEPARTMENT

81. Prior to approval of the Improvement Plans, the applicant shall submit plans for review and approval by the Public Works Director and the Fire Chief, that:
- a. Provide fire hydrants spaced throughout the project as required in California Fire Code Appendix C. Ensure sufficient fire flow for the development compliant with California Fire Code Appendix B and Alameda Municipal Code 15-1.2 where a maximum reduction of the fire flow can be reduced by 50% for fire sprinklered buildings.
 - b. Fire hydrant flows shall be a minimum of 1,500 G.P.M. from any one hydrant;
 - c. Provide adequate turn around space or acceptable emergency vehicle through access for any street greater than 150 feet in length;
 - d. Ensure that all roads that are required to be fire access roads have an adequate turning radii for fire apparatus (inside turning radius of 28" and outside turning radius of 44'8"); and
 - e. For all roads designated as fire access roads shall have a minimum fire lane width of 26' unobstructed. Should parking within the fire lane be required, the fire lane width is to be increased to an additional 8' per side for vehicle parking.

Prior to approval of the first Building Permit, the Applicant shall submit improvement plans for the project site Fire Water System. The system shall be designed to the satisfaction of EBMUD. The Applicant shall be responsible for the placement of on-site hydrants. The location and number of hydrants shall be established in improvements plans.

82. All private streets and courts within the development shall be marked as fire access roads to the satisfaction of the Fire Chief. No on-street parking shall be permitted within private streets and courts in the 20 foot wide fire lane.
83. The improvement plans and building permit plans shall include fire sprinkler systems within each residential unit including the garage and attic spaces.
84. The improvement plans and building permit plans shall, to the satisfaction of the Fire Chief and City Engineer, includes design each street and/or alley to handle the maximum weight of the heaviest equipment that may be used by the Fire Department for emergency purposes. Parking shall be prohibited within the streets and alleys dedicated as fire lanes by the Fire Chief, and the CC&R's shall include an enforcement mechanism.
85. Indemnification: The applicant shall defend (with counsel acceptable to the City), indemnify, and hold harmless the City of Alameda, the Alameda City Planning Board and their respective agents, officers, and employees from any claim, action, or proceeding (including legal costs and attorney's fees) against the City of Alameda, , Alameda City Planning Board and their respective agents, officers or employees to attack, set aside, void or annul, an approval by the City of Alameda, the Planning Building & Transportation Department, the Alameda City Planning Board, or City Council relating to this project. The City shall promptly notify the applicant of any claim, action or proceeding and the City shall cooperate in such defense. The City may elect, in its sole discretion, to participate in the defense of said claim, action, or proceeding.

EXHIBIT A

TENATIVE MAP

SUBVERSION BOUNDARY
 PARCEL LINE
 EASEMENT LINE
 EXISTING SANITARY SEWER LINE
 EXISTING WATER LINE
 EXISTING STORM DRAIN LINE
 EXISTING ELECTRICAL OVERHEAD LINE
 TOTAL
 SPOT ELEVATIONS
 CITY OF ALABAMA
 EAST BAY MUNICIPAL UTILITIES DISTRICT

ALL EXISTING ON-SITE BUILDINGS AND PAVEMENT HAVE BEEN REMOVED.



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SCALE: 1" = 40'

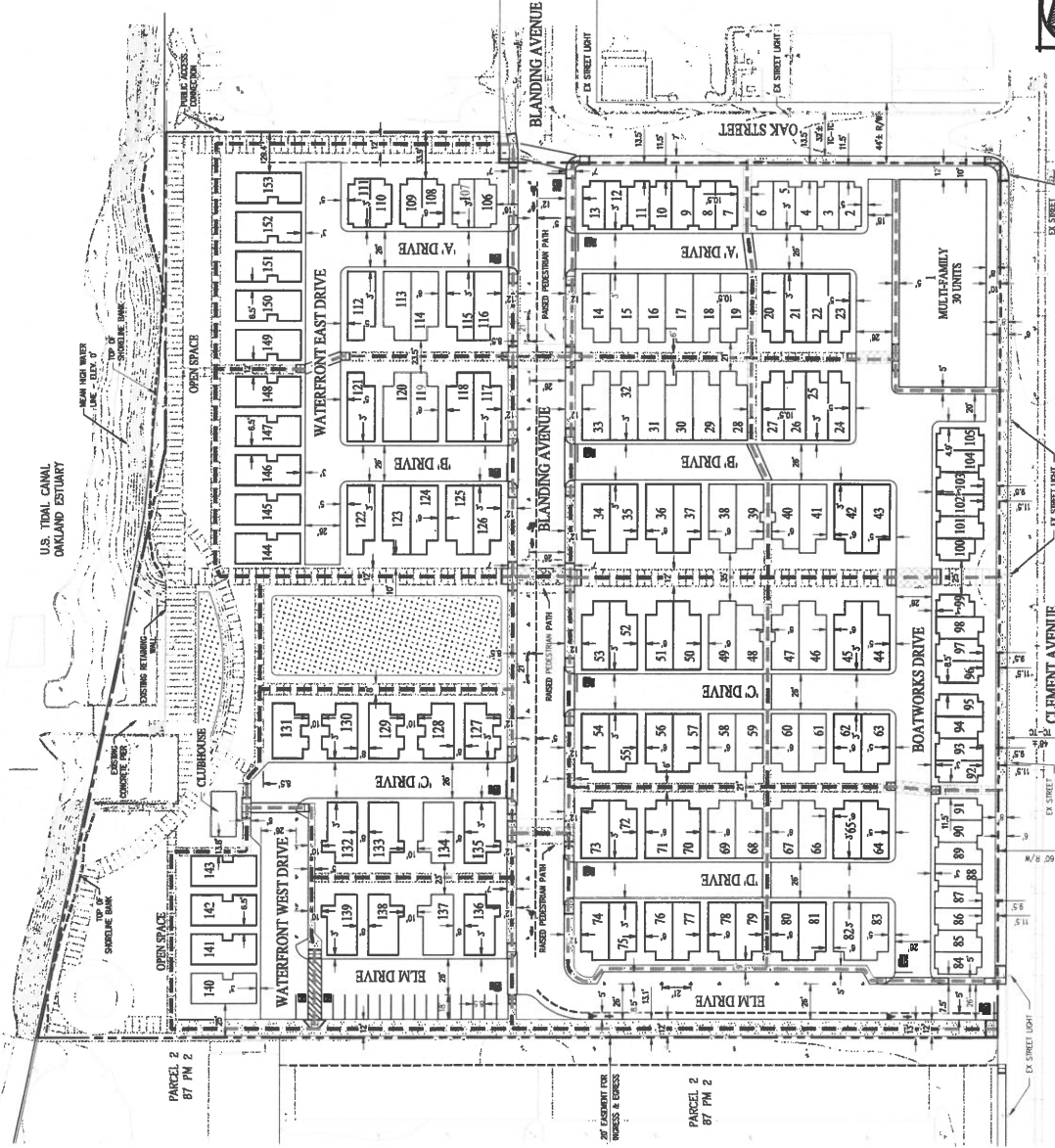
LEGEND

- PROPOSED PROPERTY BOUNDARY
- ACCESSIBLE PARK
- CONCRETE SIDEWALK
- ENHANCED PARKING
- BIPO-REVISION
- PARALLEL PARKING STALL
- ACCESSIBLE PARKING STALL
- "STOP" PAVEMENT MARKING WITH 12" WHITE LINE (LETTERING TO BE 4" MIN BEHIND STOP BAR)
- RIGHT OF WAY
- TOP OF CURB

| GUEST PARKING SUMMARY | | |
|-----------------------|----------------|--|
| LOCATION | PROVIDED TOTAL | |
| ON-SITE GUEST | 46 | |
| TOTAL | 46 | |
| OAK STREET | 0 | |
| CLEMENT AVENUE | 24 | |
| TOTAL | 24 | |

NOTES

- SEE SHEET 4.0 FOR TYPICAL PRIVATE STREET SECTIONS



TRACT 8060 TENTATIVE MAP PRELIMINARY SITE PLAN BOATWORKS

CITY OF ALAMEDA ALAMEDA COUNTY CALIFORNIA



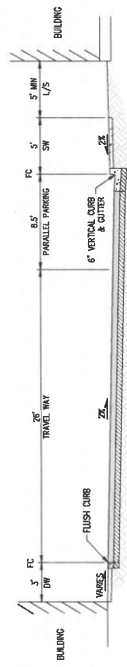
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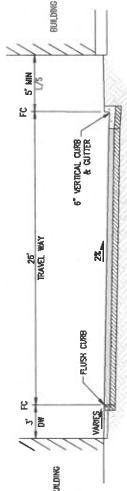


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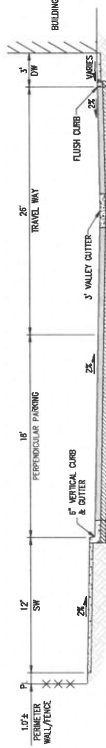
SHEET NO.
TM 3.0
OF 18 SHEETS



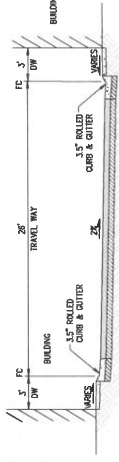
WATERFRONT WEST DRIVE
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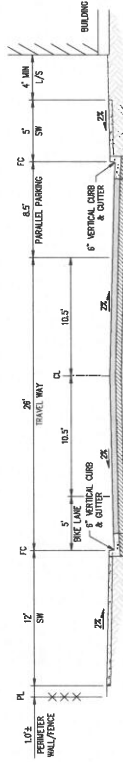
WATERFRONT EAST DRIVE
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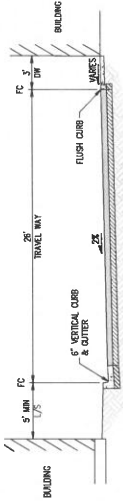
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NOT TO SCALE



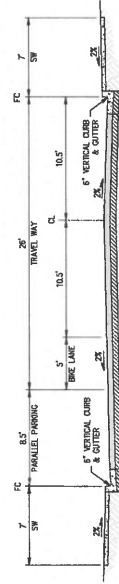
A DRIVE, B DRIVE, C DRIVE, D DRIVE
NOT TO SCALE



ELM DRIVE (SOUTH OF BLANDING)
NOT TO SCALE



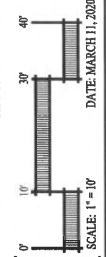
BOATWORKS DRIVE
NOT TO SCALE



BLANDING AVENUE
NOT TO SCALE

TRACT 8060 TENTATIVE MAP TYPICAL PRIVATE STREET SECTIONS BOATWORKS

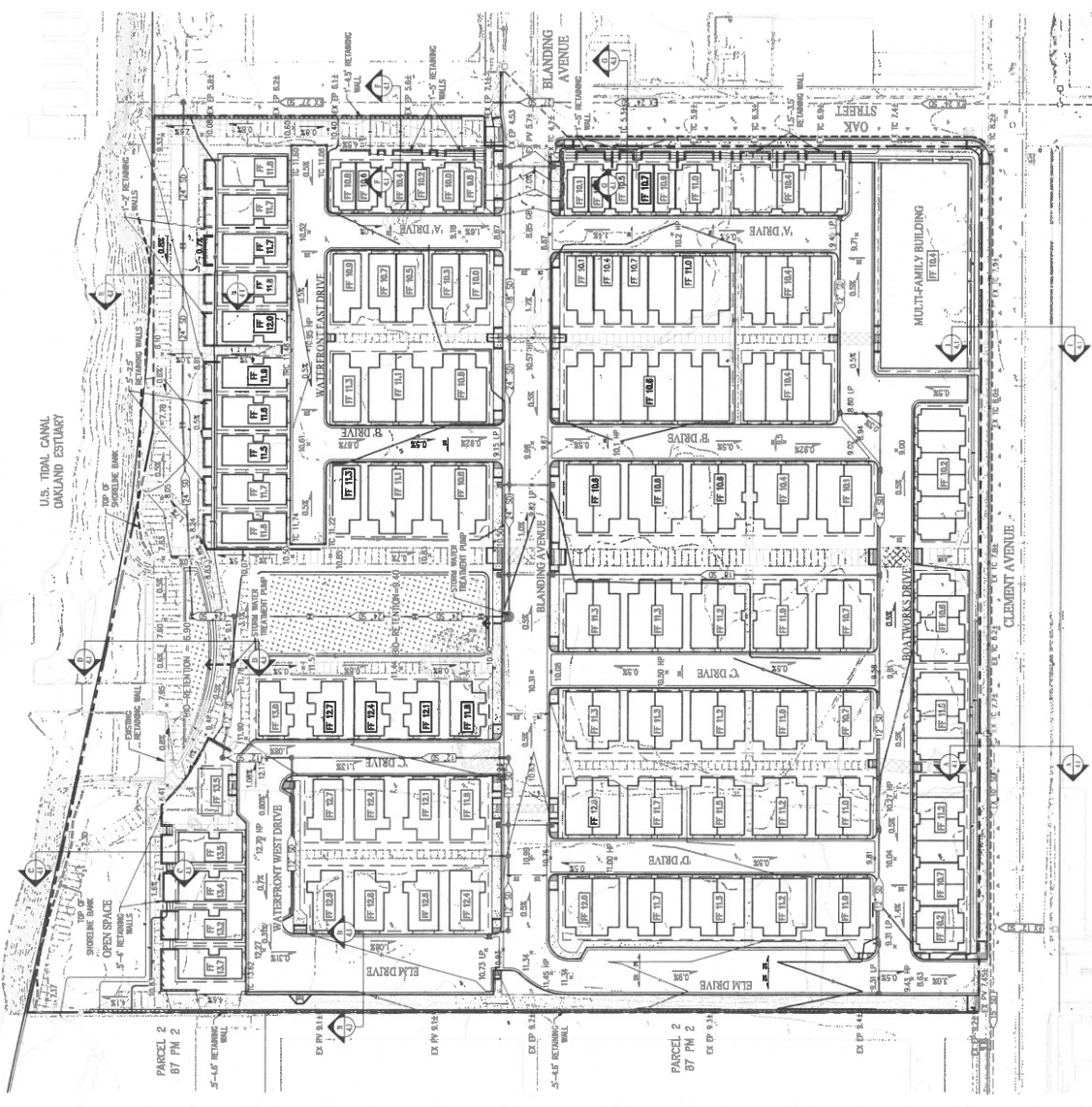
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SHEET NO.
TM 3.1
OF 10 SHEETS

DATE: MARCH 11, 2020



LEGEND:

| EXISTING | PROPOSED | DESCRIPTION |
|----------|----------|-------------------------|
| --- | --- | STORM DRAIN |
| □ | ■ | STORM DRAIN CATCH BASIN |
| ○ | ● | FIELD INLET |
| | ● | MANHOLE |
| | → | SPILL-RETENTION AREA |
| | → | DIRECTION OF FLOW |
| | HP | HIGH POINT |
| | LP | LOW POINT |
| | CB | GRADE BREAK |
| | EX | EXISTING |
| | PR | PROPOSED |
| | TC | TOP OF CURB |
| | FF | FINISHED FLOOR |
| | RF | RETAINED FOOTING |

NOTES:

1. STORM WATER TREATMENT PUMPS SHALL BE DESIGNED TO ACCOMMODATE C.J. TREATMENT PLANTS ONLY.
2. RETAINED FOOTINGS SUBJECT TO FINAL DESIGN AND STRUCTURAL CONSULTATION.

TRACT 8060
TENTATIVE MAP
PRELIMINARY GRADING AND DRAINAGE PLAN
BOATWORKS

CITY OF ALAMEDA ALAMEDA COUNTY CALIFORNIA

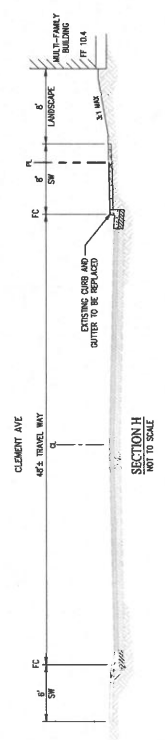
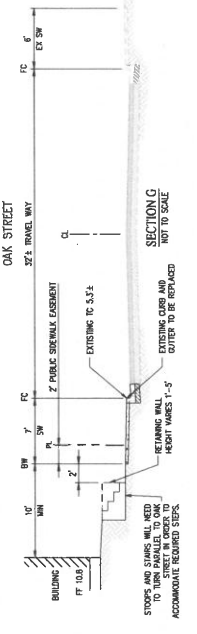
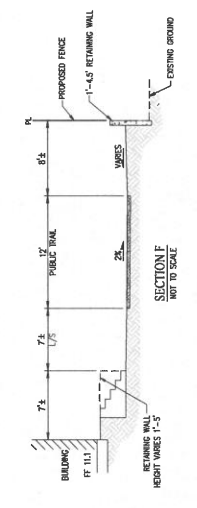
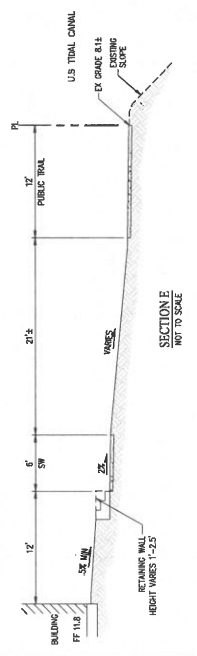
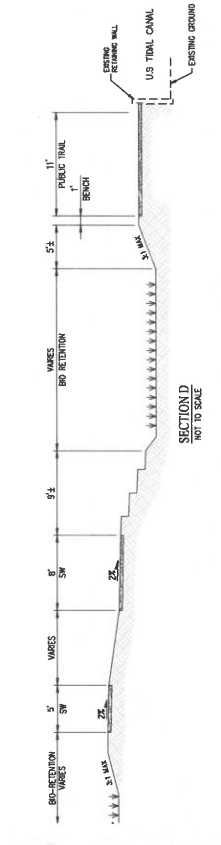
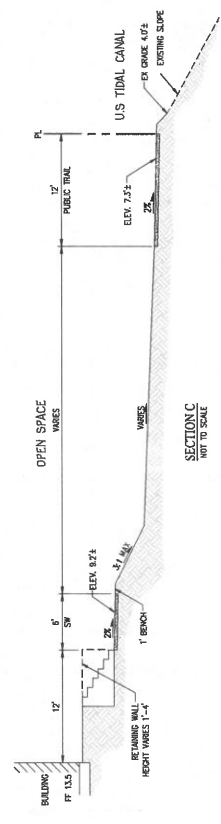
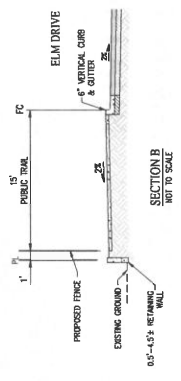
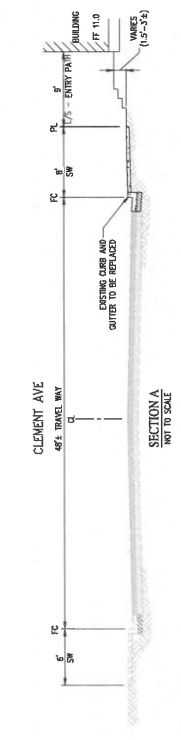
SCALE: 1" = 40'

DATE: MARCH 11, 2020

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SHEET NO.
TM 4.0
OF 19 SHEETS

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TRACT 8060 TENTATIVE MAP GRADING CROSS SECTIONS BOATWORKS

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DATE: MARCH 11, 2020

SHEET NO.
TM 4.1
OF 10 SHEETS

07/18/2020

PARCEL: 2
87 PM: 2

LEGEND

EXISTING

PROPOSED

--- 42" SD ---

--- 24" SD ---

--- 18" SD ---

--- 12" SD ---

--- 8" SD ---

--- 6" SD ---

--- 4" SD ---

--- 3" SD ---

--- 2" SD ---

--- 1" SD ---

--- 0.5" SD ---

--- 0.25" SD ---

--- 0.125" SD ---

--- 0.0625" SD ---

--- 0.03125" SD ---

--- 0.015625" SD ---

--- 0.0078125" SD ---

--- 0.00390625" SD ---

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LEGEND

EXISTING

PROPOSED

--- 42" SD ---

--- 24" SD ---

--- 18" SD ---

--- 12" SD ---

--- 8" SD ---

--- 6" SD ---

--- 4" SD ---

--- 3" SD ---

--- 2" SD ---

--- 1" SD ---

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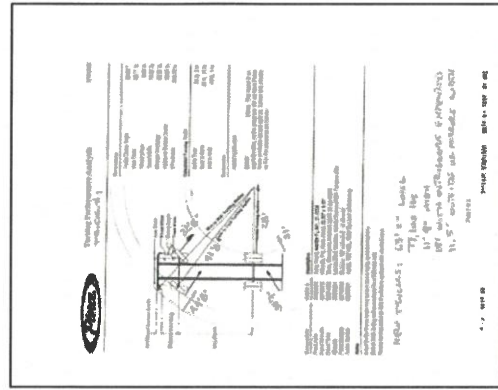
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CITY OF ALAMEDA
PIERCE FIRE TRUCK
NOT TO SCALE

TRACT 8060
TENTATIVE MAP
FIRE ACCESS AND SERVICE PLAN
BOATWORKS

CITY OF ALAMEDA ALAMEDA COUNTY CALIFORNIA



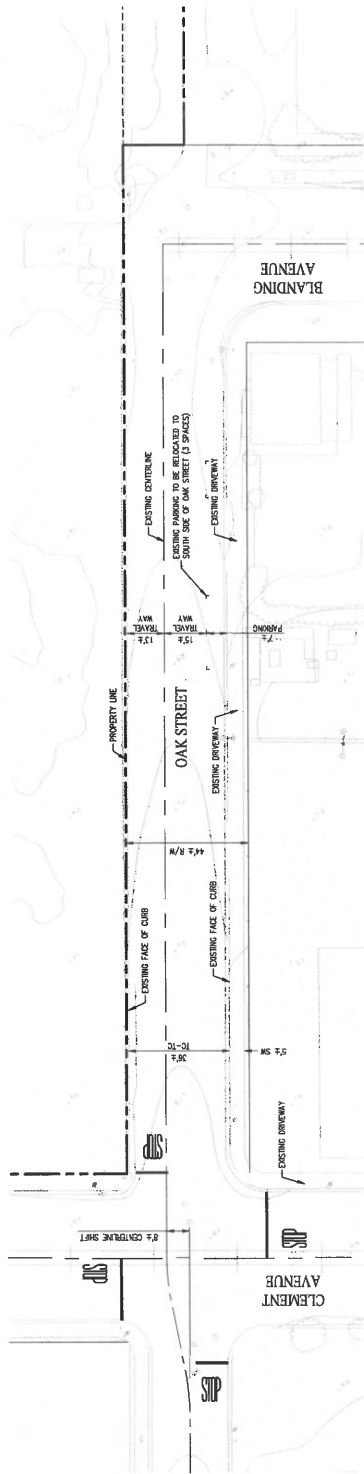
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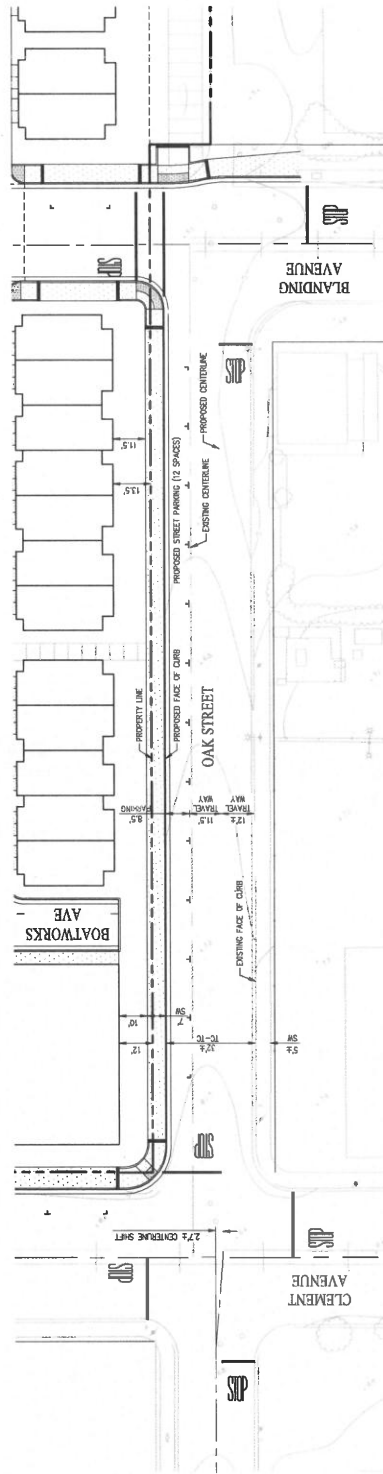


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SHEET NO. TM 7.0 OF 10 SHEETS



EXISTING OAK STREET ALIGNMENT



PROPOSED OAK STREET ALIGNMENT

LEGEND

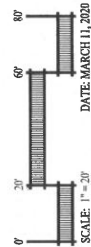
- EXISTING PROPERTY BOUNDARY
- STREET CENTERLINE
- PROPOSED CURB LINE
- STREETING
- CONCRETE SIDEWALK
- ENHANCED PAVING
- PARALLEL PARKING STALL

NOTES

- INTERSECTION LAYOUT AND STRENGTH IS PRELIMINARY AND SUBJECT TO FINAL DESIGN.
- PROPOSED OAK STREET ALIGNMENT SHOWN AS SPECIFIED BY PROJECT CONDITIONS OF APPROVAL.

TRACT 8060 TENTATIVE MAP OAK STREET ALIGNMENT BOATWORKS

CITY OF ALAMEDA ALAMEDA COUNTY CALIFORNIA



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CIVIL ENGINEERS • SURVEYORS • PLANNERS

SHEET NO.
TM 8.0
OF 10 SHEETS

DATE: MARCH 11, 2020

U.S. TIDAL CANAL
OAKLAND ESTUARY

SEAWALL WITH WATER
LINE - DUE TO
TOP OF DRAINAGE BANK

EXISTING
CONCRETE PER
SHORELINE BANK

PARCEL 2
87 PM 2

20' EASEMENT FOR
WALKWAY & LOADING

PARCEL 2
87 PM 2

WATERFRONT WEST DRIVE

WATERFRONT EAST DRIVE

ELM DRIVE

BLANDING AVENUE

BLANDING AVENUE

ELM DRIVE

C DRIVE

D DRIVE

A DRIVE

A DRIVE

BOATWORKS DRIVE

CLEMENT AVENUE

MULTIFAMILY
30 UNITS

LEGEND

OPEN SPACE AREA

| OPEN SPACE AREA | |
|--------------------------|------------|
| LOCATION | AREA (ACS) |
| OPEN | 2.0 |
| BEYOND PROPERTY BOUNDARY | 0.5 |
| TOTAL | 2.5 |

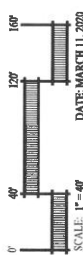
TRACT 8060
TENTATIVE MAP
OPEN SPACE DIAGRAM
BOATWORKS

CITY OF ALAMEDA ALAMEDA COUNTY CALIFORNIA



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SHEET NO.
TM 9.0
OF 14 SHEETS

EXHIBIT B

DENSITY BONUS APPLICATION

March 10, 2020

City of Alameda, Planning Department, Attn: Dept. Head, Andrew Thomas
2263 Santa Clara Avenue, Alameda, CA 94501

RE: Planning Application / Density Bonus Application for Alameda Boatworks Green
2229 Clement Avenue -2235 Clement Ave
APN's 71-289-5, 71-290-1

Dear Mr. Thomas:

The Alameda Boatworks Green property at 2229-2235 Clement Ave. is a 9.48 acre site backing up the Oakland Estuary and currently zoned R2-PD, with an MU-5 General Plan Designation. Boatworks, LLC ("Developer") proposes 161 market rate homes and apartments and 21 affordable apartments to be allocated as follows: 13 apartments for Very Low Income families, 8 apartments for Moderate Income families, all located in a multi-family apartment building at the southeast corner of the site.

A. Base Plan:

Included with this application is a Development Plan that depicts a 140-unit Base Plan proposing construction on a project site that lies in the R-2 (Two-Family Residence) District with a PD (Planned Development Combining District) Zoning Overlay. The planned development zoning allows the City the flexibility to vary the lot area, lot width, building coverage, and yard size/setback requirements from the general standards applicable in the R-2 zone in order to accommodate the project as proposed. See Alameda Municipal Code ("AMC") §30-1.13(e).

The project as proposed exceeds the development standards of the R-2 zone with respect to open space. See Alameda Municipal Code ("AMC") 30-4.2(d)(9). The R2 zone would normally require 0.96 acres of common open space for 140 lots and 1.25 acres of common open space for 182 lots. The proposed planned development project includes in excess of 2 acres of publicly accessible open space.

To conform to the City's Inclusionary Housing requirements, the project provides a total of 15% affordable housing units (21/140 units per the above referenced mix of Very Low and Moderate Income families housing). By providing 9.3% (13/140) of the units for Very Low Income families, the project qualifies pursuant to Government Code §65916(f)(2) and AMC §30-17.7.1(b) for a 30% Density Bonus, allowing a total of 182 units altogether.

| PROPERTY | AREA (SF) | ACRES |
|----------------------------|-----------|------------------------------------|
| PROPERTY PROJECT SITE | 415,568 | 9.54 |
| Common Open Space Provided | 2,000 | 2.00 |
| | 7.54 | Available for Dwelling Units |

| DWELLING UNITS | | Min. Lot Size | Minimum Base Area Required(SF) | Minimum Base Acres Required |
|--------------------------------|-------|---------------|--------------------------------------|--------------------------------|
| BASE | 140 | 2,000 | 280000 | 6.43 |
| INCLUSIONARY UNITS PROVIDED | 21 | | | |
| INCLUSIONARY UNIT % | 15% | | | |
| % Very-Low Income Units | 9 | | | |
| DENSITY BONUS | + 30% | | | |
| BONUS UNITS | 42 | | | |
| TOTAL DWELLING UNITS | 182 | | | |

| Market Rate | UNITS |
|-------------|-------|
| Base | 119 |
| Bonus | 42 |
| Moderate | 8 |
| Very Low | 13 |
| TOTAL | 182 |

B. Waivers:

As shown above, Developer qualifies for waivers pursuant to Government Code §65916(e)(1) and AMC §30-17.12,. Pursuant to the goals and objectives of the State of California's Density Bonus Law and the Alameda Municipal Code, Developer requests waivers from certain development standards that "physically preclude[e] the construction" of the project. Gov Code 65916(e)(1); AMC 30-17.12: The following list of requested waivers are required to construct the Project at the densities being requested by Developer, and are necessary to support the provision of affordable housing units:

1. **Waiver from prohibition on multifamily housing** to allow for the construction of the multifamily building and the attached townhomes.
 - a. This waiver is necessary to allow construction of the number of units permitted pursuant to the density bonus in the space allowed in light of the additional publicly accessible open space the project is providing.
2. **Wavier from the 2,000 square foot minimum lot size** to allow for homes to be built on lots less than 2,000 square feet.
 - a. This waiver is necessary to allow construction of the number of units permitted pursuant to the density bonus in the space allowed in light of the additional publicly accessible open space the project is providing.
3. **Waiver of the development height standards of the R-2 zone** to allow for:
 - a. Single family homes and townhomes with a maximum height of 45'
 - b. A multi-family/multi-story building with a maximum height of 65'
 - c. The slight height waivers requested in (a) and (b) above permit construction of the number of units promised, in a variety of sizes and configurations, within the relatively small area provided by the project site.
4. **Universal Design Waivers.** The statewide standards established in "California Building Code Chapter 11A Housing Accessibility" do not apply to single-family homes or duplexes. See 1101A.1 and 1102A.1. But they do cover all newly constructed multi-family dwellings. California Building Code Administrative section 1.1.7.3.1 exempts detached one- and two-family dwellings and townhouses not more than three stories from the California Accessibility requirements. AMC section 30-18.2 defines "Accessible" with reference to Chapter 11A of the California Building Code – i.e. as "standards for features or fixtures, designs, or other improvements,[sic] which are equal to or exceed the minimum requirements of Chapter 11A of the California Building Code." AMC section 30-18.2 applies to any "development that includes one or more new residential dwelling units," but it is not clear that it applies to all new residential dwelling units within such developments.

AMC 30-18 applies its universal design "Visitability" standards to "all new residential dwelling units *subject to the provisions of this section*," (AM 30-18.4(a) (emphasis added)), but Universal Design (AMC 30-18.4(b)) requires "30% of all new residential units" (with no restriction) to comply. Chapter 11A of the California Building Code establishes different (and more lenient) standards for accessibility *within* a dwelling unit in a multi-family building than it does for access to the multi-family building as a whole. See, e.g., Chapter 11A, Division II (Exterior Facilities) and III (Building Features) vs. Division IV (Dwelling Unit Features). The different requirements do not translate easily to the context of the single family homes or duplexes proposed as part of the Boatworks project, and constructing numerous single family

homes that are as fully accessible as multi-family apartment buildings adds considerable cost to the project. In some places, for instance on Oak Street, where the lots abut a street that is at a lower elevation, the topography and site constraints make an accessible exterior entrance impossible.

The project is able to provide affordable AND accessible units by locating 30 units in a multi-family building served by an elevator. In addition, the project will provide several more accessible duplex units throughout the project wherever topography allows for a roll-in entrance.

With respect to interior universal design, Division IV of Chapter 11 requires covered dwelling units to provide doors to the exterior that are at least 32 inches wide and to provide "accessible" routes (that are 36 inch-wide) to a powder room on the first floor. Cal. Building Code §§ 1119A, 1125A, 1126A, 1132A. Chapter 11A of the California Building Code allows interior stairways. See §1123A. A powder room must provide sufficient maneuvering space for a wheelchair to enter and close the door. The definition of "Accessible" in Chapter 11A, thus allows the interior of units to be "adaptable" for access by people with disabilities who require features of accessibility, but does not require absolute accessibility to all areas of the residence.

a. **AMC 30-18.4a Visitability.** To the extent Visitability requirements govern units outside the multi-family building on this project, Developer requests a waiver from the 100% Visitability requirement to allow, instead, 101 of the 182 residential units (55% of all units) to meet all criteria of accessibility.

- i. 100% of the affordable housing units will meet this requirement.
- ii. 25% of all the units meet all visitability criteria except for the powder room dimension requirements
- iii. 20% of all the units meet all visitability criteria except they do not have accessible exterior access due to topographical constraints.

b. **AMC §30-18.4(b) Universal Design.** Boatworks will meet the requirement of universally designing 30% of the units.

C. To the extent the PD zoning does not provide adequate flexibility, Boatworks requests waivers to allow:

- i. Reductions to required Lot Areas, Lot width, Building Coverage, and yards (front, side, and rear) and Off-Street Parking
- ii. Increases to allowable Building Heights and Densities
- iii. To the extent the City considers the Open Space the project provides to be inadequate to meet the requirements of the R2-PD zone per the minimum requirements of AMC 30-5.12(c), Boatworks requests a waiver to permit the project to be developed with the open space as shown on the Development Plan.

D. Request for Waiver to Reduce Parking Ratio

To the extent the PD zoning does not provide adequate flexibility, pursuant to AMC 30-17.3 and the density bonus laws, Boatworks qualifies for the following reduction in the required on-site parking:

- a. Studio and one-bedroom units: one parking space
- b. Two to three bedroom units: two parking spaces

As an additional waiver or concession (pursuant to 30-17.3(c), Boatworks requests that for its 4-5 bedroom units Boatworks also provide two (rather than 2.5) parking spaces per unit

As a further waiver or concession, Boatworks requests for all units in the multi-family building and for some three-bedroom units, the project will provide one parking space.

E. Incentives and Concessions:

As part of its density bonus, Boatworks qualifies for concessions and incentives pursuant to Government Code §65916 and AMC §30-17.9. Section 65916(k) defines an incentive or concession as: "[a] reduction in site development standards or modification of zoning code requirements [etc]... that results in identifiable and actual cost reductions, to provide for affordable housing costs" and "[o]ther regulatory incentives or concessions proposed by the developer, [city, or county] that result in identifiable and actual cost reductions to provide for affordable housing costs...."

Government Code section 65916(d)(2) sets a minimum number of incentives or concessions that a city must grant if requested. But Government Code section 65916(n) clarifies that: "If permitted by local ordinance, nothing in this section shall be construed to prohibit a city, county, or city and county from granting a density bonus greater than what is described in this section for a development that meets the requirements of this section." Consequently, while the state law

requires the City to provide at least one incentive or concession (see 65916(d)(2)(a)), nothing in the AMC precludes the City from exercising its discretion to provide a greater number of incentives or concessions in order to facilitate the development of housing and, in particular, affordable housing. See, e.g., AMC §§30-17.9(a) and 30-17.10(b). Furthermore, AMC §30-17.10 provides that

For large development projects, defined as projects on sites with at least one acre of land area, an applicant may be granted exceptions to the caps and limits set forth in subsection 17.10(b) through the density bonus application process if it can be shown such exceptions are needed to allow more flexibility that promotes superior site design and architectural excellence.

Boatworks requests the following incentive or concession:

1. **Relief from Affordable Housing Distribution Requirements.** Pursuant to Alameda Code Section 30-17.8.(a)(1)-(2) requirement that affordable housing units be integrated into the project and "reflect the range and number of bedrooms provided in the project as a whole." The Boatworks project does provide affordable housing units that reflect a range of affordable housing unit sizes and number of bedrooms. As shown on the Development Plan, the affordable units vary in size from 1,403 sf to 420 sf. while the market rate residences range from 2,882 sf to 1,098 sf. The floorplans of market rate units vary from 1 bedroom/1 bath to 4 bedroom/4 bath residences. The floorplans of the affordable units vary from 1 bedroom/1 bath to 3 bedroom/2 bath. To the extent the City considers that this variety of unit types does not adequately "reflect the full "range and numbers of bedrooms provided in the project as a whole," Boatworks requests a waiver from this standard as an incentive to provide the extremely affordable units it proposes.

F. Description of Market-Rate and Affordable Residences

The Boatworks proposal collects all the affordable housing units into the multi-family housing apartment building at the intersection of Clement Ave. and Oak St. This approach provides accessibility to all the units (via elevator) and locates the units as close as possible on the site for comfortable walking distance to the civic resources of the Library, the City Hall, and the Police Station, as well as to the variety of services of the downtown Park St. commercial center. All units are constructed with modern, durable, high quality finishes, appliances and fixtures throughout. Exteriors and landscaping of both market rate and affordable homes will be shared in common and maintained through the Homeowners Association. Maintenance manuals will be provided to all residents detailing procedures and responsibilities for their residences.

G. Timing of Market Rate and Affordable Sales and Occupancy

All market rate and affordable residences in the project will be offered for sale. The affordable units will all be built in construction Phase 1 of the project with an estimated completion date ready for sale approximately in the Spring of 2022. The entire project will be completed and ready for occupancy approximately in the Fall of 2025.

H. Affordable Housing Agreement

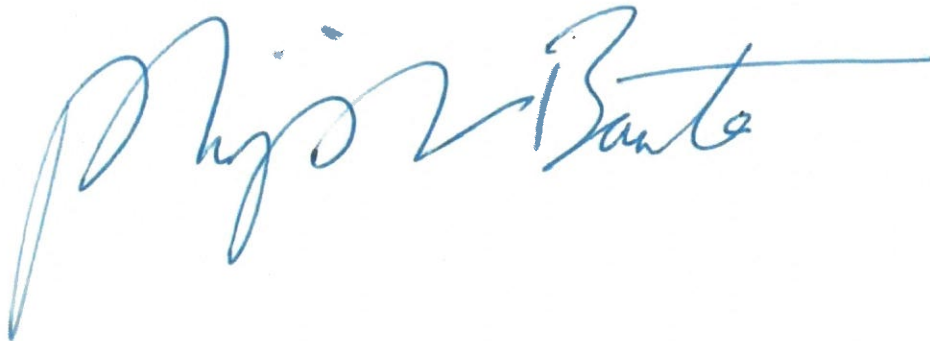
Boatworks LLC will enter in an Affordable Housing Agreement with the City of Alameda to govern the marketing, leasing, renting, sale, re-sale, and deed restrictions of the affordable homes (similar to the form included with this application). The Affordable Housing Agreement, with any other deed restrictions required by the City, will ensure that the homes remain affordable for the required term. This application and all its exhibits will become a part of the Affordable Housing Agreement.

I. Marketing Plan for Affordable Homes

The marketing plan for Affordable Homes will involve working closely with the City of Alameda and local groups such as Alameda Home Team, Renewed Hope, and the Alameda Housing Authority. We will partner with an experienced BMR marketing firm such as Hello Housing to prepare a more detailed Marketing Plan that will be provided prior to the signing of the Affordable Housing Agreement. Boatworks LLC will identify potential buyers for the affordable residences and select the applicants through a lottery system. Each of the selected applications will be screened to determine that they meet income and program-eligibility requirements. Per Alameda Muni Code 30-16.9a, the City will approve individual buyers for each residence.

Please acknowledge receipt of this application and confirm that it is complete.

Sincerely,
Phil Banta



Oakland Estuary

0.12 Acres Open Space
Outside of Property Lines

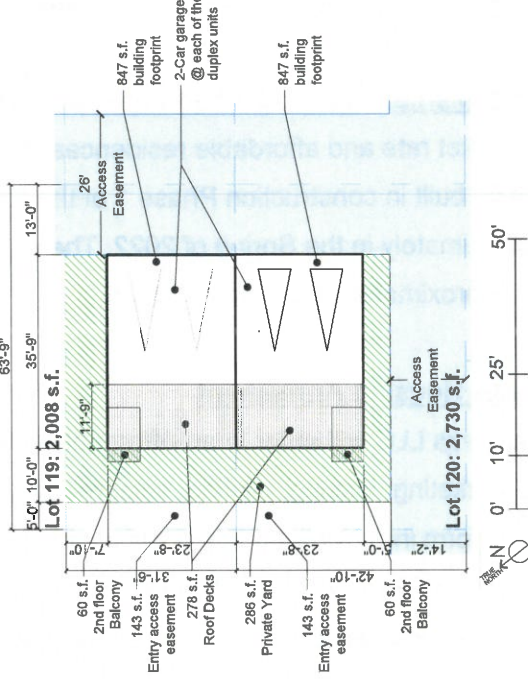
1.90 Acres Open Space
within Property Lines



140 Lots Base Site Plan (2,008 s.f. (min. lot size) up to 4,170 s.f.)
Open Space = Approx. 2 Acres (1.90 Acres)

The base site plan shows the site being divided into 140 units of 2,000 s.f. or greater along with common open space along the waterfront of approximately 2 Acres. This is the basis to which the Density Bonus of 30% has been applied (yielding a total of 182 units) as the result of the inclusion of 9% Very Low Income, 6% Moderate Income affordable housing units.

| Alameda R-2 Zoning | Required/ Unit | Proposed/ Unit |
|------------------------|--|--|
| Open Space | 120 s.f. min. for Ground level / 80 s.f. min. for non-ground level | Open Space at smallest Base Plan Unit (Lot 119) |
| Common Open Space | 300 s.f. min. | 286 s.f. |
| Total Open Space/ Unit | 600 s.f. min. | 338 s.f. |
| | | 1.90 ac/140 units = 591 s.f. (within property lines) |
| (a) Boatways | | 1,215 s.f. min. |
| | | 0.10 acres |



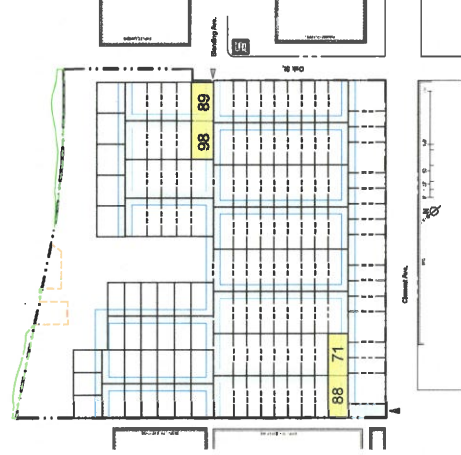
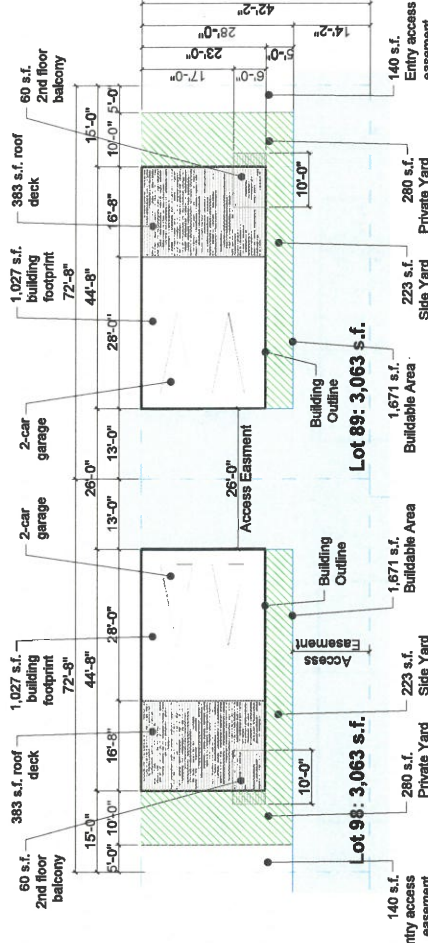
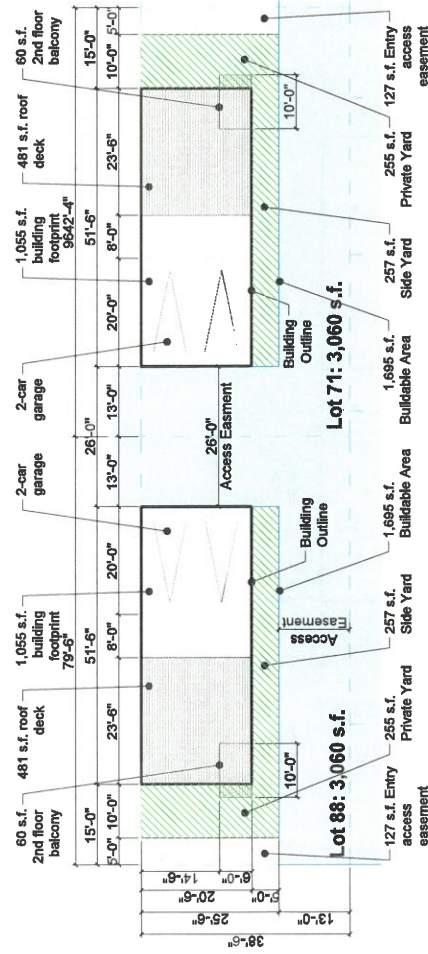
BASE SITE PLAN STUDY
Lot Divisions: 140 Residential Lots, Lots 119 & 120

Alameda Boatworks Green
MAR 3, 2020
BPS-1

TURNING RADIUS FOR PIERCE
FIRE TRUCK AS PROVIDED BY
THE ALAMEDA FIRE DEPT.

| Alameda R-2 Zoning | Required/ Unit | Proposed/ Unit |
|------------------------|--|---|
| | Open Space | Open Space at lots 88, 71 (mirror lots) |
| Open Space | 120 s.f. min. for / 60 s.f. min. for non-ground level / Ground level | 255 s.f. @ Private ground level yard (not including side yard) private decks @ 2nd floor and @ Roof |
| Common Open Space | 300 s.f. min. | 1.90 ac./140 units = 591 s.f. (within property lines) |
| Total Open Space/ Unit | 600 s.f. min. | 1,387 s.f. min. |

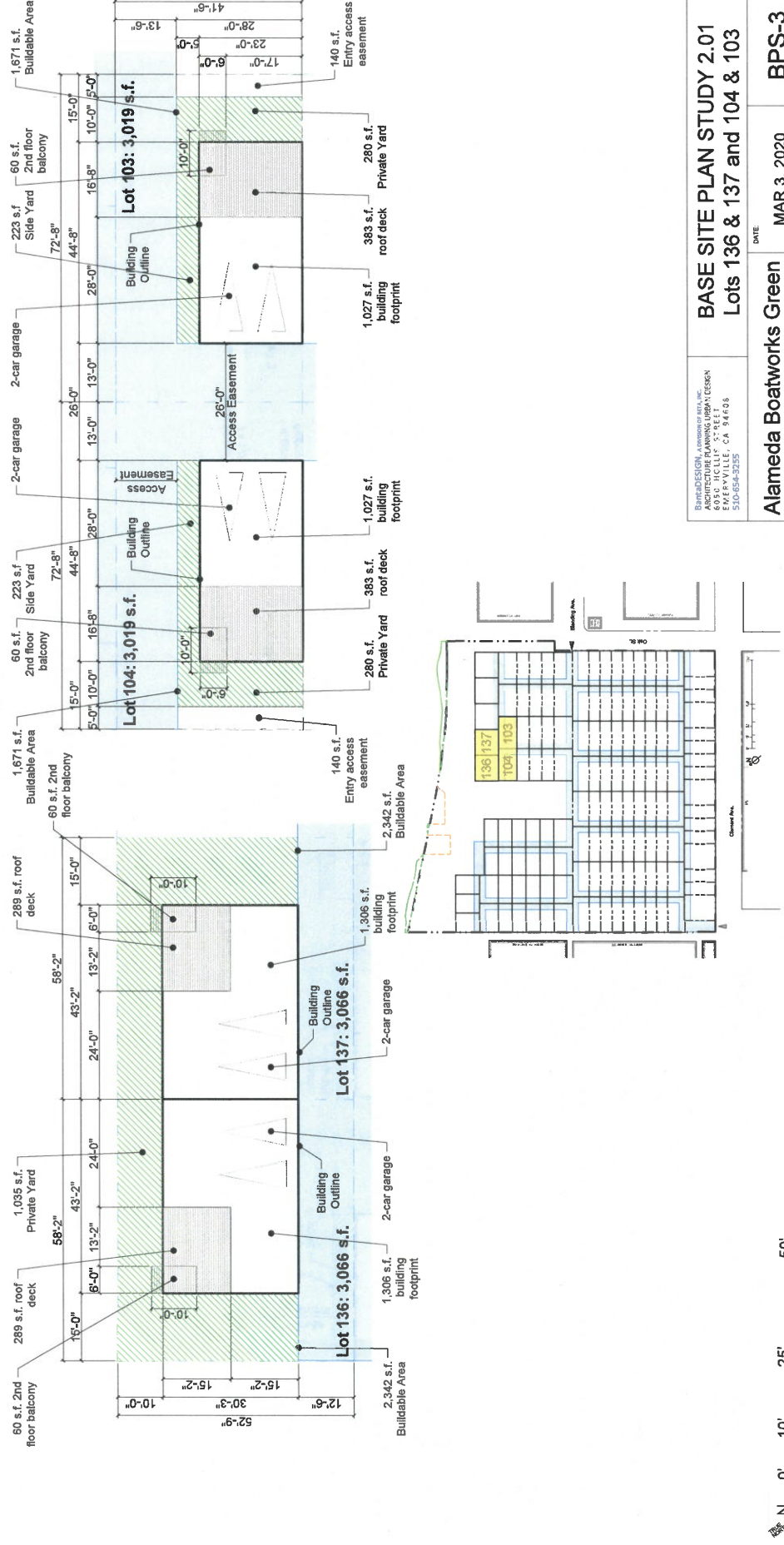
| Alameda R-2 Zoning | Required/ Unit | Proposed/ Unit |
|------------------------|--|---|
| | Open Space | Open Space at lots 98, 89 (mirror lots) |
| Open Space | 120 s.f. min. for / 60 s.f. min. for non-ground level / Ground level | 280 s.f. @ Private ground level yard (not including side yard) private decks @ 2nd floor and @ Roof |
| Common Open Space | 300 s.f. min. | 1.90 ac./140 units = 591 s.f. (within property lines) |
| Total Open Space/ Unit | 600 s.f. min. | 1,314 s.f. min. |



| | |
|--|--|
| BOARD OF SUPERVISORS, ALAMEDA COUNTY 9500 RICHMOND AVENUE, SUITE 200 OAKLAND, CALIFORNIA 94663 510-654-3255 | BASE SITE PLAN STUDY Lots 88 & 71 and 98 & 89 |
| Alameda Boatworks Green 2228 & 2225 CLEMENT AVENUE, ALAMEDA, CA | DATE: MAR 3, 2020 BPS-2 |

| Alameda R-2 Zoning | Required/ Unit | Proposed/ Unit |
|------------------------|---|---|
| Open Space | 120 s.f. min. for / 60 s.f. min. for non-ground level | Open Space at lots 136, 137 (mirror lots) |
| Common Open Space | 1,035 s.f. @ Private ground level yard | |
| | 349 s.f. private decks @ 2nd floor and @ Roof | |
| Total Open Space/ Unit | 300 s.f. min. | 1,975 s.f. min. |

| Alameda R-2 Zoning | Required/ Unit | Proposed/ Unit |
|------------------------|--|---|
| Open Space | 120 s.f. min. for / 60 s.f. min. for non-ground level | Open Space at lots 104, 103 (mirror lots) |
| Common Open Space | 280 s.f. @ Private ground level yard (not including side yard) | |
| | 443 s.f. private decks @ 2nd floor and @ Roof | |
| Total Open Space/ Unit | 300 s.f. min. | 1,314 s.f. min. |



BASE SITE PLAN STUDY 2.01
 Lots 136 & 137 and 104 & 103

BarraDesign, Architecture & Planning, Inc.
 ARCHITECTURE PLANNING URBAN DESIGN
 6050 AVENUE 111, STE 111
 SAN JOSE, CA 95128
 510-654-3235

Alameda Boatworks Green

DATE: MAR 3, 2020

BPS-3

2229 & 2235 CLEMENT AVENUE, ALAMEDA, CA

The 2020 Affordable Housing Plan Is the same as the 2011 Approved Affordable Housing Plan: 21 Affordable units located in the Multi-Family Building at Clement Avenue and Oak Street.
- 13 very low income units
- 8 moderate income units

The 2020 multi-family building consists of 30 elevator served dwelling units and provides a minimum of 1 parking space per unit. All the affordable units are accessible via laundry room and each floor features its own laundry room and trash chutes (for garbage and recycling) to a ground floor trash room. With the exception of a one Bdrm. unit (203) and a Studio unit (212) at the 2nd floor all units have private outdoor decks. All units have access to a 500 sf common space lounge near the 2nd floor elevator and stairs lobby as well as to a ground floor 830 sf lobby/waiting area with an ADA bathroom.

The project provides 21 affordable units (15% of the base site plan total of 140 units).

| | VL | Mod | Mkt | Total |
|-----------------|-----------|----------|----------|-----------|
| 4 Bdrm./ 2 Bath | | 1 | 1 | 1 |
| 3 Bdrm./ 2 Bath | | 1 | 5 | 6 |
| 2 Bdrm./ 2 Bath | | 2 | 1 | 3 |
| 2 Bdrm./ 1 Bath | 4 | 4 | 2 | 10 |
| 1 Bdrm./ 1 Bath | 8 | 1 | | 9 |
| Studio/ 1 Bath | 1 | | | 1 |
| Total | 13 | 8 | 9 | 30 |

| | Income Level Eligibility per State Requirements | | | | |
|-----------------------|---|--------------|-----------------|--|--|
| | Market Rate | T.B.D. | | | |
| Unit 501 | 3 Bdrm. / 2 Bath | 1,899 sq.ft. | T.B.D. | | |
| Unit 502 | 4 Bdrm. / 2 Bath | 1,928 sq.ft. | T.B.D. | | |
| Unit 503 | 3 Bdrm. / 2 Bath | 1,754 sq.ft. | T.B.D. | | |
| Unit 504 | 3 Bdrm. / 2 Bath | 1,123 sq.ft. | T.B.D. | | |
| Unit 401 | 2 Bdrm. / 1 Bath | 838 sq.ft. | Moderate Income | | |
| Unit 402 | 3 Bdrm. / 2 Bath | 1,403 sq.ft. | Market Rate | | |
| Unit 403 | 2 Bdrm. / 1 Bath | 1,148 sq.ft. | Market Rate | | |
| Unit 404 | 2 Bdrm. / 1 Bath | 1,148 sq.ft. | Market Rate | | |
| Unit 405 | 3 Bdrm. / 2 Bath | 1,591 sq.ft. | Market Rate | | |
| Unit 406 | 1 Bdrm. / 1 Bath | 675 sq.ft. | Moderate Income | | |
| Unit 407 | 2 Bdrm. / 2 Bath | 1,023 sq.ft. | Moderate Income | | |
| Unit 301 | 2 Bdrm. / 1 Bath | 838 sq.ft. | Moderate Income | | |
| Unit 302 | 3 Bdrm. / 2 Bath | 1,403 sq.ft. | Moderate Income | | |
| Unit 303 | 2 Bdrm. / 1 Bath | 1,148 sq.ft. | Moderate Income | | |
| Unit 304 | 2 Bdrm. / 1 Bath | 1,148 sq.ft. | Moderate Income | | |
| Unit 305 | 3 Bdrm. / 2 Bath | 1,591 sq.ft. | Market Rate | | |
| Unit 306 | 1 Bdrm. / 1 Bath | 675 sq.ft. | Very Low Income | | |
| Unit 307 | 2 Bdrm. / 2 Bath | 1,023 sq.ft. | Moderate Income | | |
| Unit 201 | 2 Bdrm. / 1 Bath | 838 sq.ft. | Very Low Income | | |
| Unit 202 | 2 Bdrm. / 1 Bath | 802 sq.ft. | Very Low Income | | |
| Unit 203 | 1 Bdrm. / 1 Bath | 587 sq.ft. | Very Low Income | | |
| Unit 204 | 1 Bdrm. / 1 Bath | 567 sq.ft. | Very Low Income | | |
| Unit 205 | 1 Bdrm. / 1 Bath | 567 sq.ft. | Very Low Income | | |
| Unit 206 | 1 Bdrm. / 1 Bath | 567 sq.ft. | Very Low Income | | |
| Unit 207 | 1 Bdrm. / 1 Bath | 567 sq.ft. | Very Low Income | | |
| Unit 208 | 2 Bdrm. / 1 Bath | 794 sq.ft. | Very Low Income | | |
| Unit 209 | 2 Bdrm. / 1 Bath | 759 sq.ft. | Very Low Income | | |
| Unit 210 | 1 Bdrm. / 1 Bath | 659 sq.ft. | Very Low Income | | |
| Unit 211 | 1 Bdrm. / 1 Bath | 590 sq.ft. | Very Low Income | | |
| Unit 212 | Studio / 1 Bath | 420 sq.ft. | Very Low Income | | |
| Total 30 Units | | | | | |

| Parking Type | Dimensions | Number of Spaces |
|----------------------------------|-------------|----------------------------|
| Standard Spaces (S) | 8'-6" x 18' | 12 spaces |
| Compact Spaces (C) | 7'-6" x 15' | 12 spaces |
| ADA Spaces | 9' x 18' | 2 spaces |
| Adjacent Spaces outside Building | 8'-6" x 18' | 2 Tandem spaces = 4 spaces |
| | 8'-6" x 18' | 2 spaces |
| | 8'-6" x 16' | 1 space |
| Total Parking Spaces | | 33 total spaces |



Ground Floor
Multi-Family Building Plans

9 Units (Market Rate)
8 Units (Moderate Income)
13 Units (Very Low Income)
30 Units total

L = Laundry Room
T = Trash Chute
E = Elevator



Alameda Boatworks Green

2229 & 2235 CLEMENT AVENUE, ALAMEDA, CA

Affordable Housing Plan

DATE: **MAR 10, 2020**

MFB-1

EXHIBIT C

DESIGN PLAN

ALAMEDA BOATWORKS GREEN



DEVELOPMENT PLAN SUBMITTAL SET EXHIBITS:

- COVER SHEET
 - VICINITY MAP- AERIAL PHOTO
 - PROPOSED SITE PLAN
 - AREA TABULATION
 - OPEN SPACE PLAN
 - PHASING PLAN
 - SITE SECTIONS: A & B
 - SITE SECTIONS: C & D
 - SITE SECTION: E
 - SITE SECTIONS: F & G
 - SITE SECTIONS: H & J
 - SITE SECTION: K
 - PROTOTYPICAL UNIT - ELEVATIONS
 - PROTOTYPICAL UNIT - PERSPECTIVE VIEWS
 - AFFORDABLE HOUSING PLAN
- OPEN SPACE DESIGN REVIEW EXHIBITS
- LANDSCAPE PLAN
 - OPEN SPACE BIO-RETENTION CONCEPT
 - LANDSCAPE SITE FURNITURE EXAMPLES
 - PROPOSED LANDSCAPE PLANT PALETTE
 - ILLUSTRATIVE SITE SECTIONS

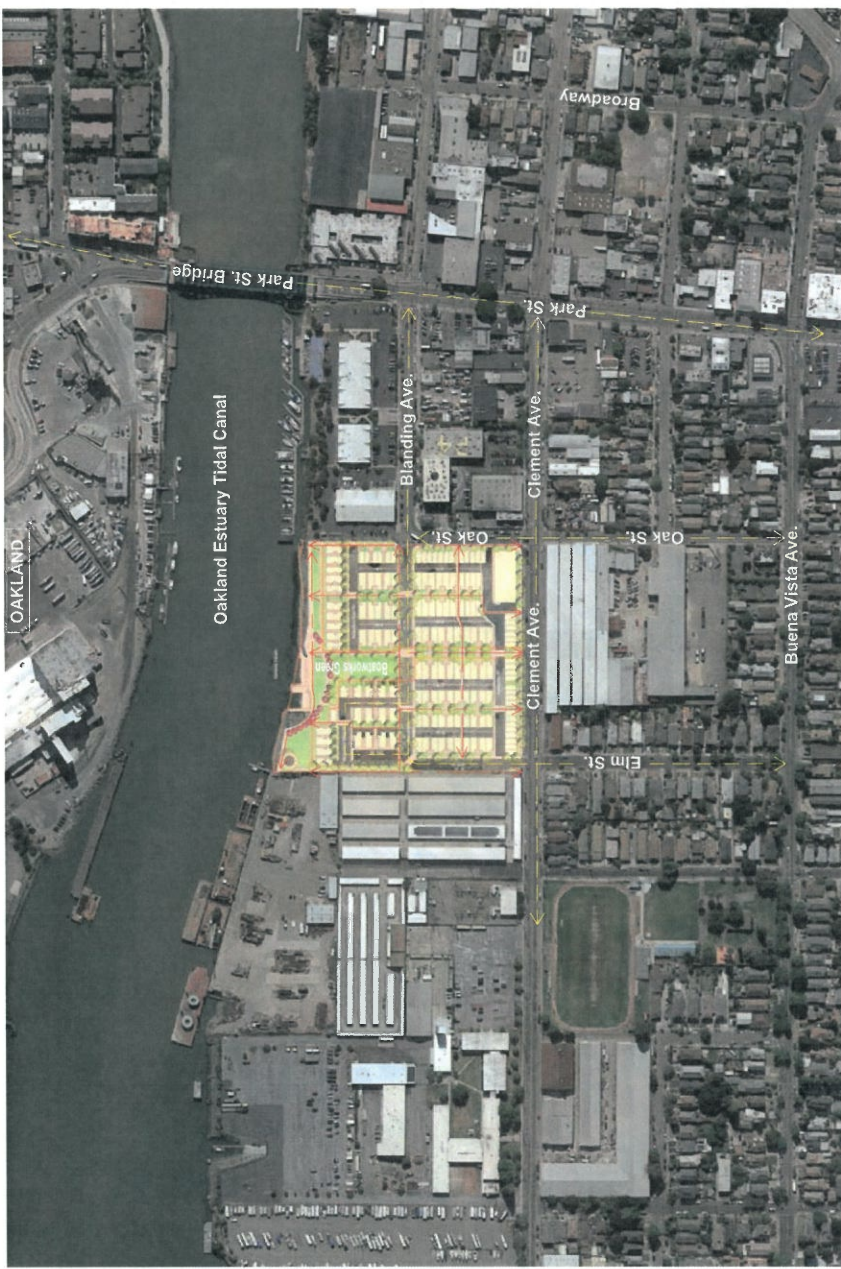
| | | | |
|---|---|----------------------|------|
| BantaDesign, a division of Banta, Inc. ARCHITECTURAL & LANDSCAPE ARCHITECTURE 1000 UNIVERSITY AVENUE, SUITE 100 EMERYVILLE, CA 94608 510-654-3255 | Development Plan Submittal Cover Sheet | DATE MAR 10, 2020 | DP-0 |
| Alameda Boatworks Green 2228 & 2235 CLEMENT AVENUE, ALAMEDA, CA | DATE MAR 10, 2020 | | |

**A New Planned Residential Community
Connecting the City with its Waterfront:**

The "Boatworks Green" Community internal vehicular drives, bikeways and pedestrian paseos are organized to match and extend existing routes in the City. Main entrances align exactly opposite Blanding Ave. and Elm St. providing direct line extensions of roadways, sidewalks and bike paths into the Boatworks site.

Landscaped pedestrian pathways continue beyond Oak St. and Elm Drive to lead pedestrians up to the Publicly Accessible Open Space at the waterfront.

A major paseo for pedestrians and bicycles-only lead from Clement Avenue up through the site, concluding at the Estuary Open Space. Our site plan is an exercise in enhancing the urban fabric of Alameda at a critical connection point to its northern waterfront.



"Boatworks Green" Site along Alameda's Northern Waterfront

- Pedestrian/ bicycle pathways
- Vehicular roads accessible to the public as extensions of the existing street grid



| | | |
|--|---|---|
| <p> <small> BAYVIEWDESIGN, A DIVISION OF BETA, INC. 14899 15TH AVE., SUITE 100 SAN FRANCISCO, CA 94134 415.455.3255 </small> </p> | VICINITY MAP | |
| <p> Alameda Boatworks Green <small>2228 & 2235 CLEMENT AVENUE, ALAMEDA, CA</small> </p> | <p> <small>DATE:</small> MAR 10, 2020 </p> | <p> DP-1 </p> |



The 2020 Site Plan for the Alameda Boatworks Green project includes 152 Townhomes and Single Family Homes along with a 30-unit Multi-Family housing building across a 9.4 acre waterfront site at the Oakland Estuary that features a large publicly accessible open space at the shoreline.

The 192-unit project extends the existing neighborhood street grid through the site, interfaced with a network of dedicated pedestrian / bicycle path paseos leading up to the waterfront open space. The project is intended to contribute a diversity of housing options within a richly landscaped pedestrian setting that connects the island community with its waterway resource at the Oakland Estuary.

| PARKING | |
|--|-----|
| 2 car garages for 104 Townhomes & SFD's | 208 |
| 1 car garages for 48 Townhomes | 48 |
| 1 space/unit for Multi-Family | 30 |
| TOTAL GARAGE PARKING SPACES | |
| | 286 |
| Parking at Private Drives Including Guest & BCDC Parking | |
| | 43 |
| TOTAL PARKING PROVIDED ON SITE | |
| | 329 |

| UNITS | | | | | | | | | | | |
|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------|--|
| A2 | B1 | B2 | B3 | B3R | B5 | D1 | E | E2 | F1 | | |
| 14 units | 8 units | 15 units | 28 units | 3 units | 8 units | 30 units | 39 units | 2 units | 5 units | Townhomes = 152 | |
| 3 or 4 Bedrooms | 3 Bedrooms | 3 Bedrooms | 3 Bedrooms | 3 Bedrooms | 2 or 3 Bedrooms | 3 or 4 Bedrooms | 2 or 3 Bedrooms | 2 Bedrooms | 3 or 4 Bedrooms | | |
| Private Roadway Space 514sf | Private Roadway Space 514sf | Private Roadway Space 438sf | Private Roadway Space 438sf | Private Roadway Space 438sf | Private Roadway Space 352sf | Private Roadway Space 352sf | Private Roadway Space 156sf | Private Roadway Space 156sf | Private Roadway Space 156sf | | |
| 2,862.0 s.f. | 2,876.0 s.f. | 2,659.0 s.f. | 2,659.0 s.f. | 2,546 s.f. | 2,108 s.f. | 2,040 s.f. | 1,672.0 s.f. | 1,098.0 s.f. | 2,718 s.f. | | |
| 14 units | 8 units | 15 units | 28 units | 3 units | 8 units | 30 units | 39 units | 2 units | 5 units | | |
| 3 or 4 Bedrooms | 3 Bedrooms | 3 Bedrooms | 3 Bedrooms | 3 Bedrooms | 2 or 3 Bedrooms | 3 or 4 Bedrooms | 2 or 3 Bedrooms | 2 Bedrooms | 3 or 4 Bedrooms | | |
| Private Roadway Space 514sf | Private Roadway Space 514sf | Private Roadway Space 438sf | Private Roadway Space 438sf | Private Roadway Space 438sf | Private Roadway Space 352sf | Private Roadway Space 352sf | Private Roadway Space 156sf | Private Roadway Space 156sf | Private Roadway Space 156sf | | |
| 2,862.0 s.f. | 2,876.0 s.f. | 2,659.0 s.f. | 2,659.0 s.f. | 2,546 s.f. | 2,108 s.f. | 2,040 s.f. | 1,672.0 s.f. | 1,098.0 s.f. | 2,718 s.f. | | |

| | |
|-----------------------|----------|
| Multi-Family Building | 30 Units |
| TOTAL DWELLING UNITS | |
| 182 | |

BARBARA DESIGN, A LIMITED LIABILITY COMPANY
ARCHITECTURE PLANNING & URBAN DESIGN
1000 RIVERVIEW DRIVE, SUITE 100
EMERYVILLE, CA 94608
510-654-9255

PROPOSED SITE PLAN

Alameda Boatworks Green
2228 & 2235 CLEMENT AVENUE, ALAMEDA, CA

DATE: **MAR 10, 2020**
DP-02

a. Accessible Open Space
 All 30 multi-family units accessible by ADA elevator

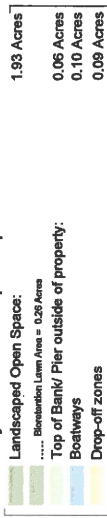
The 2020 Boatworks Green Open Space Plan features approximately 2 acres of publicly accessible open space along the Oakland Estuary that includes an estuary dock for access to the water, and waterfront landscaped picnic and playground areas. Main access paths that connect the adjacent Alameda neighborhood to the waterfront open space are found as follows:

- 1) Along the western edge of the site from Clement Ave. along the Elm Drive Walk to parking available to the public and up to the shoreline.
- 2) Up the center of the site at the main pedestrian/bicycle path Paseo that extends from Clement Ave. through to the Boatworks Green and up to the shoreline open space.
- 3) Along the eastern edge of the site at Oak St. via an improved landscaped sidewalk that widens at Blending Ave. to become the Oak st. Walk leading to the waterfront open space.

Private open space for Single Family homes includes private ground level yards and for both Single Family and Attached Townhomes includes 2nd level living room decks and private rooftop decks. The available private open space and common space areas exceed the individual unit and aggregate requirements from Alameda Planning regulations.

Overall Site Area: 9.48 Acres

a. Publicly Accessible Open Space : 2.18 Acres



b. Private Open Space :

c. Private Rooftop decks : 1.08 Acres

Total Open Space (a+b+c)

OPEN SPACE LEGEND

- Publicly Accessible Open Space
- ① Shoreline Open Space
② Main Paseo
③ Elm Dr. Walk
④ Oak St. Walk

Total Street Parking Spaces = 43

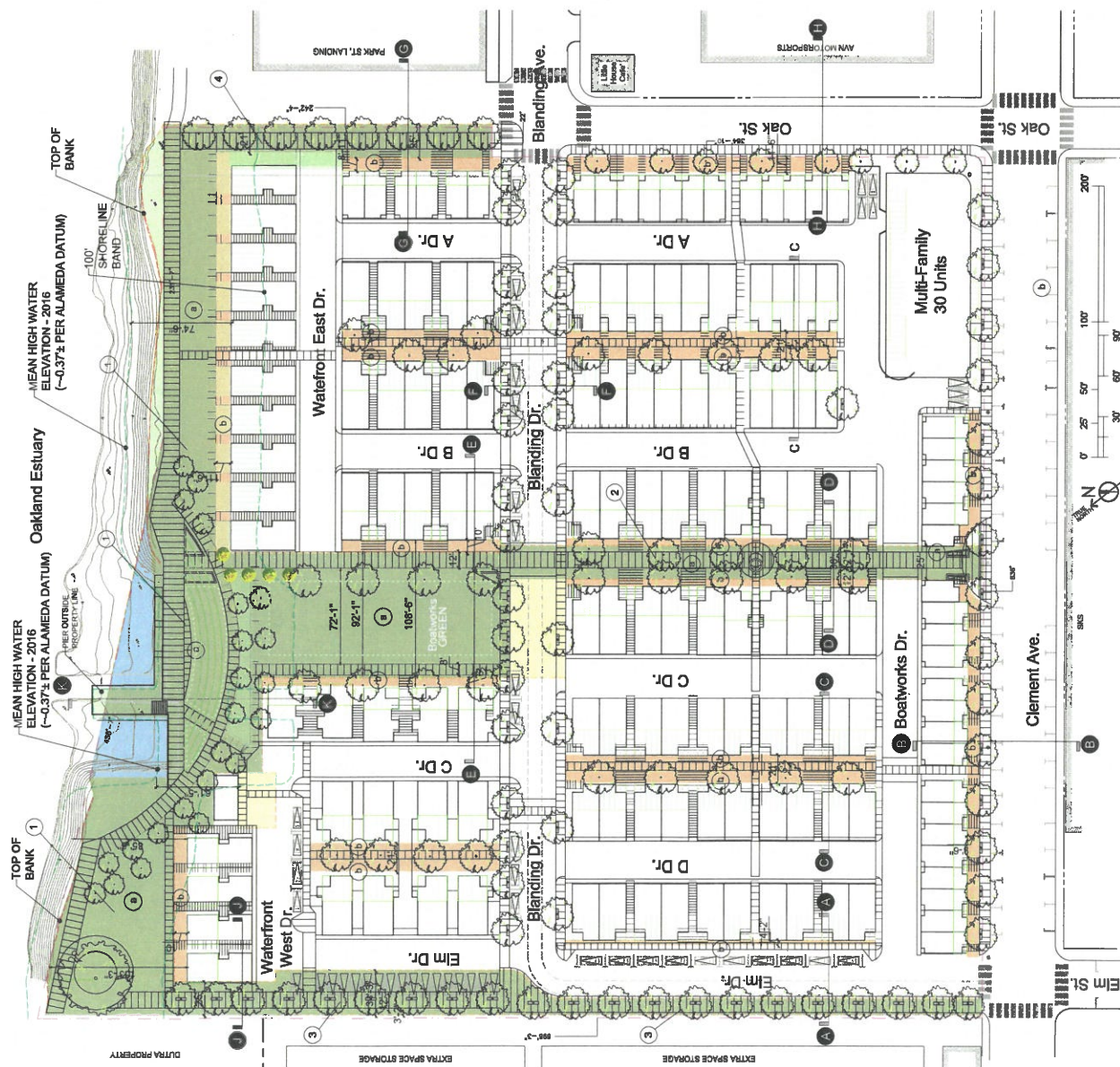
OPEN SPACE PLAN

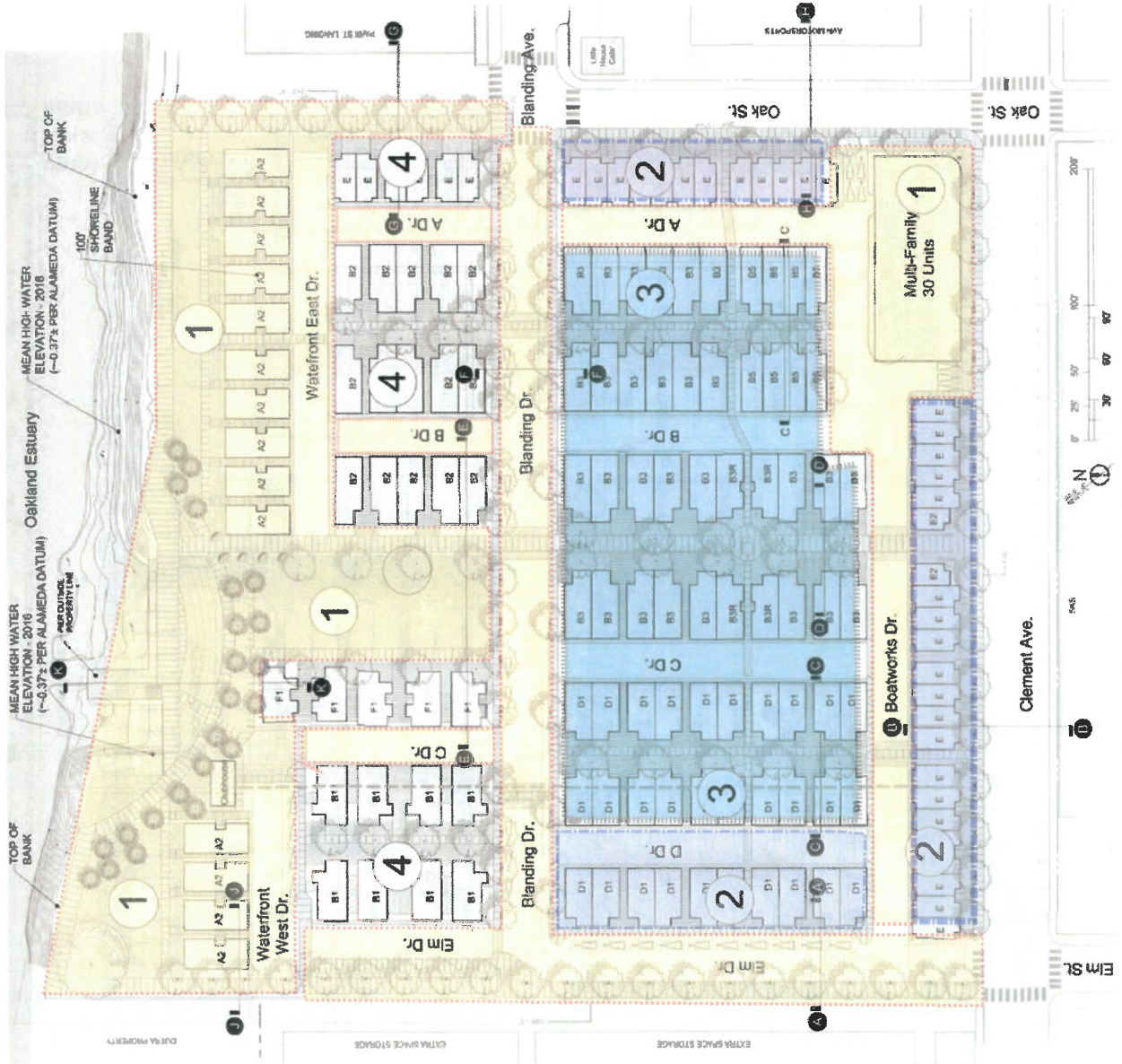
BantaDesign, A DIVISION OF BETA, INC.
ARCHITECTURE PLANNING URBAN DESIGN
2050 HOLLY STREET
EMERYVILLE, CA 94608
1-800-664-3966

Alameda Boatworks Green

MAR 10, 2020

DP-4





2020 Site Plan: Four Phases for Development of "Boatworks Green" project.

Phase 1:

This Phase of the Development Plan calls for the construction of the 30 Unit Multi-Family building that contains all the inclusionary affordable housing units, and the construction of a majority of the publicly accessible open spaces including the landscaped areas along the waterfront and through the project.

Phases 2:

This Phase includes perimeter dwelling units along Oak St & Clement south of Blanding Drive.

Phase 3:

Dwelling Units between A & D Drives south of Blanding Drive

Phase 4:

Dwelling Units between A & D Drives north of Blanding Drive

Table below estimates commencement and completion dates for each Phase and are subject to change.

Dates are contingent upon the City of Alameda awarding the project a Building Permit before the end of 2021.

| Phase | Start Date | Completion Date |
|---------|------------|-----------------|
| Phase 1 | 1/01/2022 | 12/31/2022 |
| Phase 2 | 1/01/2023 | 12/31/2023 |
| Phase 3 | 1/01/2023 | 12/31/2024 |
| Phase 4 | 1/01/2024 | 12/31/2025 |

BRUNDESIGN, A LANDSCAPE ARCHITECTURE FIRM
ARCHITECTURAL & LANDSCAPE ARCHITECTURE
1011 14TH AVE. S.W. #100
SEASIDE, CA 94134
510.454.3255

PHASING PLAN for DEVELOPMENT

Alameda Boatworks Green

2225 & 2225 CLEMENT AVENUE, ALAMEDA, CA

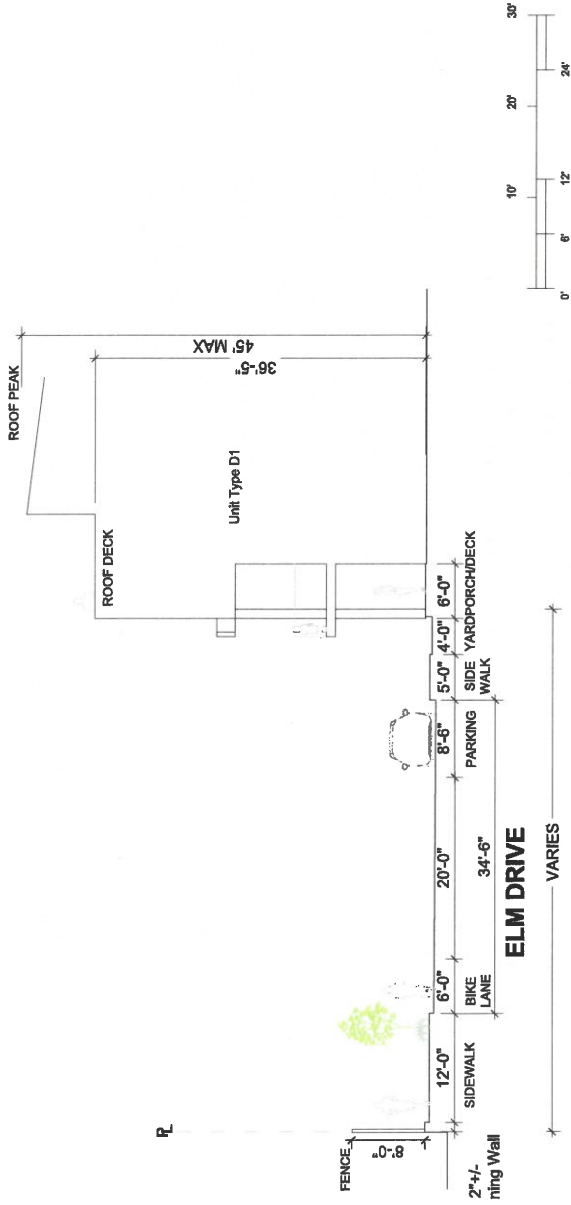
MAR 10, 2020

DP-5

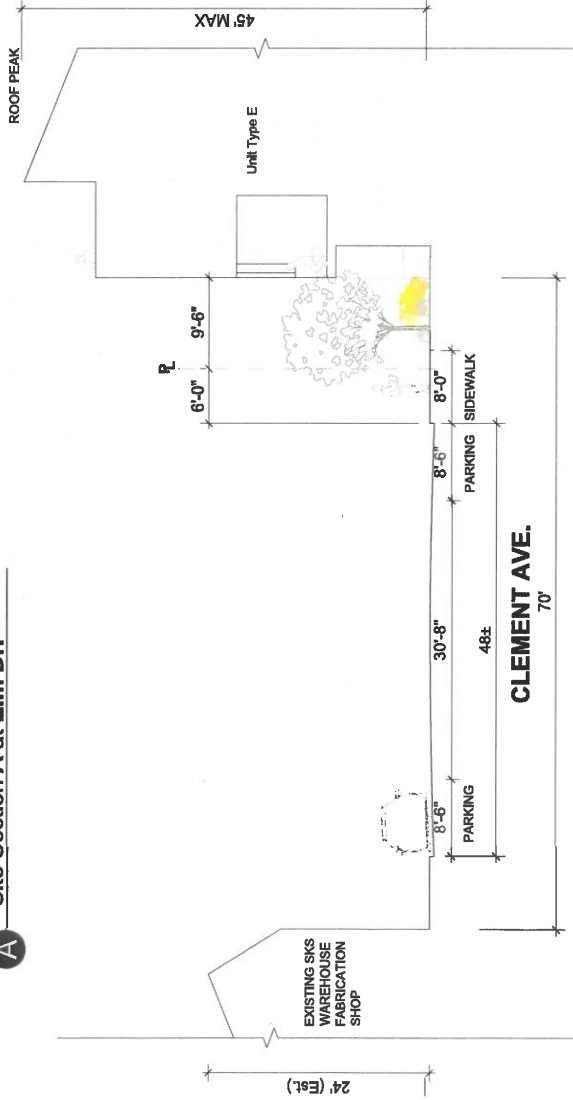
Landscaping and setback site sections at Elm Drive and Clement Ave.

Elm Drive extends into the "Boatworks Green" site along its western property line providing a landscaped buffer between the type D1 units and the self-storage facility property immediately adjacent. Upper level decks facing west enjoy views of Jack London Square, the Port of Oakland and Mt. Tam in the distance.

Units along Clement Ave. are positioned to allow for an 8' wide tree-lined sidewalk and landscaped front entrance yards as a buffer between the streetside units and the main city street. Private decks at the living room and masterbedroom levels enliven the street frontage elevations.



Site Section A at Elm Dr.



Site Section B at Clement Ave.

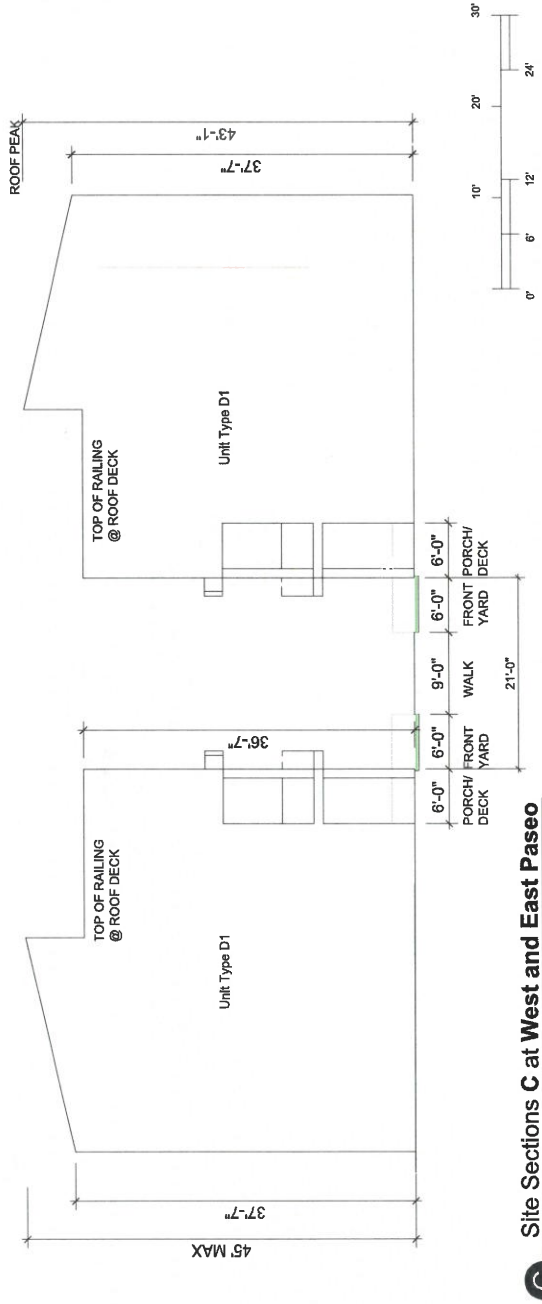
Bentley Design, a division of BENT, INC.
ARCHITECTURAL PLANNING & DESIGN
405 WILSON AVENUE, SUITE 200
MILWAUKEE, WI 53212
510-654-3235

SITE SECTIONS: A & B

Alameda Boatworks Green
2228 & 2235 CLEMENT AVENUE, ALAMEDA, CA

DATE
MAR 10, 2020

DP-6



Site Section C at West and East Paseo



Site Section D at Main Paseo

Common & Private Open Space along the Paseos.

2020 site plan includes large roof top decks, for most of the unit types: (A-2, B-1, B-2, B-3, D-1, F-1). This elevated plane of recreational space provides not only a secure and private outdoor area with great views for individual units but also continues the step up from front yard, to living room deck, to rooftop deck exterior living space that contributes a multi-level private/public latticework to the paseos and roadway frontages.

This multiplicity of indoor/outdoor spaces builds up the frequency of social interaction possibilities that helps build a strong community. From many of these rooftop decks Mt. Tam and the western sky will be visible along with the East Bay Hills and the breadth and width of Alameda itself.



BentDesign, a division of BDT, Inc.
1000 California Street, Suite 1000
San Francisco, CA 94109
510-654-3255

SITE SECTIONS: C & D

Alameda Boatworks Green
2225 & 2235 CLEMENT AVENUE, ALAMEDA, CA

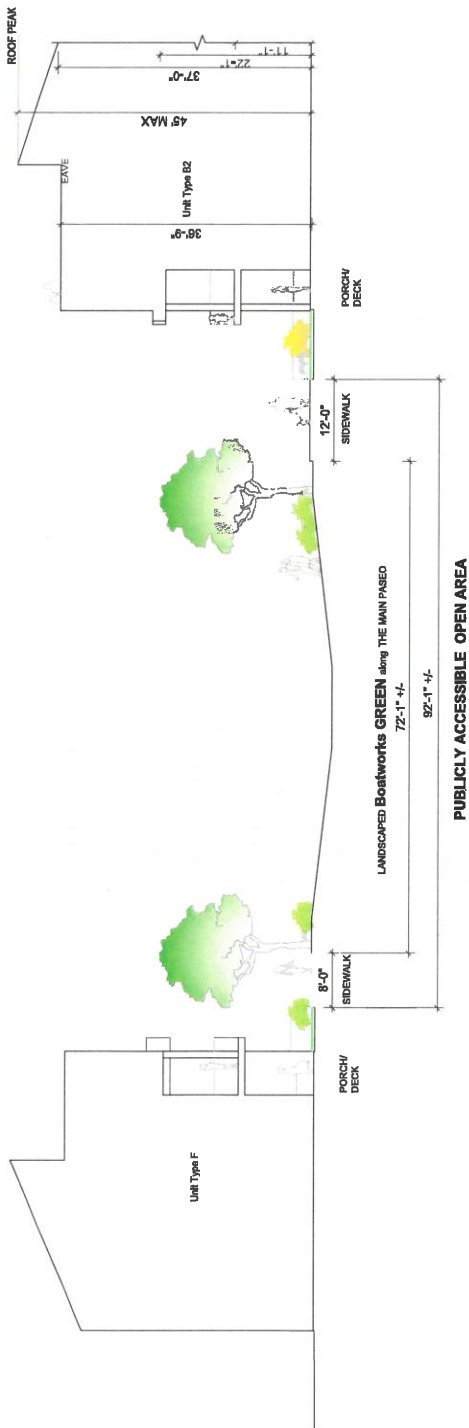
DATE: MAR 10, 2020

DP-7

Common & Private Open Space

The expansive Boatworks Green landscaped commons area at the intersection of the East-West Blanding Dr. and North-South Main Paseo is located at the heart of the Boatworks project and at the nexus of the main vehicular and pedestrian traffic flows.

This publically accessible open space doubles as a bio-swale and storm water management area



E Site Section E at Boatworks GREEN



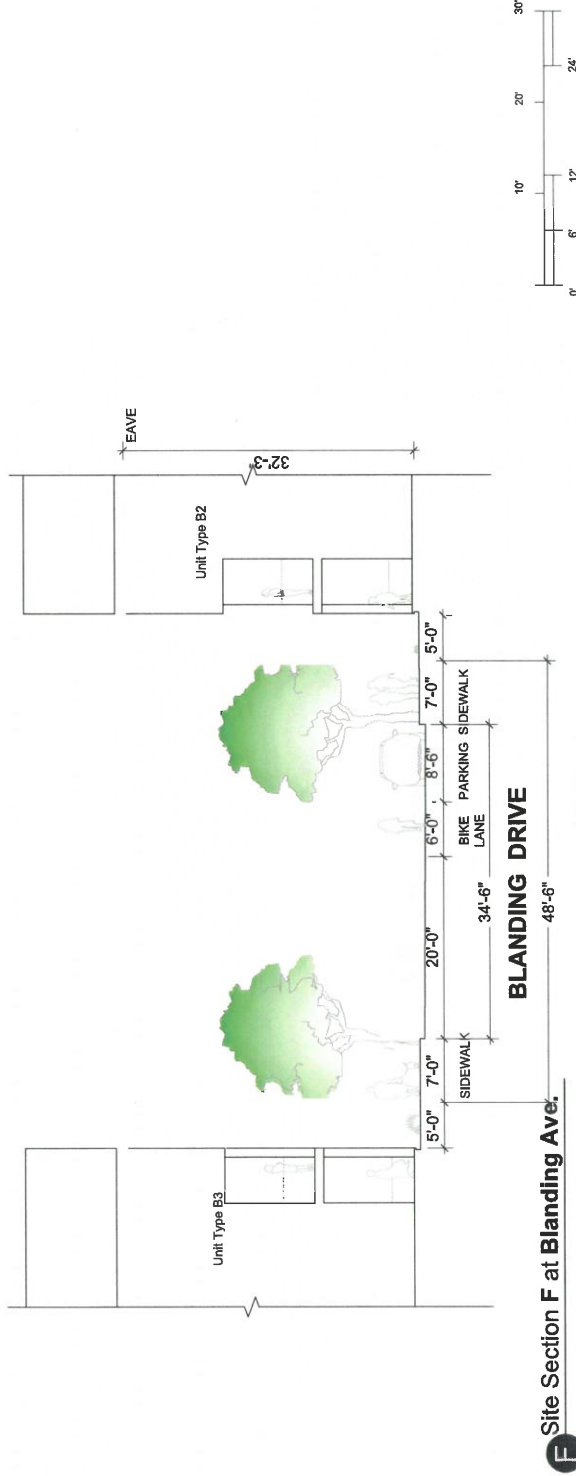
| | |
|---|----------------------------------|
| <p>Barbale Design, a division of BDC, Inc. ARCHITECTURAL & INTERIOR DESIGN 1100 CLAY STREET, SUITE 100 ALAMEDA, CA 94601 510-654-3255</p> | <p>SITE SECTION: E</p> |
| <p>Alameda Boatworks Green 2229 & 2235 CLEMENT AVENUE, ALAMEDA, CA</p> | <p>DATE: MAR 10, 2020</p> |
| <p>DP-8</p> | |

Extension of Blanding Drive through center of site and Oak St. Pathway at Eastern boundary of site.

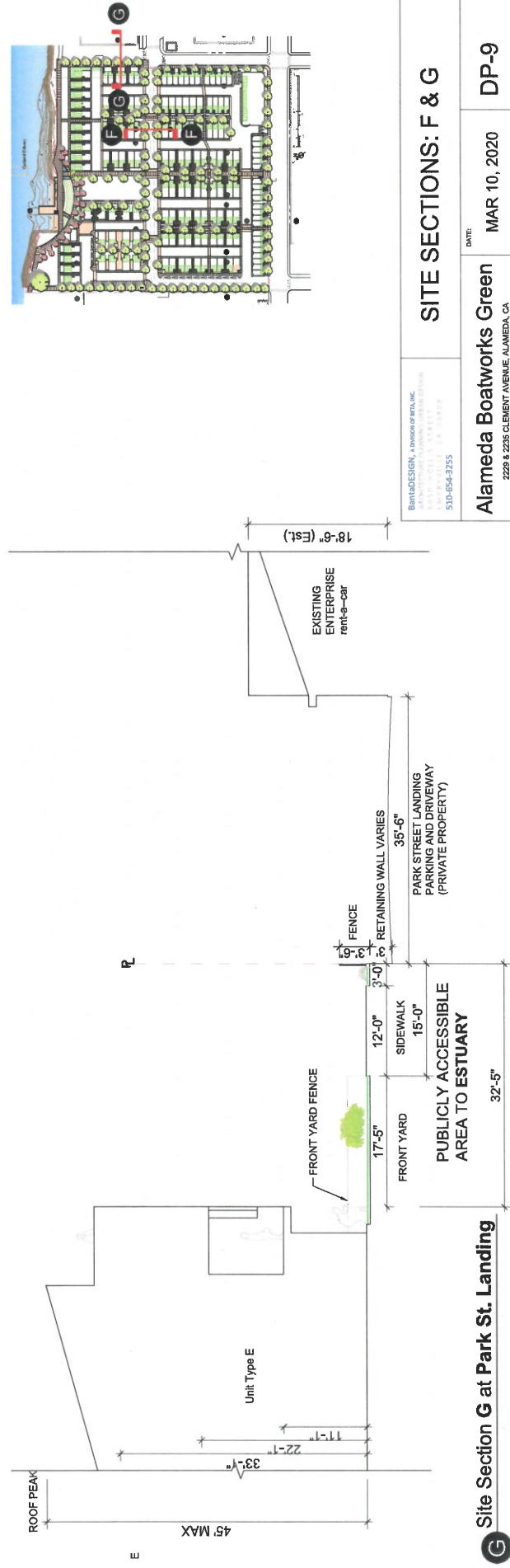
The main East-West arterial through the site is the Blanding Drive extension of Blanding Ave.

Blanding Drive (section FF) features parallel street parking along its northern side along with a dedicated Bike Lane and tree-lined landscaped sidewalks at either side of the road. The well defined spatial corridor breaks at the southern edge of the Community Green to provide an invitation to passers-by to explore the Waterfront Open Space beyond.

Blanding Drive leads to the public parking at Elm Drive which connects the two main vehicular entrances to the site off Oak Street and Clement Ave.

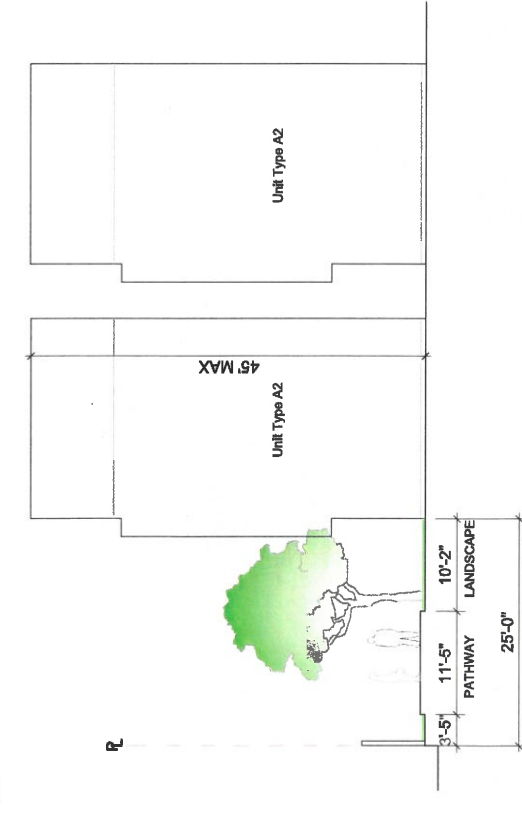
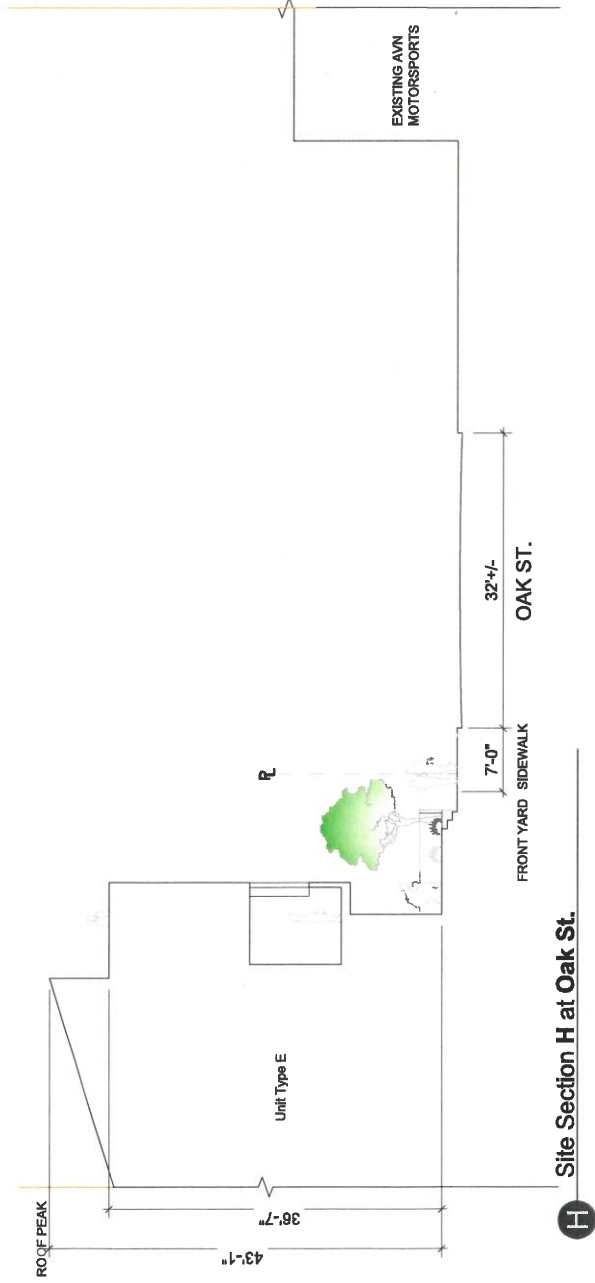


Site Section F at Blanding Ave.

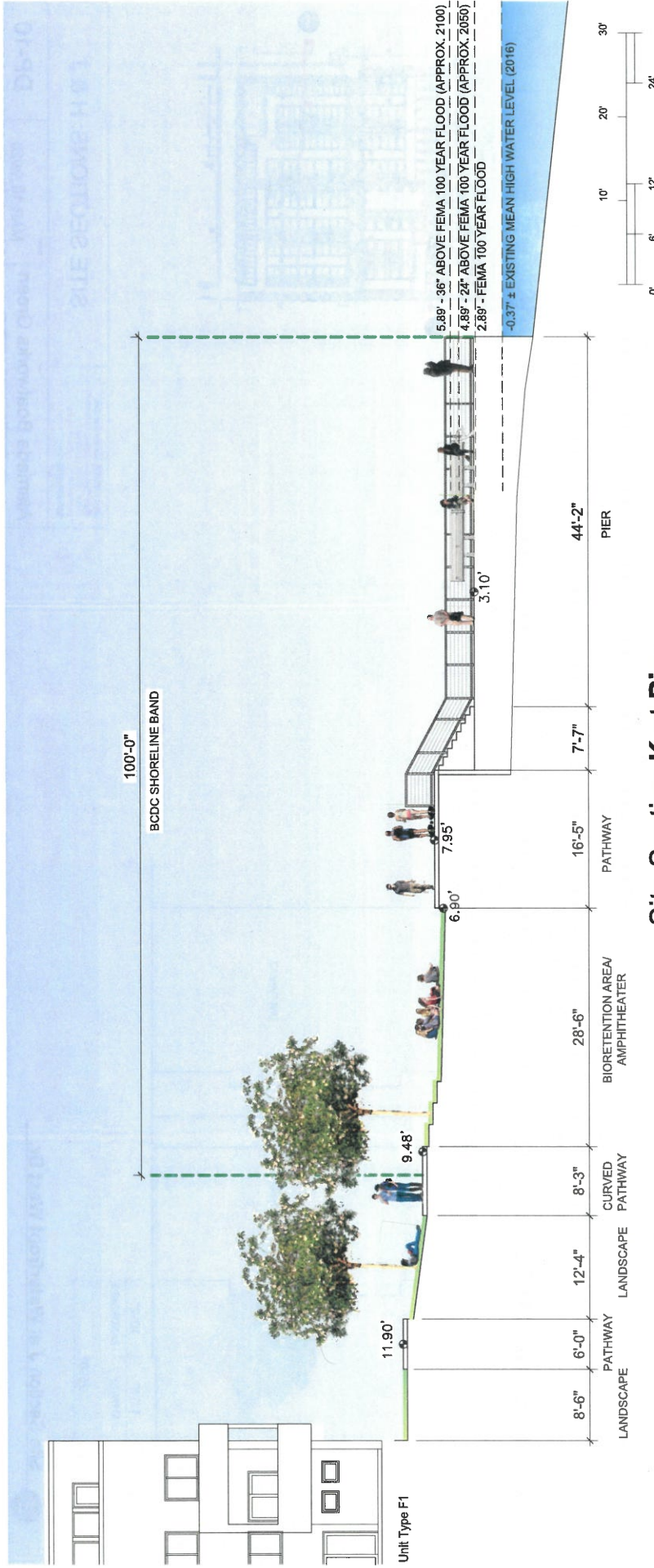


Site Section G at Park St. Landing

| | |
|--|--|
| BIRN DESIGN, A DIVISION OF BIRN, INC. 2229 & 2235 CLEMENT AVENUE, ALAMEDA, CA 510-654-3255 | SITE SECTIONS: F & G DATE: MAR 10, 2020 DP-9 |
|--|--|

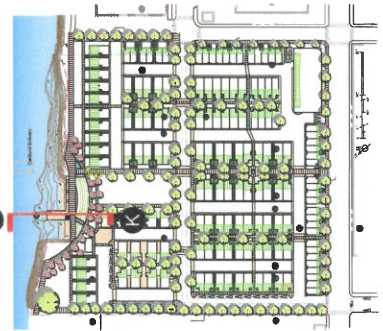


| | | |
|---|----------------------|-------|
| SITE SECTIONS: H & J 2228 & 2235 CLEMENT AVENUE, ALAMEDA, CA 510-654-3255 | DATE MAR 10, 2020 | DP-10 |
|---|----------------------|-------|

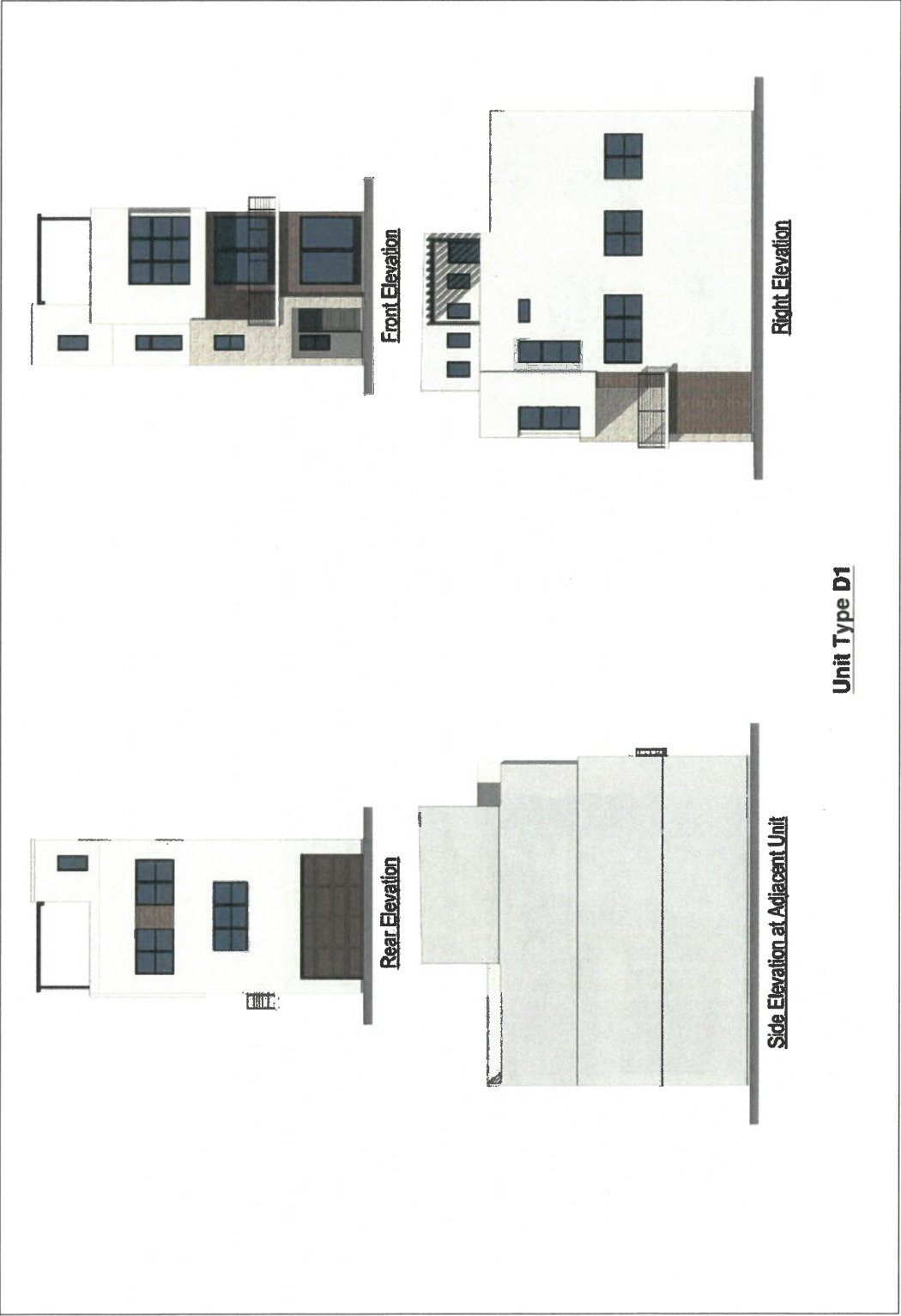


Site Section K at Pier

K



| | | |
|---|-----------------------|-------|
| BIRDADESIGN, A DIVISION OF BIRDA, INC. 4825 MARSH STREET 510-654-3255 | SITE SECTION: K | |
| Alameda Boatworks Green 2229 & 2235 CLEMENT AVENUE, ALAMEDA, CA | DATE: MAR 10, 2020 | DP-11 |



Unit Type D1

| | | |
|--|--|------|
| <div>BRUNDA DESIGN, A DIVISION OF BETA, INC. 1000 UNIVERSITY AVENUE, SUITE 200 EMERYVILLE, CA 94608 510-654-3255</div> | Prototypical Unit D1- Exterior Elevations | |
| | DATE | DATE |
| Alameda Boatworks Green | MAR 10, 2020 | DR-1 |
| 2229 & 2235 CLEMENT AVENUE, ALAMEDA, CA | | |



Perspective 1



Perspective 2

Unit Type D1

| | | |
|---|--|-------------|
| BIRN DESIGN, a division of BDA, INC. 2228 & 2235 CLEMENT AVENUE, ALAMEDA, CA 94606 510-654-3255 | Prototypical Unit D1- Perspective Views | |
| Alameda Boatworks Green 2228 & 2235 CLEMENT AVENUE, ALAMEDA, CA | DATE: MAR 10, 2020 | DR-2 |

The 2020 Affordable Housing Plan is the same as the 2011 Approved Affordable Housing Plan: 21 affordable units located in the Multi-Family Building at Clement Avenue and Oak Street.
- 13 very low income units
- 8 moderate income units

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The project provides 21 affordable units (15% of the base site plan total of 140 units).

| | VL | Mod | Mkt | Total |
|-----------------|-----------|----------|----------|-----------|
| 4 Bdrm./ 2 Bath | | | 1 | 1 |
| 3 Bdrm./ 2 Bath | | 1 | 5 | 6 |
| 2 Bdrm./ 2 Bath | | 2 | 1 | 3 |
| 2 Bdrm./ 1 Bath | 4 | 4 | 2 | 10 |
| 1 Bdrm./ 1 Bath | 8 | 1 | | 9 |
| Studio/ 1 Bath | 1 | | | 1 |
| Total | 13 | 8 | 9 | 30 |

| | Income Level Eligibility per State Requirements | | | | |
|----------------|---|------------------|--------------|-----------------|--|
| | Market Rate | T.B.D. | | | |
| Fifth Floor | Unit 501 | 3 Bdrm. / 2 Bath | 1,899 sq.ft. | T.B.D. | |
| | Unit 502 | 4 Bdrm. / 2 Bath | 1,928 sq.ft. | T.B.D. | |
| | Unit 503 | 2 Bdrm. / 2 Bath | 1,754 sq.ft. | T.B.D. | |
| | Unit 504 | 2 Bdrm. / 2 Bath | 1,123 sq.ft. | T.B.D. | |
| | Unit 401 | 2 Bdrm. / 1 Bath | 838 sq.ft. | Moderate Income | |
| Fourth Floor | Unit 402 | 3 Bdrm. / 2 Bath | 1,403 sq.ft. | Market Rate | |
| | Unit 403 | 2 Bdrm. / 1 Bath | 1,148 sq.ft. | Market Rate | |
| | Unit 404 | 2 Bdrm. / 1 Bath | 1,148 sq.ft. | Market Rate | |
| | Unit 405 | 3 Bdrm. / 2 Bath | 1,591 sq.ft. | Market Rate | |
| | Unit 406 | 1 Bdrm. / 1 Bath | 675 sq.ft. | Moderate Income | |
| Third Floor | Unit 301 | 2 Bdrm. / 2 Bath | 1,023 sq.ft. | Moderate Income | |
| | Unit 302 | 3 Bdrm. / 2 Bath | 1,403 sq.ft. | Moderate Income | |
| | Unit 303 | 2 Bdrm. / 1 Bath | 1,148 sq.ft. | Moderate Income | |
| | Unit 304 | 2 Bdrm. / 1 Bath | 1,148 sq.ft. | Moderate Income | |
| | Unit 305 | 3 Bdrm. / 2 Bath | 1,591 sq.ft. | Market Rate | |
| Second Floor | Unit 201 | 2 Bdrm. / 2 Bath | 838 sq.ft. | Very Low Income | |
| | Unit 202 | 2 Bdrm. / 1 Bath | 802 sq.ft. | Very Low Income | |
| | Unit 203 | 1 Bdrm. / 1 Bath | 587 sq.ft. | Very Low Income | |
| | Unit 204 | 1 Bdrm. / 1 Bath | 567 sq.ft. | Very Low Income | |
| | Unit 205 | 1 Bdrm. / 1 Bath | 567 sq.ft. | Very Low Income | |
| Ground Floor | Unit 206 | 1 Bdrm. / 1 Bath | 567 sq.ft. | Very Low Income | |
| | Unit 207 | 1 Bdrm. / 1 Bath | 567 sq.ft. | Very Low Income | |
| | Unit 208 | 2 Bdrm. / 1 Bath | 794 sq.ft. | Very Low Income | |
| | Unit 209 | 2 Bdrm. / 1 Bath | 759 sq.ft. | Very Low Income | |
| | Unit 210 | 1 Bdrm. / 1 Bath | 659 sq.ft. | Very Low Income | |
| Ground Floor | Unit 211 | 1 Bdrm. / 1 Bath | 590 sq.ft. | Very Low Income | |
| | Unit 212 | Studio / 1 Bath | 420 sq.ft. | Very Low Income | |
| Total 30 Units | | | | | |

| Parking Type | Dimensions | Number of Spaces |
|----------------------------------|-------------|----------------------------|
| Standard Spaces (S) | 8'-6" x 18' | 12 spaces |
| Compact Spaces (C) | 7'-5" x 15' | 12 spaces |
| ADA Spaces (A) | 9' x 18' | 2 spaces |
| Adjacent Spaces outside Building | 8'-6" x 18' | 2 Tandem spaces = 4 spaces |
| | 8'-6" x 18' | 2 spaces |
| | 8'-6" x 16' | 1 space |
| Total Parking Spaces | | 33 total spaces |

BAYVIEW DESIGN, A DIVISION OF BAYVIEW, INC.
4400 CLAYTON AVENUE, SUITE 100
CLAYTON, CA 94520
510-654-3255

Affordable Housing Plan

Alameda Boatworks Green

MAR 10, 2020

DATE

MFB-1

2228 & 2235 CLEMENT AVENUE, ALAMEDA, CA

Ground Floor

Multi-Family Building Plans

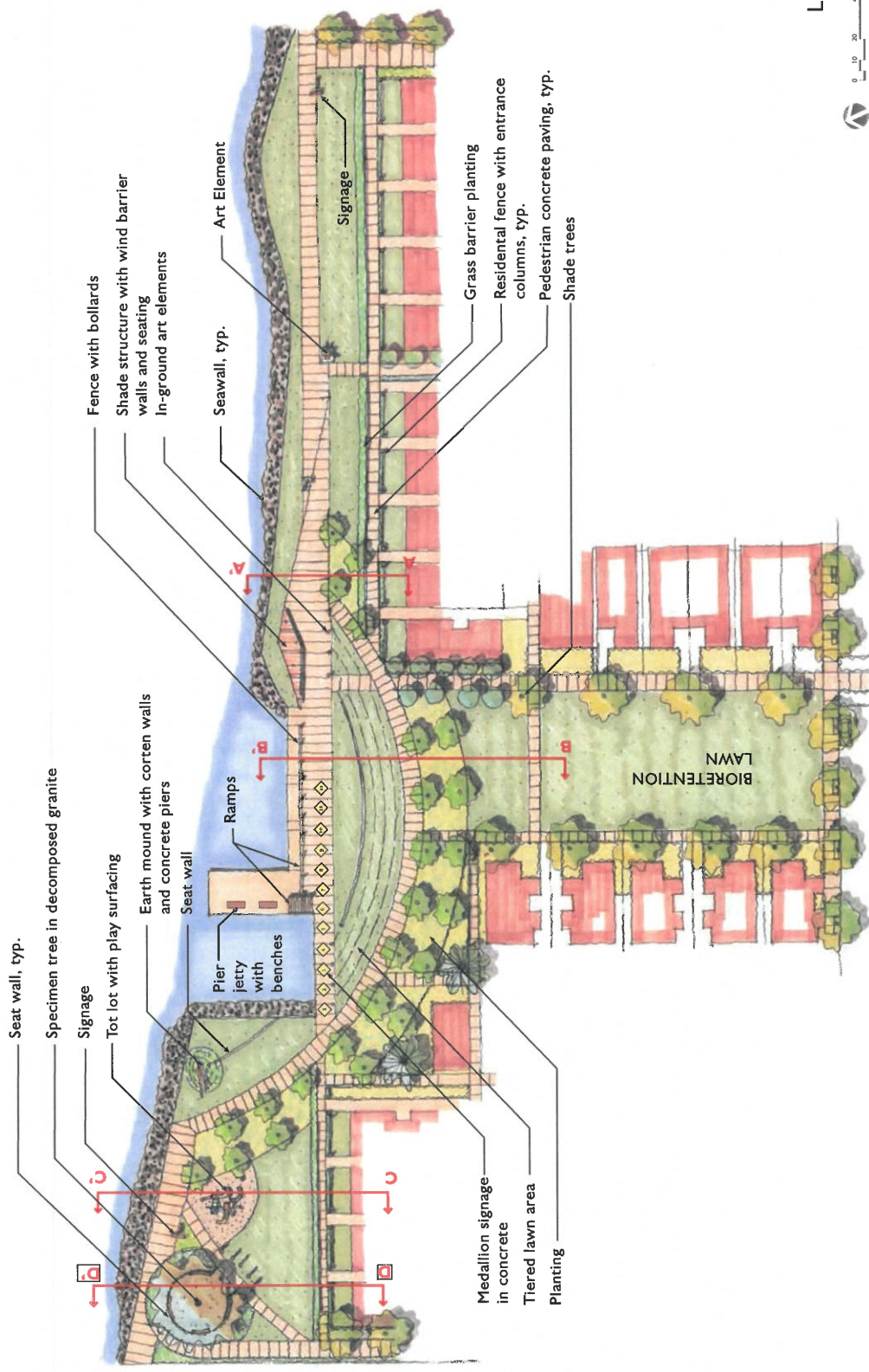


Mrkt=Market Rate 9 Units
Mod=Moderate Income (Affordable) 8 Units
VL=Very Low Income (Affordable) 13 Units
30 Units total

L = Laundry Room
T = Trash Chute
E = Elevator



0' 4' 8' 20'

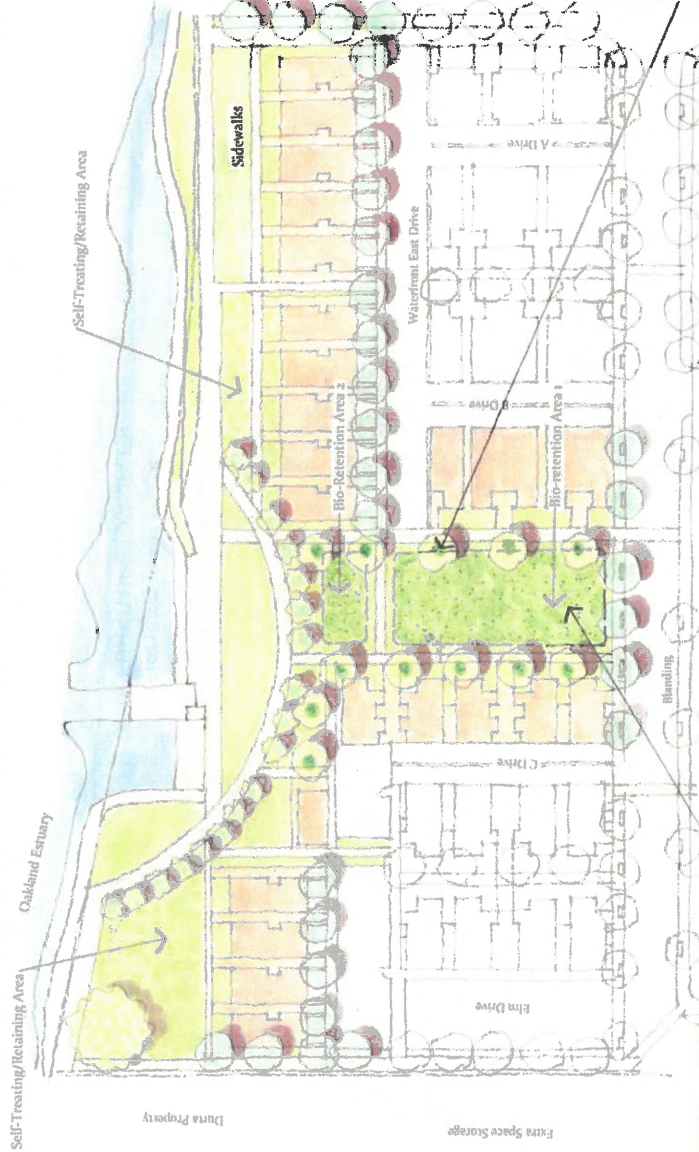


L1



The Boatworks Central Green Space/Bio-retention area is planted with Blue Rye Grass, a mix of Ryegrass and Kentucky Blue Grass typically installed on athletic fields. This area will be able to withstand daily use by project residents and community visitors. This mix can also take a lot of sunshine near the coast, requiring more water and more fertilizer than dwarf fescue (especially during hot seasons). Blue Rye Grass has good winter disease tolerance, excellent reparability and presents a deep green color across all seasons. Use of the area after a storm depends on rainfall intensity and duration. The Central Green Space/Bio-retention area should drain at a minimum percolation rate of 5" per hour. If properly installed the space should be able to accept use within a few hours.

The trees along the perimeter and planted in the sidewalk shoulder at the Central Green Space/Bio-retention area are Melaleuca quinquenervia-Cajuput Tree, an Australian native that grows well in Northern California in coastal environments. Also known as the "Tea Tree" it is an evergreen that requires little to regular water, has thick light brown/whitish bark, narrow oval pale green leaves and foliage that turns purple in frosts. This species also has yellowish white sometimes pinkish flower spikes and makes an excellent street tree.



Blue Rye Grass

Alameda Boatworks Green Open Space Bio-Retention Concept

2.5.16



Melaleuca quinquenervia





PLAY STRUCTURE



HARDSCAPE



SEATING



SCULPTURE / ART ELEMENT



RAILING TREATMENT



VIOLET'S



STORY BOARD

PROPOSED LANDSCAPE
SITE FURNITURE EXAMPLES

MAY. 18, 2016

L3

TREES



Acer palmatum 'Bloodgood'
Japanese Maple



Aucuba japonica
Red Horse Chestnut



Corylia alba 'Serrata'
Hildenhart Tree



Quercus agrifolia
Coast Live Oak



Thuja occidentalis 'Spiral'
Silver Linden



Zelkova serrata 'City Spirit'
Zelkova

TURF

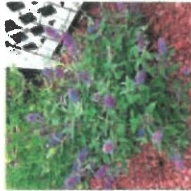


Della Blue Grass
50% Bluegrass / 50% Ryegrass Blend

SHRUBS



Anemone hybrid
Kangaroo Paw



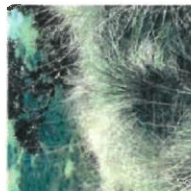
Buddleia davidii 'Blue Chip'
Butterfly Bush



Oenothera biennis 'El Compadre'
Cape Bush



Cornus 'Dorothy'
Red Fuchsia



Pennisetum setaceum
California Fescue



Helianthus scaberrimus
Blue Oak Grass



Juncus patens
California Gray Rush



Lonicera japonica
English Lavender



Metrosideros polymorpha
'Prosperrum'



Pennisetum setaceum
'New Zealand Flax'



Pennisetum setaceum
'New Zealand Flax'



Pennisetum setaceum 'Canyon Prince'
Canyon Prince Wild Rice



Lomandra longifolia
'Dwarf Plant Rush'



Muhlenbergia rigens
Dwarf Grass



Agrostis nemoralis
Sweetgrass



Saxifraga oppositifolia
'Saxifraga'



Tussockia verticillata
Princess Flower



Pennisetum setaceum 'Eaton Canyon'
Dwarf Purple Fountain Grass



Pennisetum setaceum 'Eaton Canyon'
Dwarf Purple Fountain Grass



Saxifraga oppositifolia
'Princess Flower'

PROPOSED LANDSCAPE PLANT PALETTE

MAY. 18, 2016

L4

* * * * *

I, the undersigned, hereby certify that the foregoing Resolution was duly and regularly adopted and passed by the Council of the City of Alameda in a regular meeting assembled on the 7th day of April, 2020, by the following vote to wit:

AYES

NOES:

ABSENT:

ABSTENTIONS:

IN WITNESS, WHEREOF, I have hereunto set my hand and affixed the seal of said City this 8th day of April, 2020.

Lara Weisiger, City Clerk
City of Alameda

APPROVED AS TO FORM:

Yibin Shen, City Attorney
City of Alameda